

GWL Realty Advisors

1220 STATION STREET

Rezoning Application

October 2nd, 2025



Musson
Cattell
Mackey
Partnership



MCM Partnership and GWL Realty Advisors acknowledge that this development is situated on the unceded traditional territories of the **xʷməθkʷəy̓əm** (Musqueam), **Sḵwxwú7mesh** (Squamish), and **səlilwətał** (Tseil-Waututh) Nations. This place is the unceded and ancestral territory of the **hən̓q̓əmi̓n̓əm** and **Sḵwxwú7mesh** speaking peoples, the **xʷməθkʷəy̓əm**, **Sḵwxwú7mesh**, and **səlilwətał** Nations, and has been stewarded by them since time immemorial.

EXECUTIVE SUMMARY



GWL Realty Advisors Inc. ("GWLRA") is pleased to provide this revised application package. This package outlines our design response to City Staff commentary which form the basis of our revised rezoning application.

First, some background on the applicant. GWL Realty Advisors Inc. is a real estate investment and management company whose mandate is to deliver stable investment returns to its pension and insurance fund clients through long-term investment. Our practice is to acquire, develop and maintain best-in-class buildings, as well as to offer unparalleled service to the residents and tenants of those buildings and the communities in which they are located.

Our objectives for the site are as follows:

- Create a unique place defined by high quality design, functionality and first-class amenities, for hundreds of new residents to call home.
- Maximize the number and diversity of homes in the form of purpose-built rental suites that we can offer to Vancouver rental market, including in support of the adjacent New St Paul's Hospital.
- Achieve excellence in design and integration into the surrounding neighbourhood, respecting all stakeholders to the process including the City.

We have worked with staff for over two years and appreciate the strategic collaboration as the City policy has shifted on the project site. The plans that are being resubmitted follow careful consideration of what we heard in our discussion with Planning staff, balanced with what we require to execute on this project.

GWLRA are longstanding developers in Vancouver with an established track-record of delivering office and rental housing that meet critical Council objectives.

For this reason, we appreciate Staff's consideration and support of our design, which carefully considers the long-term financial viability and operational feasibility required to get this project off the ground.

It is also worth noting that Staff's collaborative approach on our project aligns strategically with City Council's recent decisions to encourage and support housing development in the city in light of the unprecedented financial challenges facing the development industry.

We look forward to further discussions with City Staff, advisory bodies, and the community as the application proceeds through the latter stages of the rezoning process.

Sincerely,

Geoff Heu
Vice President, Development -
Western Canada
GWL Realty Advisors

Sincerely,

Michael Reed
Vice President, Development -
Western Canada
GWL Realty Advisors



Project Rendering - 1220 Station Street

WHO WE ARE



About GWL Advisors Inc. (GWLRA)

GWL Realty Advisors Inc. ("GWLRA") is a real estate investment and management company who creates stable investment returns for pension funds and institutional clients through long-term investment in, and proactive management of, real estate assets.

As a national real estate services firm, our practice is to acquire, develop and maintain sustainable, accessible and best-in-class buildings, as well as to offer excellent service to the residents and tenants of those buildings.

Commitment to Residential Rental in Vancouver

Though GWLRA invests in all asset classes of commercial real estate, we are strong proponents for, and have a substantial portfolio of, rental housing. With regional and local property management offices and community managers working directly within our multi-family rental buildings, we offer residents consistent and professional service on a personal basis. We are a reputable, experienced, and committed rental developer with extensive experience in property management and development both in Vancouver and across Canada.

In addition to the recently completed Chronicle (825 Nicola St), a 128 unit rental building in the Lower Robson Corridor – the first of its kind in decades, we have current projects at 1055 Harwood St (269 units) and at 1555 Robson Street (393 Units).



Project Rendering - Harwood



Project Rendering - Robson



Project Rendering - Chronicle



Project - Laurier House

THE VISION

GWL is thrilled to play an integral role in creating a vibrant community in the False Creek Flats. This site is the missing link that will connect the Main Street SkyTrain Station to the new St. Paul's Hospital - creating a lively and attractive node for the West Flats neighbourhood to live, work, shop, and play.

Transit-Oriented, Amenity-Rich Location

The site is adjacent to the Main Street-Science World SkyTrain station, and within a designated Transit-Oriented Area (TOA). In addition, the site is well served by multiple bus routes, bicycle infrastructure, Pacific Central Station, and is walkable to community amenities including schools, community centre, parks, shops and services and the False Creek seawall.

We are envisioning bringing a transit-oriented, mixed-use project that will add workforce-supporting rental homes immediately adjacent to Main Street SkyTrain Station and within walking distance to the new St Paul's Hospital.

Our proposal for the site is for:

- two 28 & 36 storey residential rental towers linked together by an 8-storey podium;
- a total project density of 11.19 FSR, including 1.96 FSR of employment generating uses;
- 470 rental homes (20% of which will be below-market);
- approx. 4,700 sq.ft of ground-level retail space;
- approx. 26,000 sq.ft grocery store (second-level);
- approx. 50,550 sq.ft of office/medical lab space;
- a 5,320 sq.ft public plaza at the southwest corner of the site.

Since the site is currently vacant and previously had a commercial building on it, the redevelopment will not displace any residential tenants and results in a 100% net gain in rental housing, which can be efficiently delivered upon completion of a successful rezoning.

Upon completion, this project will serve as both the gateway into the False Creek Flats and as the hub for the emerging West Flats neighbourhood.



PLANNING CONSIDERATIONS

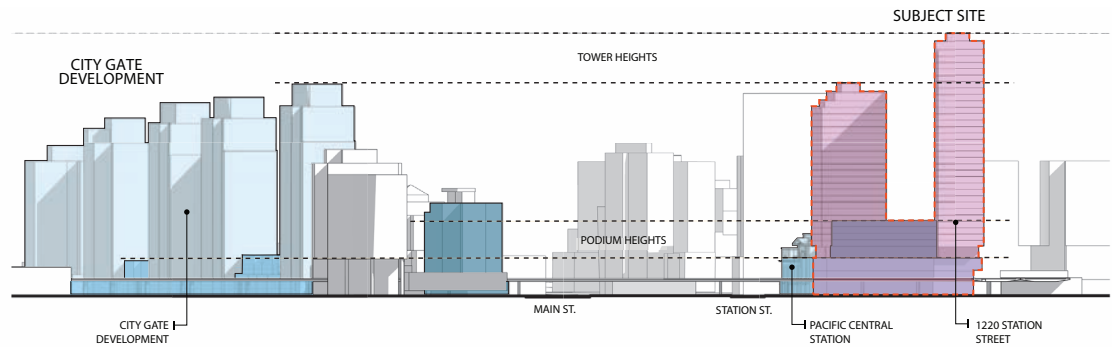
Our proposal for the site addresses a number of regional and municipal planning considerations and objectives.

1. ALIGN WITH PROVINCIAL LEGISLATION, REGIONAL AND CITY OF VANCOUVER POLICIES AND CITY-BUILDING OBJECTIVES

A mixed-use, transit-oriented, hospital-supporting project at this location aligns and supports Provincial, regional, and municipal city-building objectives including locating a mix of uses, including workforce rental housing and employment opportunities, by transit.

The proposal is guided by and meets the intent of the False Creek Flats Plan's Health Hub sub-area by delivering hospital-supporting employment uses including retail, and medical offices and laboratories, and public realm improvements including a new public plaza and connections to the hospital.

The proposed building height of 28 & 36 storeys responds to the amended view cone limits, while respecting the Queen Elizabeth view cone. Shadow impact on Thornton Park are mitigated by a new public plaza proposed at the southwest corner of the site, which provides additional outdoor space and solar access for the neighbourhood.



Cross-section through Terminal Avenue looking North



2. FITS IN WITH THE NEIGHBOURHOOD CONTEXT

Designed by MCMP Architects, the proposed building form fits seamlessly into the existing urban fabric. At 28 & 36 storeys, the proposed building height takes reference from the City Gate towers to the west and respects the amended view cone height limits, while responding to the Provincial direction to locate more housing opportunities near rapid transit.



Project rendering - looking south from Station Street

3. PODIUM HEIGHT CONSISTENT WITH CITY PRECEDENTS

Context to Surrounding

The proposed podium height reflects the general podium heights of the mid-rise buildings and larger towers of adjacent City Gate, SEFC mid-rise forms, and some of the unique building forms on Main Street and adjacent Chinatown.

The proposed podium also reflects the heights and cornice lines of the Pacific Central Station heritage character, as referenced from the heritage directives in the New St Paul's Guidelines by the City.



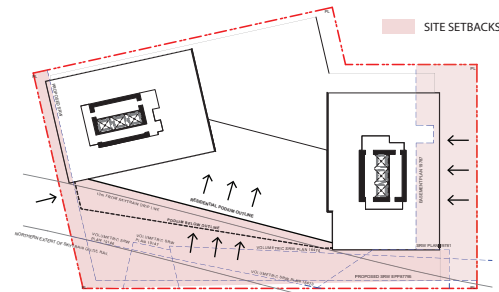
There are numerous precedents for podiums between 8-10 storeys in the immediate area (along Main St, in SEFC and Chinatown) and adjacent to Skytrain guideways on both the Expo and Canada Line.

There are also precedents for larger podiums with similar wide-street conditions such as the Cambie St boulevard or Boundary Rd.

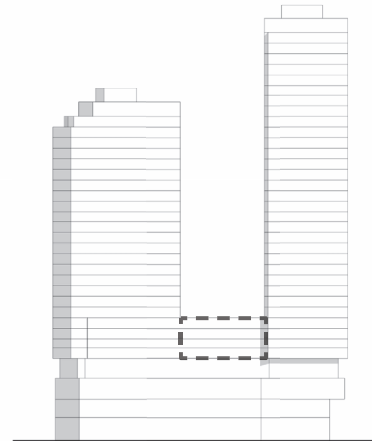
Site Constrains on Podium

The site is significantly constrained by Skytrain setbacks and maintaining an easement to support adjacent industrial properties. This results in a 38.4% reduction in site area for building.

The commercial, retail and potential furnished rental uses that require location in the podium result in an 8-storey mixed-use form (6-storey commercial equivalent).



Site setbacks floor plan.



Schematic elevation - residential podium 4-storeys high.

Visual Expression and Visibility

The dominant Skytrain guideway and articulated podium massing result in a perceived podium height of 4-storeys for a person viewing the development from key perspectives (e.g. Thornton Park or Main & Terminal).



Guideway reduces visible height of podium.

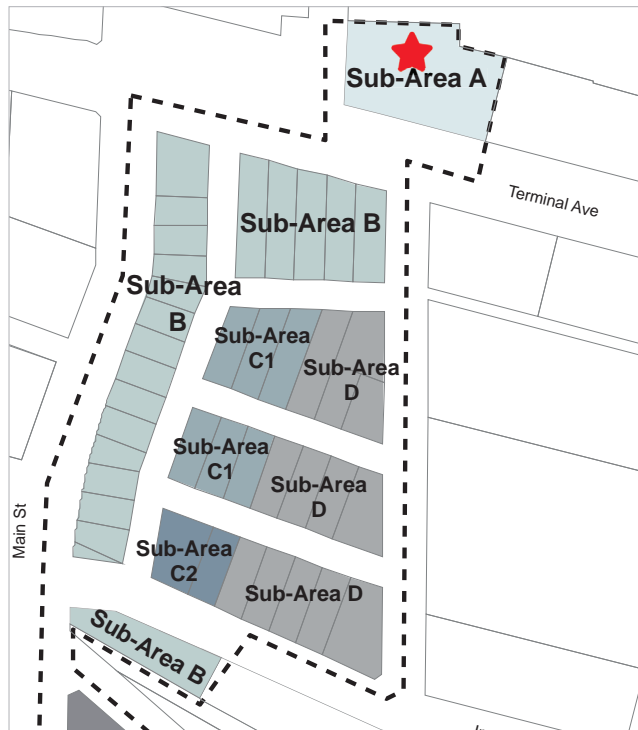


Only 4 stories of podium visible from North, West & East.

4. AMENDMENT TO EXISTING ZONING TO ALIGN WITH NEIGHBOURING SITES AND FALSE CREEK FLATS PLAN

The site comprises the entirety of sub-area A of the FC-2 zoning district, which is the only sub-area where residential is not permitted.

With the dip in demand for office space since the pandemic, planned supply of offices associated with the new hospital, and the Province’s direction to build more housing, especially near transit, we are seeking a rezoning to permit residential and additional height and density on this site, in alignment with sub-area B.



FC-2 Zone Sub-Areas



The site is located within the False Creek Flats Plan’s Health Hub sub-area. The proposal will deliver hospital-supporting employment uses including medical offices and lab space, and public realm improvements, including a new public plaza. An amendment to the Plan and zoning is required to allow for the delivery of over 470 rental, workforce-focused homes on site.



5. AMOUNT OF OFFICE IS APPROPRIATE AND MEETS ORIGINAL REQUIREMENTS

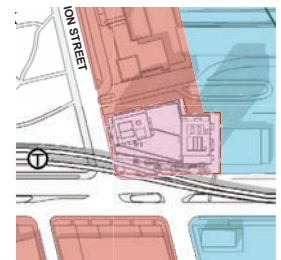
The 2025 EY office supply/demand report (commissioned for this application and submitted under separate cover) confirms a 55+ year supply of office in the area, and City policy generally seeks a 30-year supply. The current/former owners attempted unsuccessfully to lease the site for office multiple times over the past decade.

Impediments include the size of the site and building, coupled with the poor geo-technical conditions. Any demand in the next 30 years will be supplied by recent, still-vacant builds (Archetype and Nexus) and the 800,000 sq. ft. of office rezoned by Council to financially support the new St Paul’s Hospital.

6. REGIONAL GROWTH STRATEGY COMPLIANCE

The site is mixed-use FC zoning since 1986. Metro Vancouver staff confirms that the site is “General Urban”, even with a small portion of the eastern site as “Mixed Employment”.

An easement is maintained on the Mixed Employment portion to support access for the adjacent industrial properties.



- INDUSTRIAL
- MIXED-USE COMMERCIAL (FC-2A)
- MIXED-USE (FC / CD-1)
- - - PROPERTY LINE

4. SHADOW MANAGEMENT

The site is located east of Thornton Park, which contains a large number of mature trees that cast shadows upon the park regardless of any new development.



The previously approved 13-storey commercial building contributed additional shadows onto the park. A 20-storey building, as enabled by Provincial Legislation, will also do so. The proposed 28 & 36 storey building will cast some incremental shadow on the park, but it clears it at lunchtime throughout the entire year. A minimum of 80% of the park area is maintained for solar access at all times.



Shadow at 12PM on March 21

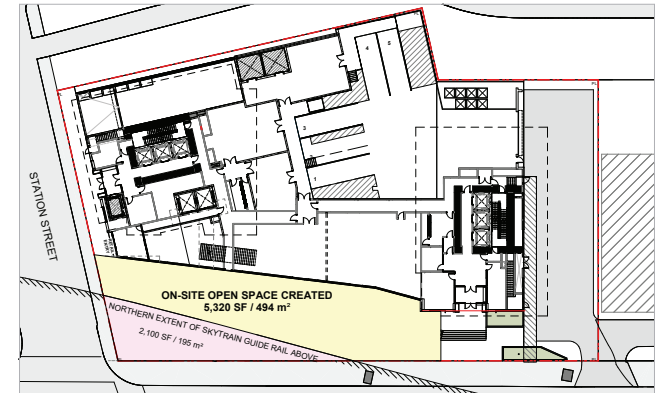


Shadow at 12PM on June 21



Shadow at 12PM on September 22

- PROPOSED BUILDING (36 STOREYS RESIDENTIAL)
- PROPOSED BUILDING (28 STOREYS RESIDENTIAL)



Proposed Public Plaza

The site is located at the edge of Downtown - the City's approach to Downtown sites is to "manage" as opposed to "outright restrict" additional shadows on parks. In response to the incremental shadowing on Thornton Park that will be attributed to this project, we are proposing a new 5,320 sq.ft public plaza at the southwest corner of the site, which will complement the park across the street and add to the amount of public space in the neighbourhood.

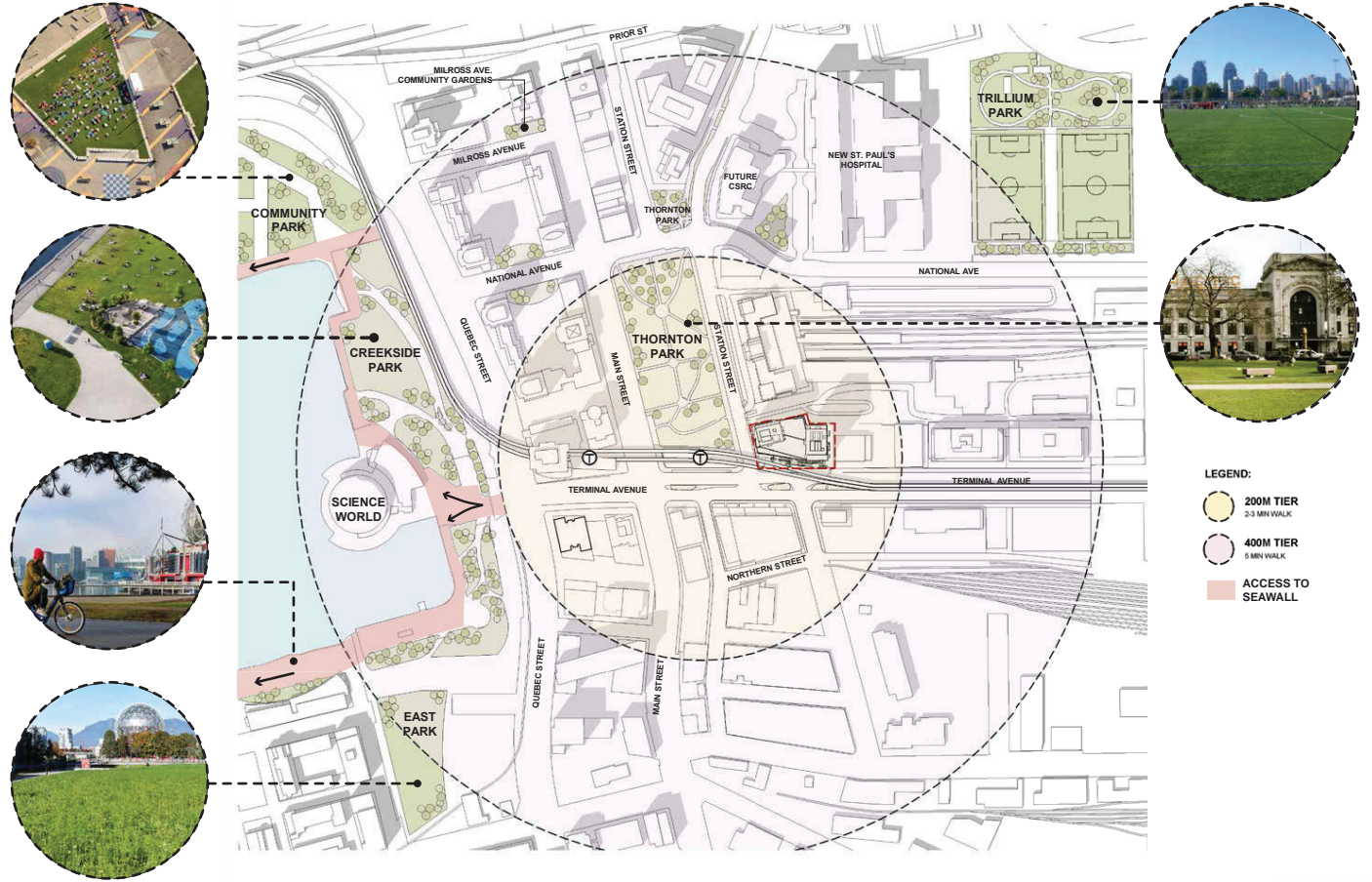


Proposed Public Plaza

EXCELLENT PARK/OPEN SPACE OPPORTUNITIES FOR SUNLIGHT WITHIN 5-MINUTE WALK

A significant benefit from the proposal for local residents, transit users and hospital employees is making Thornton Park safe and usable by increasing public safety from “eyes on the street” provided by building tenants and customers at all times of day.

There are also exemplary opportunities for solar access at all times of day in six parks within a five-minute walk of the site, including waterfront parks and seawall.



Solar Access Map within 5 minute walk.

COMMUNITY BENEFITS

This site represents the emerging western most neighbourhood within the False Creek Flats, anchored by the new St Paul's Hospital, connected by transit, and a gateway from False Creek to the Flats.

Our proposal will:



Create a Neighbourhood Hub to support the new St Paul's Hospital

- 1220 Station Street will provide an anchor for the West Flats neighbourhood creating a central node for different uses and activities. The proposal contributes to the creation of complete communities by providing a mix of uses that serve and enhance the neighbourhood including rental housing options, grocery, office, and retail. This is consistent with Council's direction to increase height in the area to create a vibrant neighbourhood around the new St Paul's Hospital.
- This proposal will help energize and improve public safety around Station Street and Terminal Avenue, and the area directly across from the SkyTrain entrance, and eventually connect the new St Paul's Hospital to the SkyTrain.

Increase the Supply of Rental Housing next to Rapid Transit

- In September 2023 the Province released housing targets for the City of Vancouver to deliver a total of 28,900 housing units over the next five years.
- Our proposal will locate workforce housing (approx. 470 units, 20% of which will be below-market rentals) close to transit (SkyTrain and bus routes), which responds to Provincial directions, encourages ridership, reduces car reliance, and provides convenient housing options for doctors, nurses, and other hospital staff close to the new St Paul's Hospital and the general public.
- The previous commercial use of the site means that no residential tenants were displaced, resulting in a 100% net gain in rental homes to help Vancouver get closer to meeting its housing targets.



Provide furnished rentals to support hospital workers and users

- Given the limited supply of hotels and restrictions on short-term rentals in the city, we are exploring the provision of furnished rentals to support visiting doctors, researchers, patients and their families in close proximity to the hospital.

Bring new Retail Opportunities & Economic Growth

- 1220 Station Street contemplates over 81,260 sq.ft of new commercial space including ground-level units to animate Station Street and Terminal Avenue, and an approx. 26,000 sq.ft grocery store, which is desperately needed in the area.
- A retail frontage will generate foot-traffic, draw people in, inject energy and life, and improve public safety of Thornton Park and the surrounding neighbourhood.
- The commercial component (retail + office) results in upwards of 400 new job opportunities, which supports the economic growth of the city.



Increase Medical Office and Lab Space in the area

- 1220 Station Street provides for over 50,550 sq.ft of office space to support the emerging hospital campus and contributing to the creation of a Health Hub.

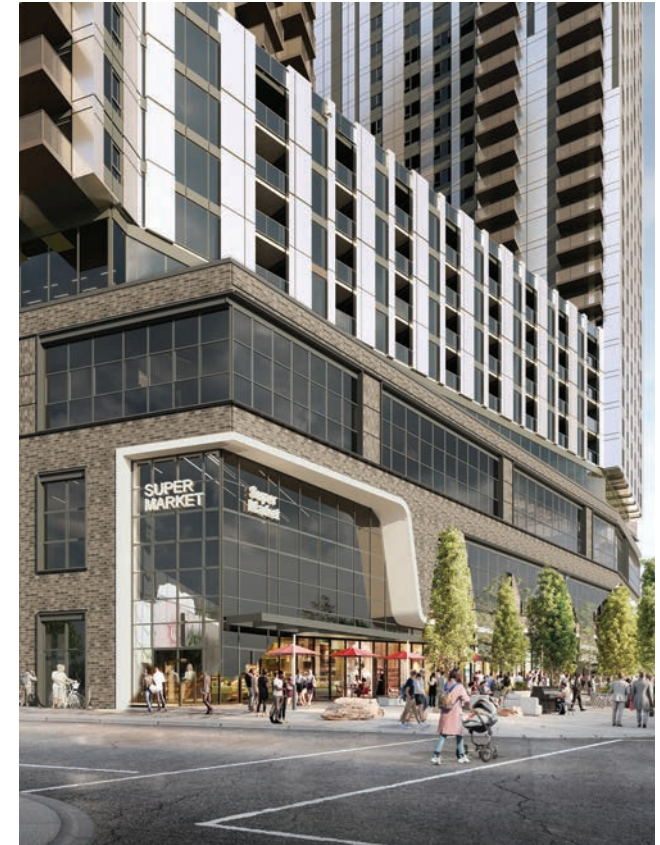


Animate the Public Realm and enhance Thornton Park

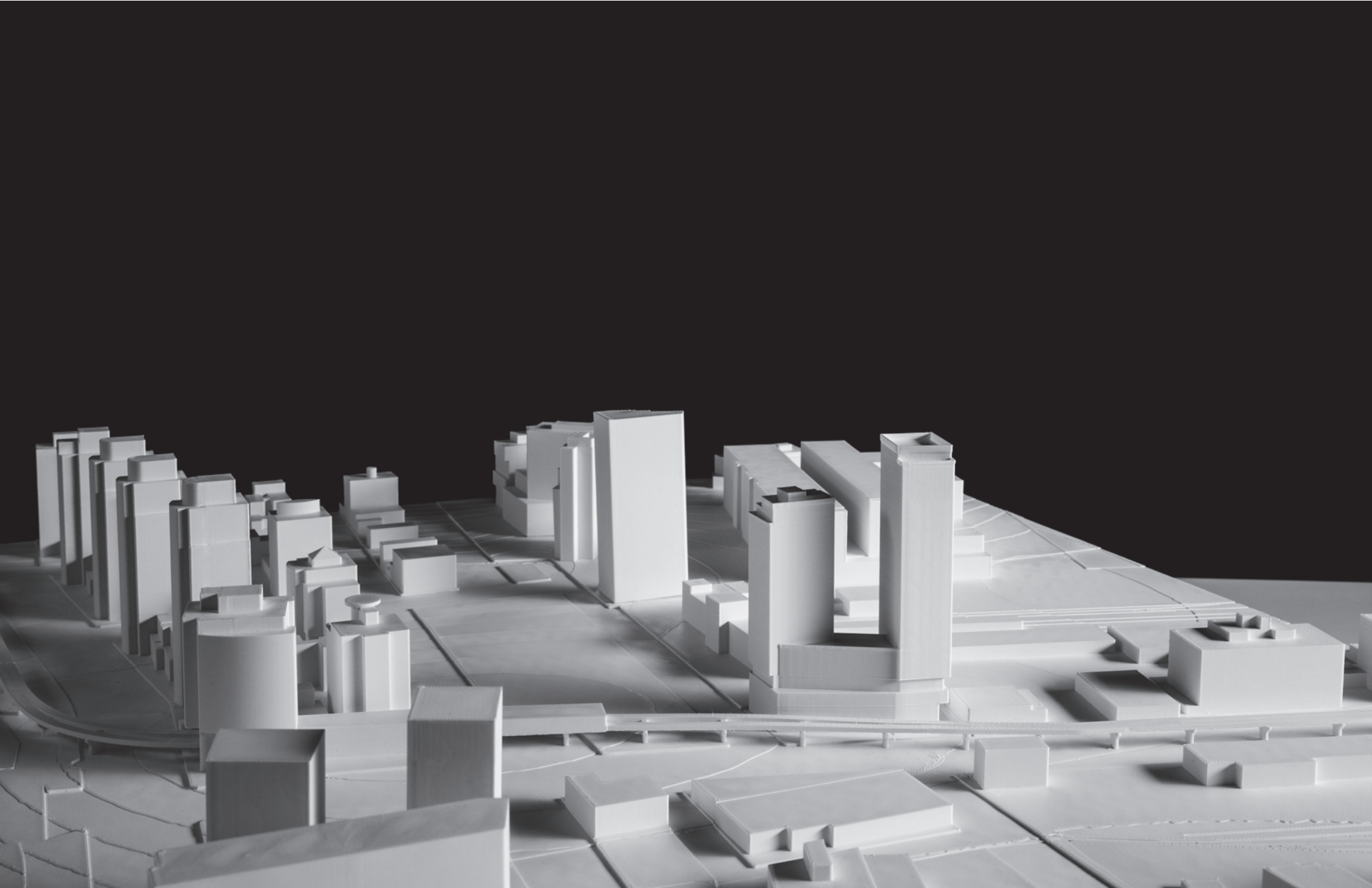
- Upgrades along Station Street and Terminal Avenue including new public realm improvements that will make the street frontages safer and more attractive for all users.
- The fronting of active retail uses onto the streets will invite people to the area and generate interest and provide “eyes-on-the-street” and onto Thornton Park and the SkyTrain entrance.
- A well programmed public realm will help create a gateway feel into the Health Hub sub-area.

Deliver a new Public Plaza

- A 5,320 sq.ft public plaza is proposed at the southwest corner of the site, which will be programmed with outdoor seating for the retail units, and planters and benches; creating an inviting and safe space for those living and working in the building and those visiting the site to enjoy.



Proposed Public Plaza



PROJECT TEAM

ARCHITECT

Musson Cattell Mackey Partnership

Suite 1900 – 1066 West Hastings Street
Vancouver, BC, Canada, V6X 3X1
T 604 687 2990

CLIENT

GWL Realty Advisors Inc.

1600 – 650 West Georgia Street
Vancouver, BC, Canada, V6B 4N7
T 604 713-6450

Acoustic

Rowan Williams Davies & Irwin Inc.

280 – 1385 W 8th Ave
Vancouver, BC, Canada, V6H 3V9
T 604 730 5688

Arborist

Durante Kreuk Ltd.

102 – 1637 W 5th Ave
Vancouver, BC, Canada, V6J 1N5
T 604 684 4611

Civil Engineering

Creus Engineering

610 - East Tower, 221 Esplanade West
North Vancouver, BC, Canada, V7M 3J3
T 604 987 9070

Code

Jensen Hughes

500 – 1901 Rosser Ave
Burnaby, BC, Canada, V5C 6R4
T 604 684 2384

Electrical

Nemetz (S/A) & Associates Ltd.

2009 W 4th Ave W
Vancouver, BC, Canada, V6J 1N3
T 604 736 6562

Elevator

Gunn Consultants Inc.

166 – 1020 Mainland St
Vancouver, BC, Canada, V6B 2T5
T 604 630 2276

Envelope

BC Building Science Ltd.

611 Bent Ct
New Westminster, BC, Canada, V3M 1V3
T 604 520 6456

Environmental

SLR Consulting Ltd.

200 – 887 Great Northern Way
Vancouver, BC, Canada, V5T 4T5
T 604 738 2500

Geotechnical

GeoPacific Consultants Ltd.

1779 W 75th Ave
Vancouver, BC, Canada, V6P 3T1
T 604 439 0922

Landscape

Durante Kreuk Ltd.

102 – 1637 W 5th Ave
Vancouver, BC, Canada, V6J 1N5
T 604 684 4611

Mechanical

Introba

180 – 200 Granville St
Vancouver, BC, Canada, V6C 1S4
T 604 687 1800

Planning

Pooni Group

200 – 1055 West Hastings
Vancouver, BC, Canada, V6E 2E9
T 604 731 9053

Structural Engineering

Glotman Simpson

1661 W 5th Ave
Vancouver, BC, Canada, V6J 1N5
T 604 734 8822

Survey

Bennett Surveys Ltd.

201 – 275 Fell Avenue
North Vancouver, BC, Canada, V7P 3R5
T 604 980 5856

Sustainability

Introba

180 – 200 Granville St
Vancouver, BC, Canada, V6C 1S4
T 604 687 1800

Traffic

Bunt & Associates

1550 – 1050 W Pender St
Vancouver, BC, Canada, V6E 3S7
T 604 685 6427

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Chapter 1: Applicable Policies

DEVELOPMENT PROPOSAL AND OBJECTIVES

- A transit-oriented, mixed-use development of secured rental homes and retail (incl. grocer) to support St Paul's Health Hub/Creative District
- Rezoning response to Provincial TOA and Housing Targets, and Council decisions to catalyse rental neighbourhood in the West Flats through View Corridor changes
- Site is vacant, serviced, and ready for > 400 new rental homes (with no tenant displacement)
- Meets min. 1.5 FSR job space requirement of FC-2 zoning, and consistent with City and Metro industrial land policy ("General Urban")
- Rezoning application seeks to amend FC-2 sub area / False Creek Flats Plan (2017)



CURRENT ZONING AND POLICY CONTENT

Policy Alignment

This rezoning application for 1220 Station Street aligns with Municipal, Regional and Provincial policy to create affordable and secure market rental homes (and 20% below-market units) immediately adjacent to the rapid transit access at Main Street-Science World SkyTrain.

The application also generally aligns with Council's recent decisions to create a new transit-oriented neighbourhood of rental housing in the West Flats through the implementation of the TOA Rezoning Policy and the relaxation of protected public views across the site and area to create additional rental housing opportunities.

Finally, this application is consistent with the intent of the False Creek Flats Plan to create rental housing opportunities to support the newly-constructed St Paul's Hospital and transition to the industrial lands east of the project site.

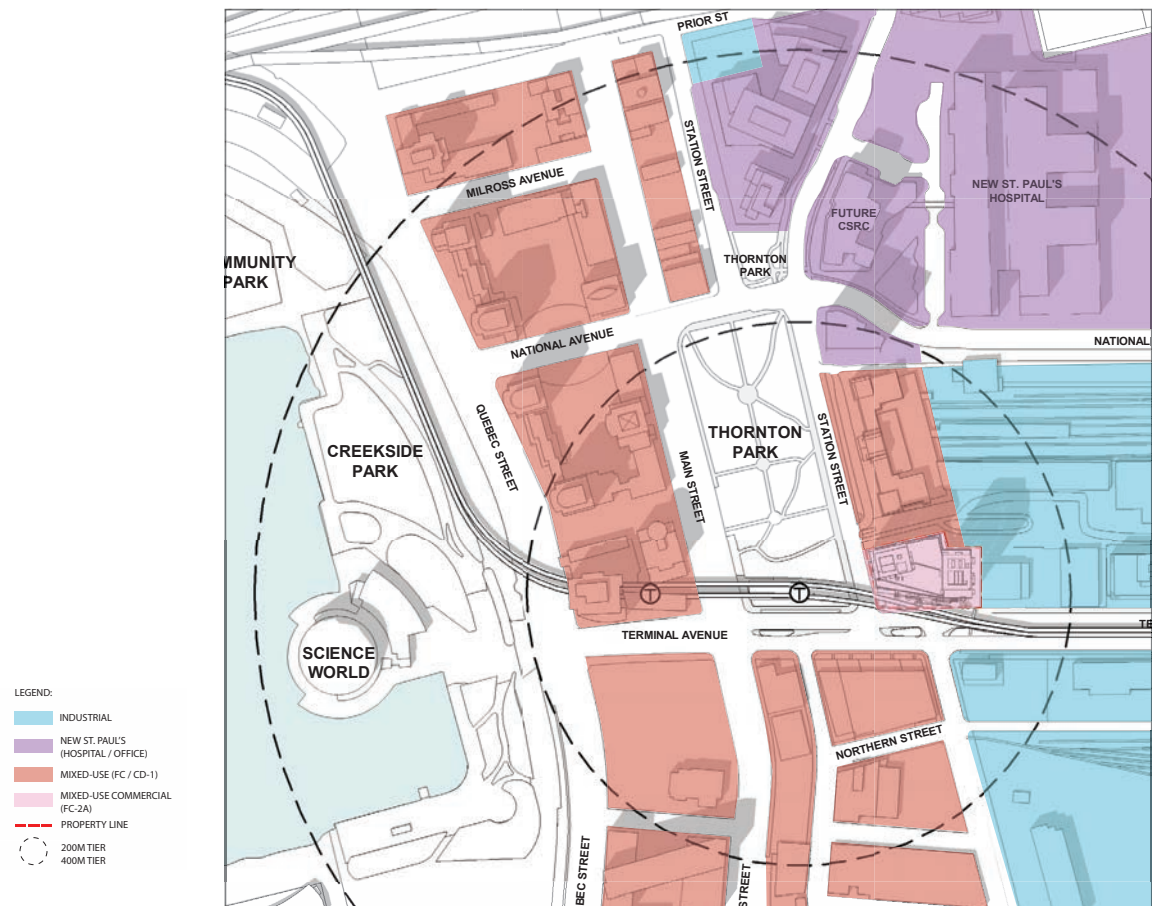
The rezoning application reflects the general intent of the existing FC-2 zoning in creating mixed-use/residential zoning in the West Flats, but seeks rezoning of the FC-2A sub-area to restore the residential use that was removed by the Flats Plan in 2017 due to a no-longer present livability concern created by the former intercity bus station at Pacific Central (Greyhound ceased operations in 2018, see Land Use section).

Land Use Policy / Regulation	Proposal Concurrence with Policy / Regulations	Y/N
Housing Vancouver & Province Housing Targets	~400 rental homes and workforce housing for New St Paul's Hospital, without tenant displacement	✓
Metro Vancouver / City Industrial Lands Policy	"General Urban" designation, and maintains easement to access to adjacent industrial lands	✓
Provincial TOA Regulations	Aligns with Provincial TOA Tier 1 at Main St Skytrain, proposes 20+ stories and >5 FSR	✓
View Protection Guidelines	Compliant with Council's recent revision to Main St View/QE View	✓
City-Wide Development Guidelines	Floorplates/shadows to be managed as per longstanding Downtown practice	✓
FC-2 Zoning District	Consistent with mixed-use FC-2 zoning, sub-area amendment required to restores residential use	Sub-Area Rezoning

CURRENT ZONING AND POLICY CONTENT

This proposed development has been guided by the relevant zoning policies, and takes into consideration the recent changes in policy, as listed below:

- Opening of New St Paul's and future office developments
- Provincial TOA Guidelines and Municipal Housing Targets
- View Corridor Changes by Council, including granting significant height increases on-site
- Planning for West Flats rental neighbourhood
- Closure of adjacent 24-hour Bus Station and elimination of livability concerns for the site



PROPERTY REPORT AND ZONING BY-LAW

Property Report

Local Area: Strathcona
 Multi-family Collection Zone: Purple North
 Sanitation: Purple North
 Nearest Community Centre: Creekside
 Nearest Community Centre Address: 1 Athletes Way
 Nearest Fire Hall: 1
 Nearest Fire Hall Address: 900 Heatley Av
 Responding Fire Hall: 1
 Responding Fire Hall Address: 900 Heatley Av

DISTRICT LOT 2037 NWD EXC PRT IN PL 22525.
 Postal Code:
 Record Status:
 Neighbourhood: 013

Address Information

Civic Number: 1220
 Street Name: STATION ST
 Address Status: Active
 Address Type: Main

Legal Information

Full Address: 1220 STATION ST
 PID: 015081575
 Primary Lot: D
 Block: G
 Plan Number: VAP22488
 District Lot: 2037
 Legal Type: LAND
 Folio Number: 630193070000
 Legal Description: LOT D BLOCK G PLAN VAP22488

Assessment Information

Assessment Year: 2023
 Big Improvement Year: 2020
 Current Improvement Value: \$10,000
 Current Land Value: \$69,044,000
 Previous Improvement Value: 0
 Previous Land Value: \$60,240,000
 Tax Levy: \$648,660.98
 Build Year: 2020

Zoning Information

Zoning: FC-2
 CD1 Number:
 Zoning District: FC-2
 Zoning Classification: Commercial

FC-2

FC-2

District Schedule

1 INTENT AND OVERVIEW

1.1 Intent

The intent of this schedule is to permit and encourage the development of a high-density mixed-use neighbourhood, including some dwelling and compatible industrial uses, and job space to support a wide spectrum of economic uses representing the broader False Creek Flats economy. This includes the creative, cultural and food economy, industrial production, research and development, and employment intensification. Acoustic regulations in [Section 10](#) of this by-law apply to dwelling units close to arterial streets.

The FC-2 district consists of 6 sub-areas identified as "sub-area A", "sub-area B", "sub-area C1", sub-area C2", "sub-area D" and "sub-area E" in [Map 1: Sub-Areas A, B, C1, C2, D and E in the FC-2 District](#) for the purpose of establishing permitted uses, floor area and building height.

Without limitation, applicable Council policies and guidelines for consideration include the [False Creek Flats Urban Design and Development Policies and Guidelines for FC-2 – The Innovation Hub](#).

Chapter 2: Analysis

THE SITE

The site is located immediately adjacent to Main Street-Science World SkyTrain Station on the western edge of the **False Creek Flats**, an area of significant historical, cultural, geographical, and economic importance to the city of Vancouver and its surrounding region.

Approximately 100 years ago, the Flats was a tidal estuary of great natural and cultural significance to the First Nations for thousands of years. Today, it spans over 450 acres of primarily employment land, situated near both downtown and the port. The Flats hosts hundreds of businesses across diverse and thriving sectors of the local economy. Located at the base of the downtown peninsula, the area is well-connected by rapid transit and surrounded by residential neighbourhoods including **Chinatown** and **Strathcona** to the north, **Grandview Woodland** to the east, **Mount Pleasant** to the south, and **Southeast False Creek, City Gate**, and the future **Northeast False Creek** neighbourhood to the west.

Over the last century, the site has undergone a significant transformation from natural to industrial land use.

The site is located in the **FC-2 Innovation Hub**, which falls within the **Creative Campus** sub-area. Located on the northwest edge of the Flats, this area is well-served by transit. Currently home to one SkyTrain Station, serving the Expo Line, this area will see another station built as part of an extension of the Millennium Line along Broadway. The area is centrally located between the Health Hub to the north and the Creative Campus to the south.

Covering an area of 41,427 SF / 3,849m², the site is adjacent to **Pacific Central Station** and across from **Thornton Park** and the **Main Street-Science World SkyTrain station**. The neighbouring buildings to the east are tenanted by industrial manufacturing companies and commercial office uses along Terminal Avenue. This development would be an identifiable marker for the Flats.

‘Key to False Creek Flats is the strategic economic potential of the seven acres termed the Innovation Hub that embraces business and economic experimentation and growth. Additional heights and densities should be supported by an amenity-rich node including a plaza, pedestrian connections, ground floor activations and amenity spaces.’

(Extracted from false-creek-flats-plan-2017-10-31)

This site has an enormous opportunity to support the “Innovation Hub” but providing the critical rental housing, retail services (including groceries) and amenities for the employees of the newly-constructed St Paul’s Hospital and the adjacent technology companies in the I-3 “High Tech” zoning.



NATURAL CONTEXT

The False Creek Watershed

The **False Creek Flats** was once a muddy tidal flat on the eastern end of False Creek until the early 20th century. As Vancouver's resource economy grew, filling the downtown peninsula and the shores of False Creek with industrial activity, the City sought to accommodate further economic expansion and additional rail terminals.

In 1913, prompted by several rail companies, the City held a plebiscite asking Vancouver residents to support filling in the eastern end of False Creek. After a favourable vote, the project began in 1915, using various materials from nearby districts, including landfill from development projects, scrap lumber and bricks from local mills, and general industrial waste. By 1917, the Flats was completely filled in.



Oil painting of the False Creek Flats in the 1950s by Tom Carter

LEGEND

- Today
- 1939
- Historic Shoreline

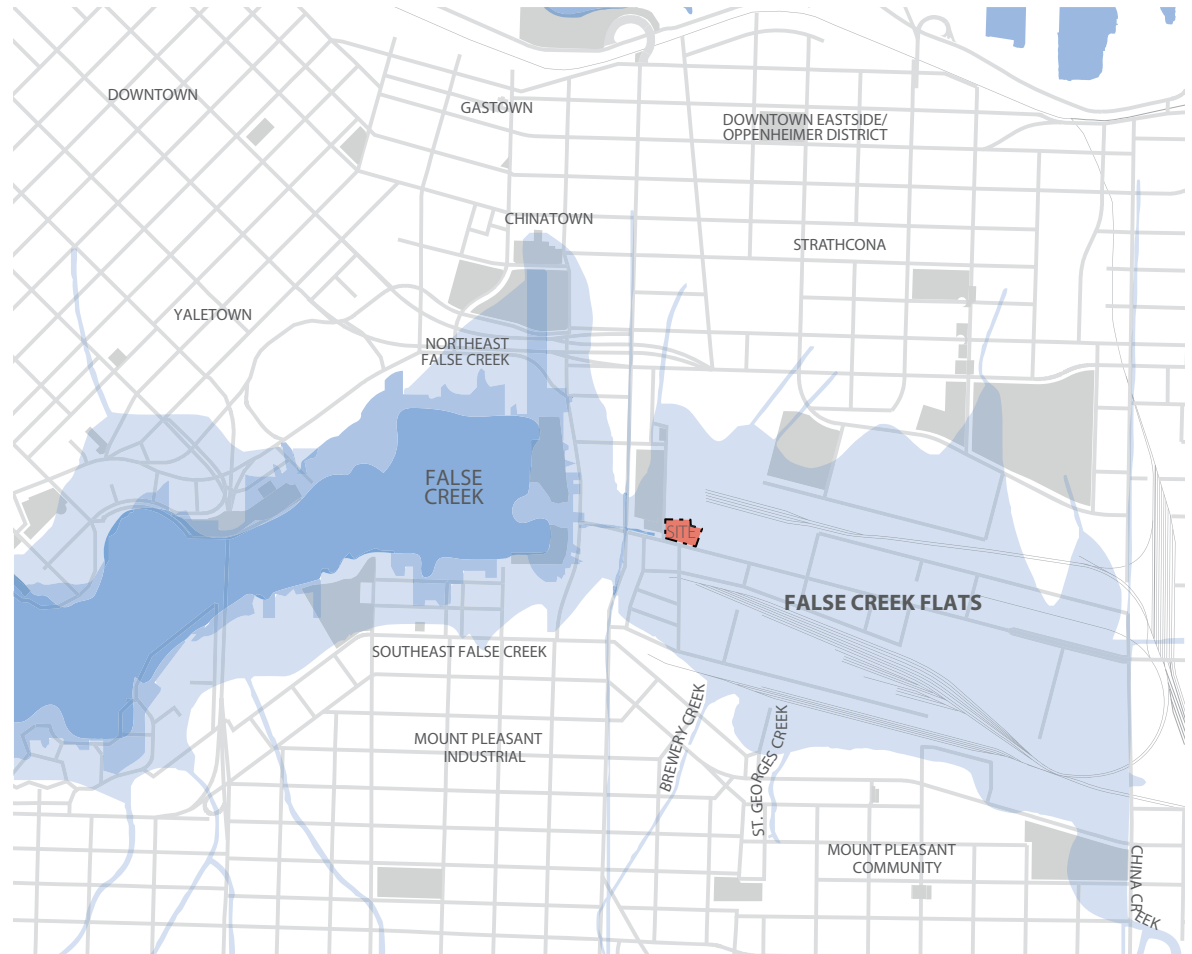


Diagram showing the False Creek Flats historic shoreline

LAND USE

Industrial/Rail Transportation

By 1919, both the Great Northern Railway (GNR) and the Canadian Northern Railway had established their new western terminals in the **False Creek Flats**, laying the groundwork for the area's industrial future. While the rail companies quickly set up their new rail yards and distribution networks, it took much longer for the Flats to establish itself as a viable alternative to the more central business locations in the city. As industrial lands in the urban areas of Gastown and Yaletown approached capacity, companies began to consider the Flats as a new option.

Throughout the 20th century, the rise of the automobile and trucking as dominant transportation modes led to the development of the four major arterial roads surrounding the Flats: Main, Terminal, Clark, and Great Northern Way, which remain significant borders today. Despite the prominence of automobile and trucking transport, rail infrastructure has remained a dominant feature in the Flats and continues to see varying use.

Today, the Flats are largely divided into east-west corridors due to the incursion of three major rail lines and yards into the area up to Station Street. The presence of these rail lines creates challenges for establishing a connected, cohesive environment.

The subject site for this proposal is defined by the **Pacific Central Station** to the north, which is the western terminus of the Canadian National Railway.



APPLICABLE POLICIES

1. KEY DIRECTIONS

Along the **Terminal Spine**, intensification of employment through the introduction of a limited amount of **Creative Products Manufacturing** is anticipated by the Flats, with the delivery of ground floor light industrial production, distribution, repair or industrial artist space. While this sub-area will no longer permit mini-storage, auto retail use could remain, but future developments will need incorporate a higher density of employees than is typically delivered in a stand-alone auto retail showroom.

In the western and southern areas of the **Health Hub** and **Creative Campus**, the intensified mixed employment area will be anchored around St. Paul's and Emily Carr, which will drive economic growth and synergistic spin-off opportunities. Both of these areas are well-served by rapid regional transit, and the are sub-areas will be anchored by the St. Paul's hospital and health campus, connected by Station Street linking the Pacific Central station through the new health campus to the DTES on Gore Avenue. Significant intensification of employment is anticipated across the area, with the Health campus including laboratories, research centres, creative products manufacturing, high tech, general office and health care offices.

In the south, the Creative Campus will be anchored by the new Emily Carr University of Art and Design (ECUAD) and the established and future institutional and economic uses tied to the Great Northern Way.

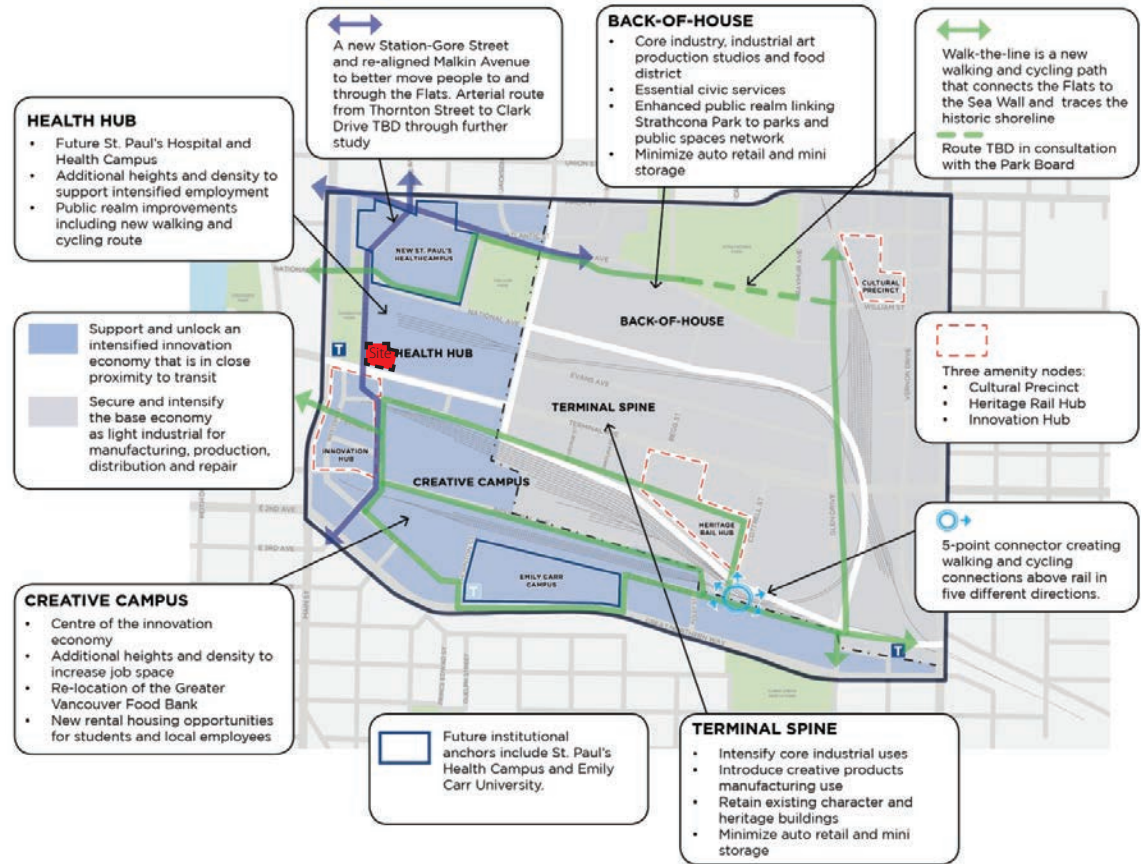


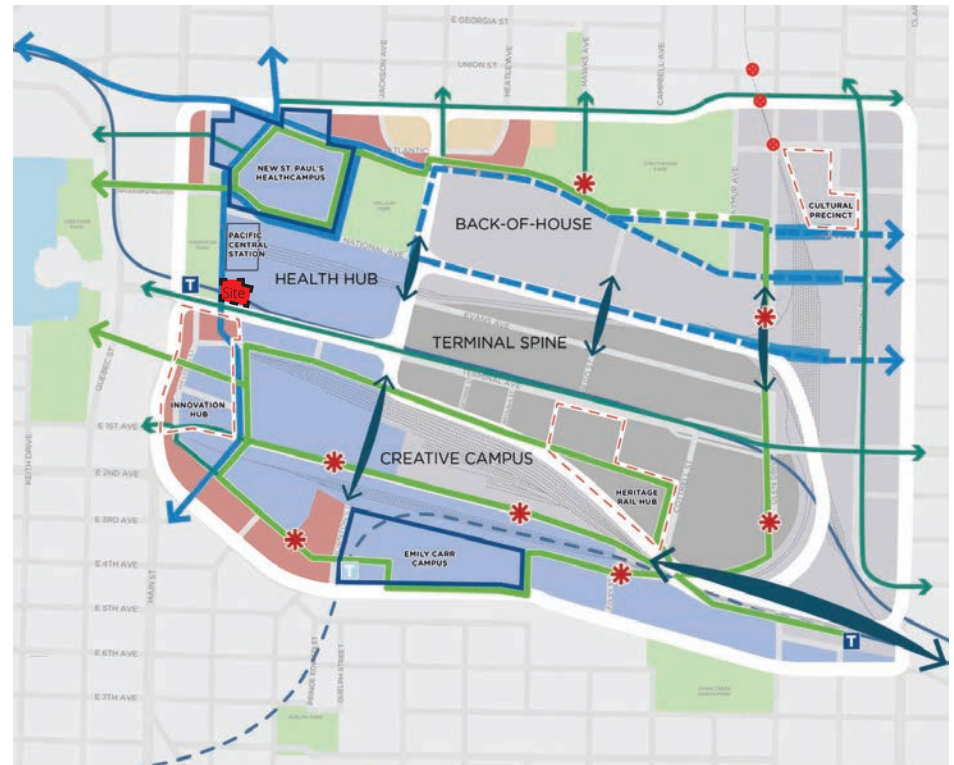
Figure 1: Summary of Key Directions

APPLICABLE POLICIES

2. THE STRUCTURE PLAN

The structure plan provides the overall physical framework for the local area plan and the policy directions outlined in the following chapters:

- Create a well-connected public realm that integrates the new hospital and health campus into the city and adjacent neighbourhoods.
- Create a strong linkage and northern starting point for Walk-the-Line.



FALSE CREEK FLATS STRUCTURE PLAN

LEGEND

- | | | | | | |
|---|---|--|---|--|--|
| | East-West arterial overpass 3 options TBD | | Amenity node | | Employment Areas |
| | New street linkages | | Public node | | Innovation, Technology, Creative, Research, Industrial |
| | Existing Transit station/skytrain line | | Residential | | Industrial, Production, Distribution, Service, Art |
| | Future transit station/skytrain line | | Mixed use with residential | | Industrial, Commercial, Creative Manufacturing |
| | Street closure at rail crossing | | | | |
| Walking and Cycling Improvements | | | | | |
| | New walking/ cycling bridges | | | | |
| | Walk the Line | | Route TBD in consultation with the Park Board | | |
| | Proposed walking/cycling routes | | | | |

Figure 2: False Creek Flats Structure Plan

APPLICABLE POLICIES



Figure 3: Pacific Central Station in the Health Hub

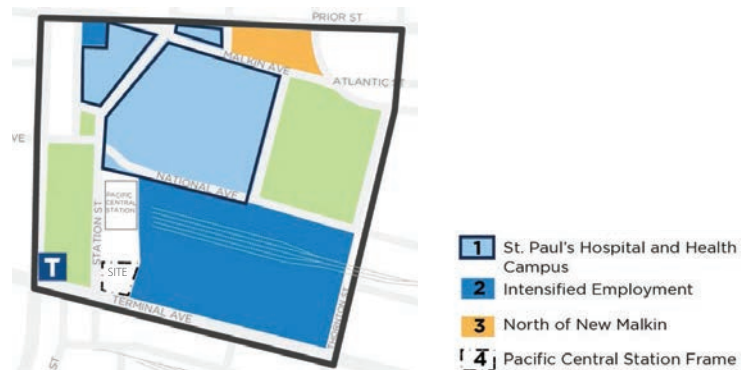


Figure 4: Taken from False Creek Flats Plan 7.1 Health Hub

3. ST. PAUL'S HOSPITAL AND HEALTH CAMPUS

Details of this area of the Health Hub will be finalized through the New St. Paul's Hospital and Health Campus Policy Statement process aligned with the following general policies.

GENERAL POLICIES

- Support new uses related to health care and the innovation economy
- Gore-Station linkage should become the high-street and central spine providing a key linkage of the innovation corridor from Railtown to Mount Pleasant (see 8.4.3).
- Buildings next to Pacific Central Station should be respectful of the historic building and create a frame.
- Ground floor uses should activate public life, especially along the

high-street and the connection to the Walk-the-Line loop (see 8.4.5).

- Establish places for public life along this key route including new plazas or significant public places within the St. Paul's Institutional Anchor and the Innovation Hub Amenity Node.
- Encourage more lively edges along this route (restaurants, cafés, and retail), as well as a higher level of transparency and more frequent openings for a more interesting street environment.
- Establish a high street along this route through the St. Paul's Institutional Anchor.

APPLICABLE POLICIES

4. ST. PAUL'S HOSPITAL HEALTHCARE CAMPUS (NSPHC) CD-1 GUIDELINES

The New St Paul's Healthcare Campus (NSPHC) CD-1 Guidelines were carefully reviewed.

The guidelines which are applicable to this project are as follows:

RETAIL SPACES:

- Designed with a minimum 4.8m (16ft) ceiling height
- Interior depth of no less than 10.6m (35ft)
- Express frontages of no more than 15.3m (50ft) in width
- Continuous weather protection must be provided along all retail frontages
- Primary building entrances should be clearly expressed

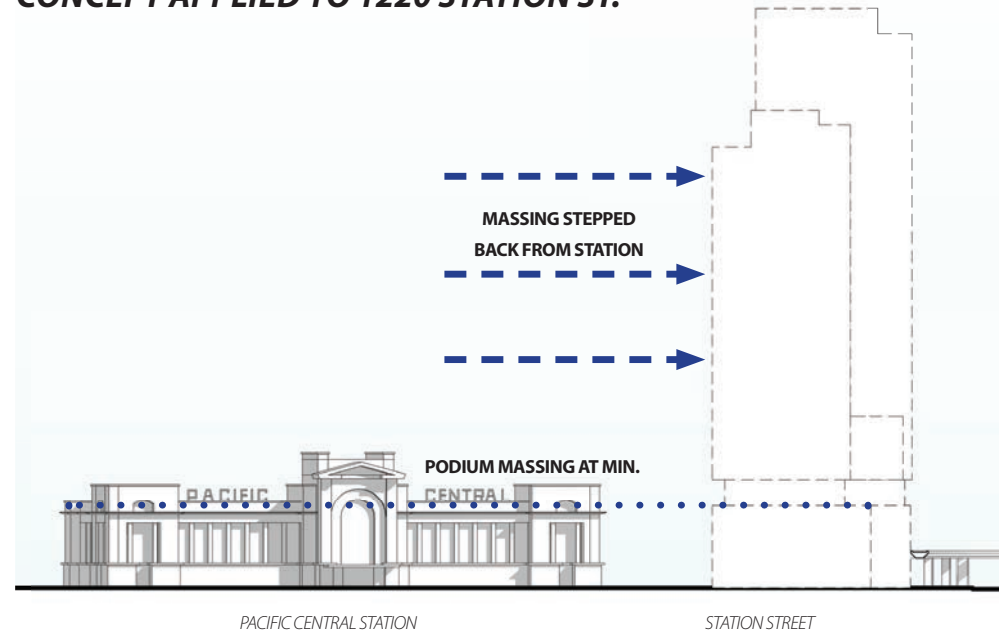
BUILDING FORM AND EXPRESSION:

- Informed by its heritage context
- Provide contemporary references of form and height of Pacific Central Station
- Minimize shadows on Thornton Park
- Orientating the massing of the building to street grids
- Providing retail uses at grade
- Mimicry of heritage is not supported
- Act in concert with Saint Paul's Hospital South Precinct to formally frame Pacific Central Station

"The building should present a minimum four-storey street wall to better respond to its corner situation. Further stepping back of the building form may be provided at upper levels as needed to mitigate shadowing on adjacent open spaces and to provide for modulation and visual interest at the roofline."

(Taken from Building Form and Expression in Section 5.4.2)

CONCEPT APPLIED TO 1220 STATION ST.



West Elevation of 1220 Station St.

TRANSPORTATION

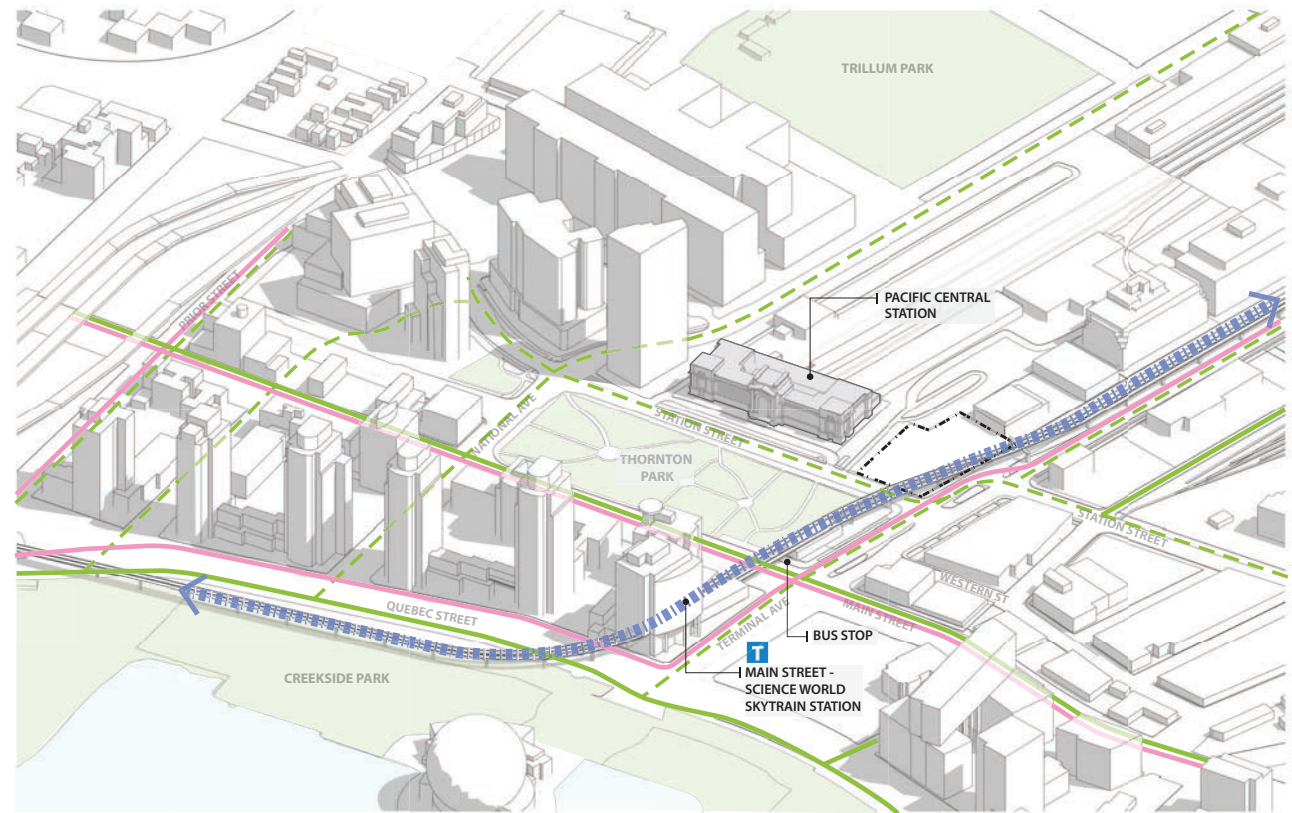
The site is well served by transit, with the existing Expo Line running past the site to the nearby **Main Street - Science World SkyTrain Station** and several bus routes encompassing the site.

The **bike network** currently runs along Main Street and Quebec street. A future bike route is planned to run along Station Street, National Avenue and Terminal Avenue.

Arterial streets—Great Northern Way, Main Street, Terminal Avenue, and Clark Drive—enclose the Flats and serve as high-traffic routes for passenger and commercial vehicles, facilitating the movement of goods and people throughout the city.

The **Pacific Central Station** is Vancouver's railway station which acts as Canada's western terminus of Via Rail's cross country train service to Toronto and the northern terminus of Amtrak's Cascades service to Seattle and Portland.

The False Creek Flats policy highlights the necessity for significant densification of residential and employment spaces with the greatest densities achieved near the SkyTrain stations. This provides a strong framework for this proposal as a densified hub for retail, commercial and residential space.



LEGEND

- ACTIVE TRANSPORTATION
- - - BIKE NETWORK EXPANSION
- SKYTRAIN - EXPOLINE
- BUS ROUTES

SITE CONTEXT

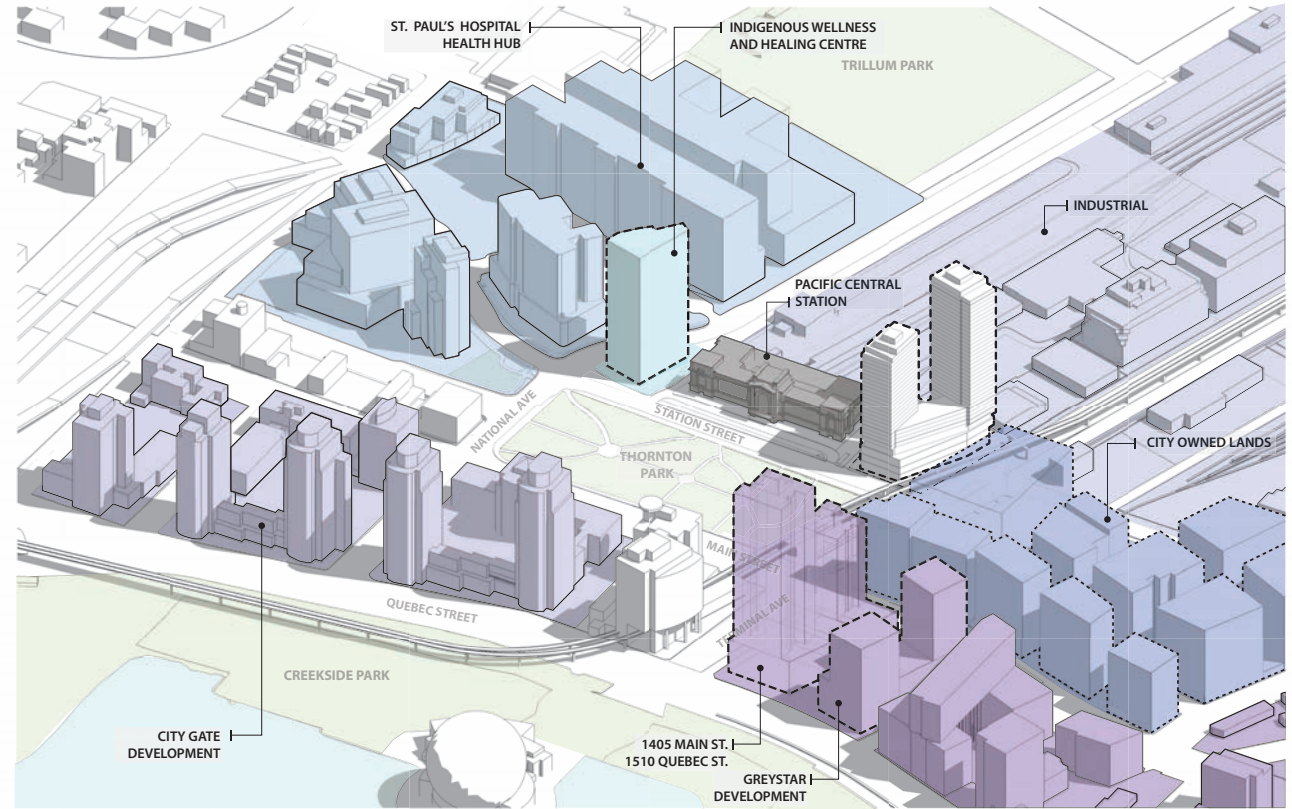
Directly North of the site, is **Pacific Central Station**, which overlooks Thornton Park. The station serves as a prominent historical landmark in Vancouver and was designated a heritage railway station in 1991.

Framing the station to the North will be the **Indigenous Wellness and Healing Centre** and **St. Paul's Hospital Health Hub**, scheduled to be completed in 2027.

Overlooking Thornton Park to the West, **City Gate development** comprises multiple mixed-use residential towers, also built up to the Queen Elizabeth View Cone.

To the South of the site and the SkyTrain guideway are the **City-owned lands** and to the East, the **industrial** warehouses and offices.

The site is located at the intersection of the "**Health Hub**", the Industrial lands to the east, and mixed-use residential developments. The proposal includes commercial, retail, and rental residential spaces, serving as a central hub that connects the diverse uses in the surrounding area.



LEGEND

- HEALTH HUB
- CITY GATE DEVELOPMENT
- CITY OWNED
- SOUTH EAST FALSE CREEK
- INDUSTRIAL
- INDIGENOUS WELLNESS & HEALING CENTER
- FUTURE DEVELOPMENT

SITE PHOTOGRAPHS AND VIEWS



The existing area is a transitional zone, which is consistent with the site's FC-2 zoning that seeks to make the logical shift from the high intensity, mixed-use/residential character of False Creek to the industrial focus of the lands further into the Flats.

The project site is adjacent to the Pacific Central Station and across from Thornton Park and the Main Street-Science World SkyTrain station. The neighbouring buildings to the east are tenanted by industrial manufacturing companies and commercial office uses along Terminal Avenue. This development would be an identifiable marker for the Flats.



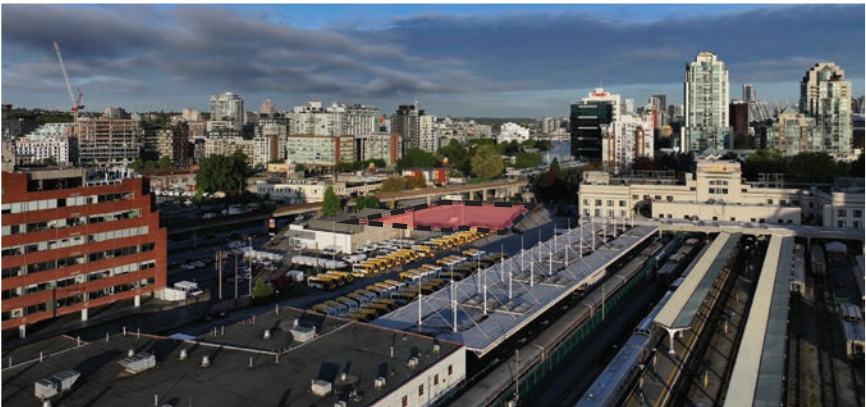
The Flats, an industrial district of Vancouver, is poised to become the new neighbourhood mixing existing industrial business, with commercial and community amenities. The Flats plan allows rental residential to be considered in the western and southern Flats, and the recent changes by City Council to implement provincial TOA regulations and increase height through View Corridor modernization anticipates this new neighbourhood. This district will densify with families, medical and technology employment with the continued residential development in South False Creek, Emily Carr University at Great Northern Way Campus and the planning of the future home of St. Paul's Hospital north of the Pacific Central Station.

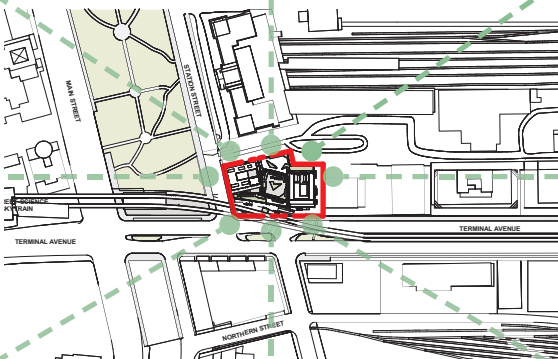


With the Main Street-Science World SkyTrain station directly across from this site, this development would become a transit-based anchor hub, further activating Thornton Park and the West False Creek Flats emerging neighbourhood.

1. Existing Industrial buildings along Terminal Avenue
2. Intersection at Terminal Avenue & Station Street
3. Station Street looking north.

SITE PHOTOGRAPHS AND VIEWS



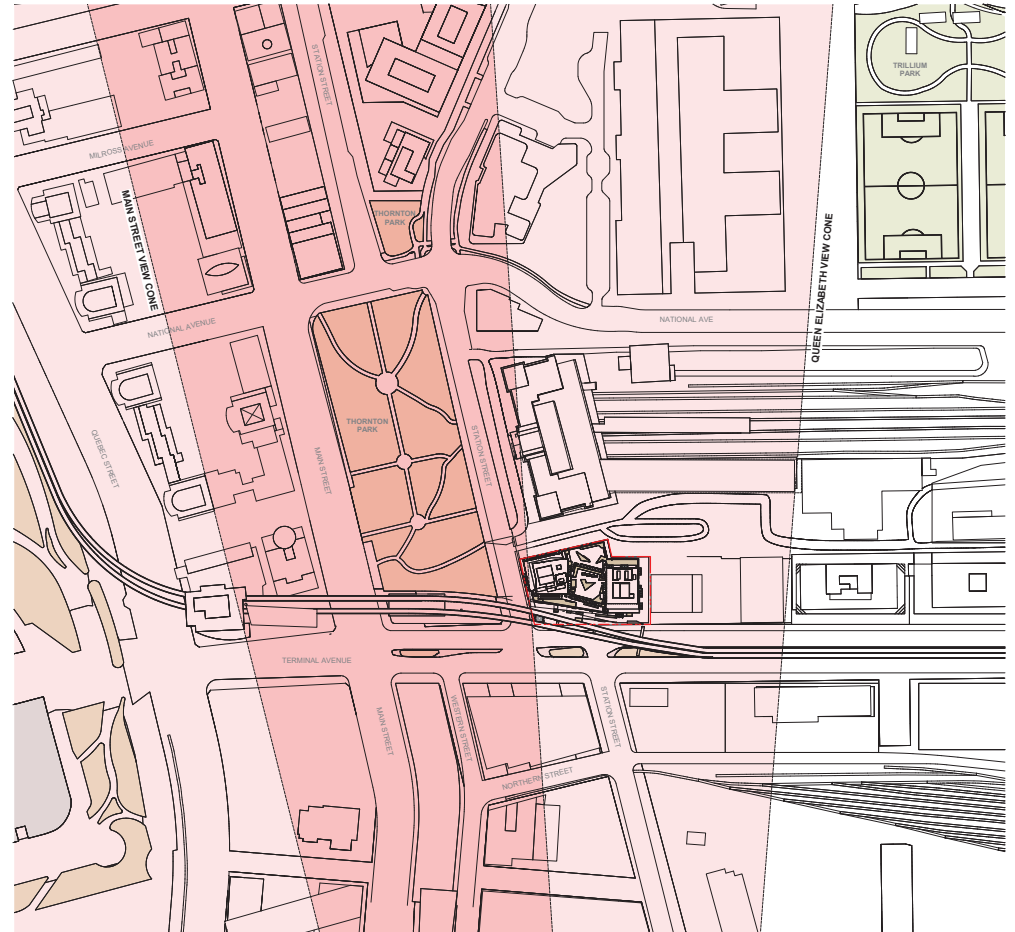
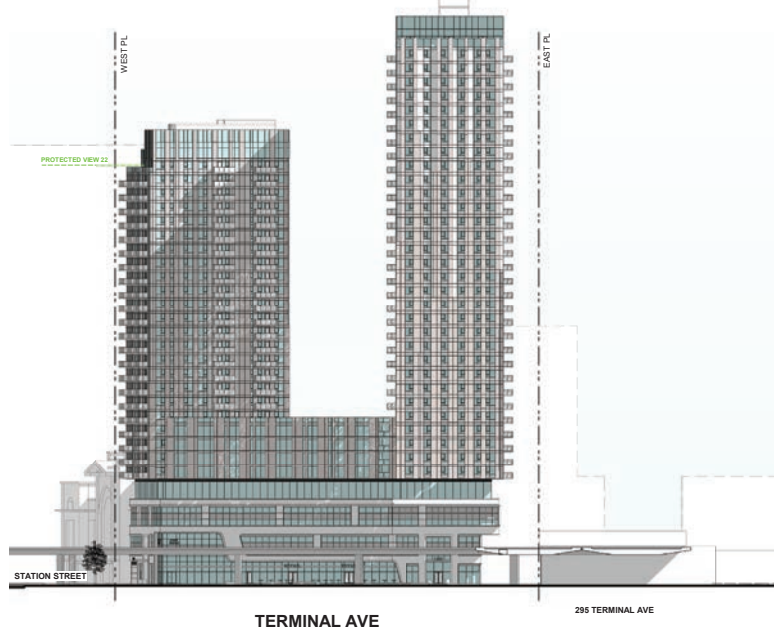




VIEW CONES AFFECTING THE SITE

The site is under the Queen Elizabeth View Cone 3.2.4, which currently caps the site at an elevation of 115 m. Site 3.2.4 is under review through the revisions to the Downtown Eastside Plan and is proposed to be raised to the same height as the adjacent 3.2.3 view across the downtown. The project is designed to adhere to the revised view height in anticipation of Council's decision on the DTES Plan revisions (late 2025). Since June 2024, the Main Street View Cone 22 has been reduced in width and affects only the West corner of the West tower, capping the site at 84.26m. The FC-2 zoning maximum building height limit on the site is 51.8 m which is below the view cone height limit.

The proposal respects building height limit on the site from the Council-approved protected views, but seeks to rezone above the height limitation of the existing FC-2 zoning.



VIEW CONES AFFECTING THE SITE

The graphics on this page are an excerpt from the current consultation on changes to protected views and building heights as part of the Downtown Eastside Plan update. The project site is under the Queen Elizabeth View Cone 3.2.4, which currently caps the site at an approximate elevation of 115 m. Site 3.2.4 is under review through the revisions to the DTES Plan and is proposed to be raised to the same height as the adjacent 3.2.3 view across the downtown. The project is designed to adhere to the revised view height in anticipation of Council's decision on the DTES Plan revisions (late 2025).

WHAT ARE PROTECTED PUBLIC VIEWS?

Since 1989, Vancouver has protected certain views in the city's downtown and central neighborhoods. These protected views help balance economic growth and housing needs with preserving important views for the future.

Although some views have shifted over time, protecting views from public spaces remains a key part of the city's planning.

WHAT CHANGES ARE WE PROPOSING?

The three views being proposed for change are:

Queen Elizabeth Park (View 3.2.4)

Olympic Plaza Stage (View H)

Creekside Park (View J2)

Two views in the Downtown Eastside Oppenheimer District (DEOD) aren't proposed to be changed because altering them would significantly affect the view:

- » Cambie Bridge (View E)
- » Olympic Shipyard Pier (View G2)

There is also one protected view in the Thornton Park sub-area that doesn't need changing since it's already high enough to accommodate the proposed building heights and densities.

WHY ARE WE DOING THIS?

We propose adjusting some protected public views in the DEOD and Thornton Park to balance the need to protect public views to the North Shore Mountains while enabling the delivery of more housing. The changes will enable taller buildings which are necessary to replace Single Room Occupancy (SRO) buildings and provide more social and below-market rental housing.

There are currently five protected public views in the DEOD and Thornton Park that limit building height to around 13 storeys. After reviewing these views, we found that we can raise the height limit for three of these views without significantly affecting the views of the mountains.

PROPOSED VIEW CHANGES

Queen Elizabeth Park (View 3.2.4)

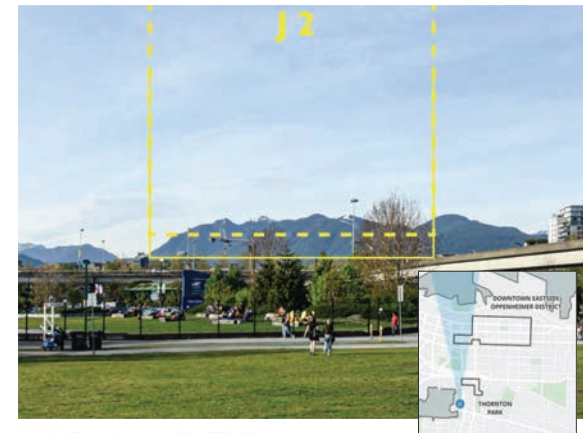


Olympic Plaza Stage (View H)

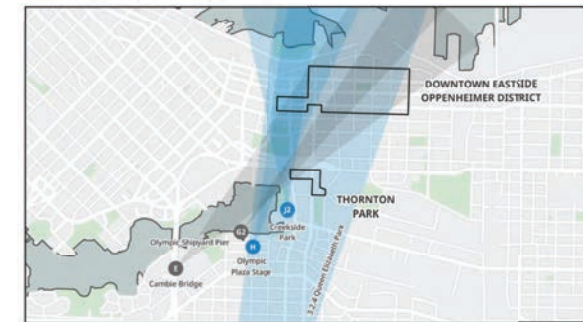


Creekside Park (View J2)

Creekside Park (View J2)



- Current protected public view
- Proposed protected public view



- Protected public view proposed for change
- Protected public view unchanged

Chapter 3: Architectural Proposal

DESIGN PRINCIPLES

Located on the border of the Health Hub and the Creative District, and at the intersection of residential and industrial areas, 1220 Station represents the hub that weaves all these spaces together.

The proposal acts as a hub for the Creative District, offering a blend of employment spaces that honour the area's commercial heritage, such as retail services (including groceries) alongside office and residential components.

Emphasis is placed on enhancing the public realm at level 1, which plays a vital role in linking the site with its surroundings. Pedestrian-oriented retail along Terminal Avenue encourages foot traffic and provides a much needed secure public space. This public plaza will facilitate pedestrian movement and commercial activity, but also offers an opportunity for public art installation.

As a central Hub, the proposal will also provide accommodation for Hospital staff working in the Health Hub nearby and for people working downtown who can use the SkyTrain station or bus stops close-by to commute.

1 | URBAN FIT

Create a sense of place and home in the False Creek Flats' Health Hub District while evoking the industrial heritage of the area.



2 | RELATIONSHIP TO STATION

Located next to the Pacific Central Station, a Heritage building of historical importance, 1220 Station takes cues from its massing and materiality.



3 | SUSTAINABILITY

Incorporate sustainable measures that seek to improve efficiency and moderation in the use of materials, energy, development space and the surrounding ecosystem at large.

4 | VIEWS

The site has the opportunity of providing views to the North Shore mountains and downtown Vancouver while respecting protected public views



5 | BLOCK RESPONSE

Located in the Health Hub close to St. Paul's Hospital, 1220 Station will create office / laboratory space to help support the hospital, create housing for and retail for staff and hospital visitors.



6 | ENHANCE PUBLIC REALM

The plaza on level 1 provides a retail-fronted promenade bordered by greenery that will enhance the public realm facing Terminal Ave. Lighting features and public activities will provide much needed safety at night.



FORM & DENSITY RATIONALE

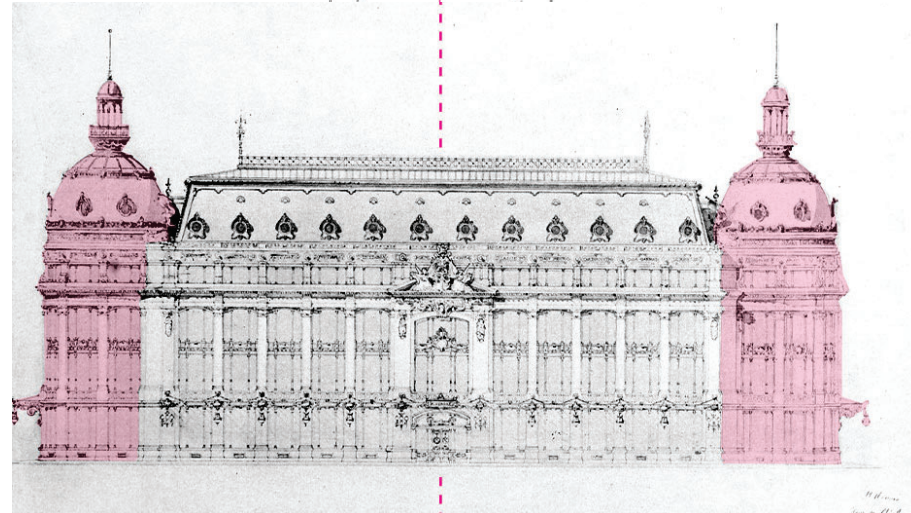
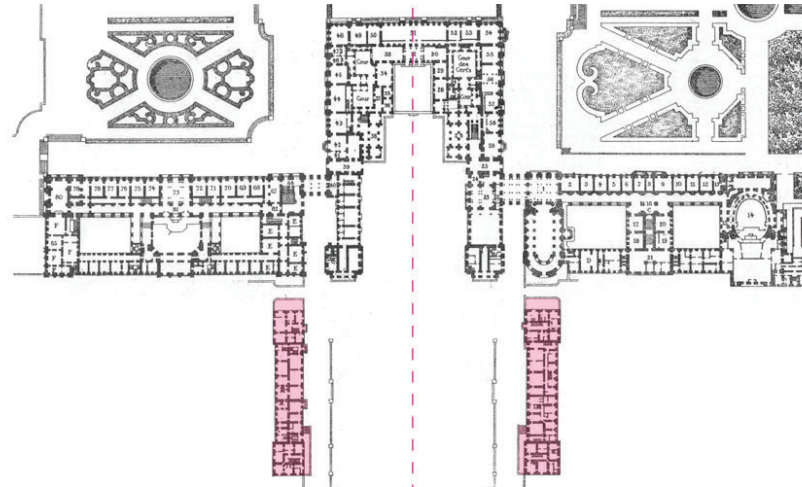
This proposed development, guided by the relevant zoning policies, also takes into consideration the historical urban response to its context.

The formal relationship between the **Pacific Central Station** and **Thornton Park** sets the hierarchy for city edges along the park's perimeter. The symmetry of the building to park relationship evokes a Beaux-arts parti. Like great formal city parks, such as Bryant Park in New York and Erzsébet Square in Budapest, the buildings define the outdoor room.

With the planning of the future St. Paul's Hospital to the north of Pacific Central Station, the form and density of this proposed building seeks to anchor the southeast corner of Thornton Park, by building to the edge of the site.

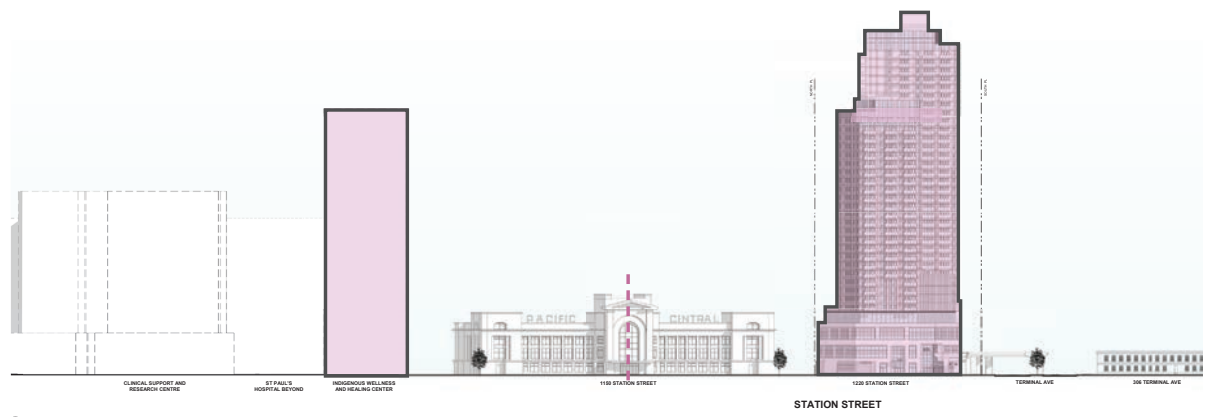


Bryant Park, New York -- urban room defined by building edges

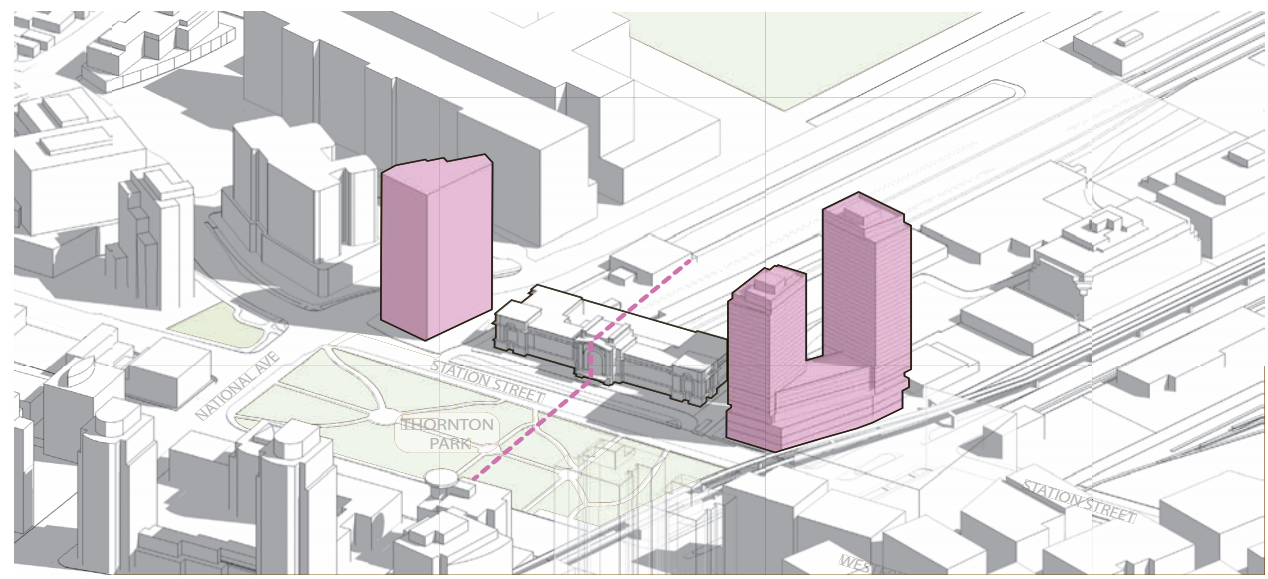


Beaux-Arts design strategies:
Symmetry and Framing

FORM & DENSITY RATIONALE

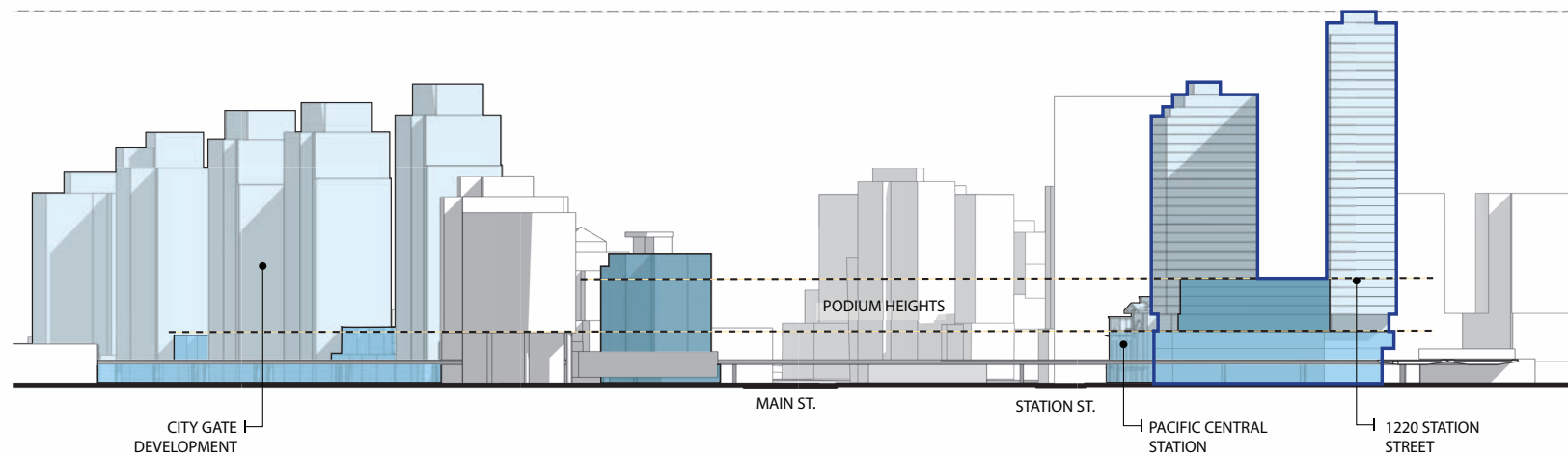


STREETScape ELEVATION - STATION STREET
Scale 1:100



Beaux-Arts logic applied to the site on Urban Planning Scale.

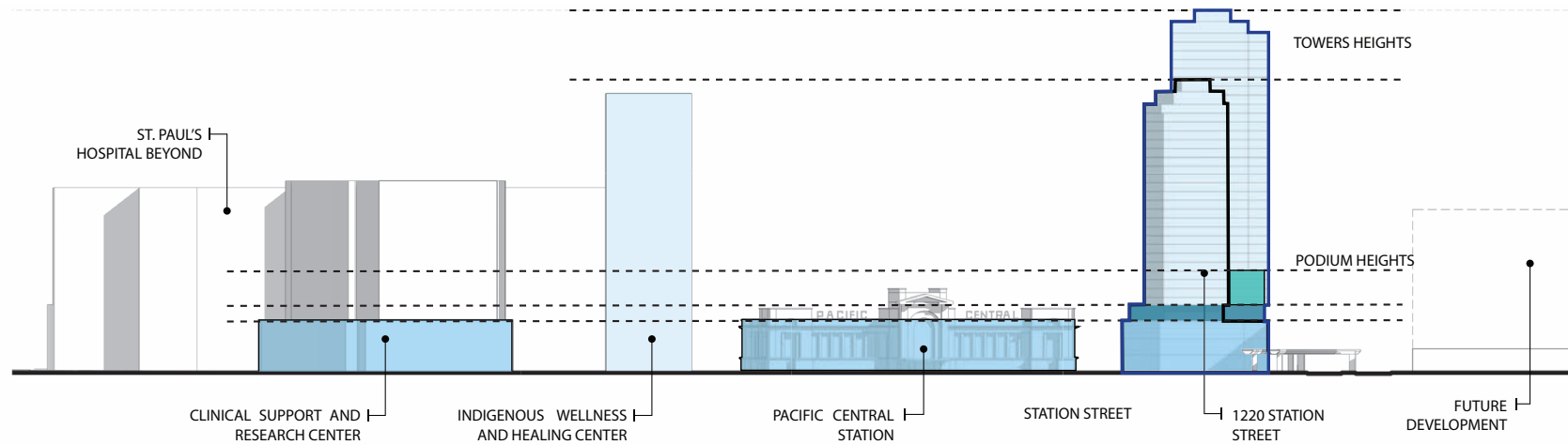
FORM & DENSITY RATIONALE



Cross-section through Terminal Avenue looking North

The form, height and density also respond to the surrounding “family of buildings”, with similar tower heights, podium forms and tower spacing. One significant difference is the intentionally smaller floor plates of the proposal from those of City Gate across from Thornton Park, which is intended produce slender tower forms and reduce any morning shadows on Thornton Park (6,600-6,800 sq ft proposed on-site, versus 6,490 – 12,400 sq ft floor plates at existing City Gate). The proposed development frames the park and mirrors the City Gate Development in its massing form: the tower heights both sit below the Queen Elizabeth View Cone, the podium height matches the City Gate Podium and the residential podium aligns with the Southern podiums.

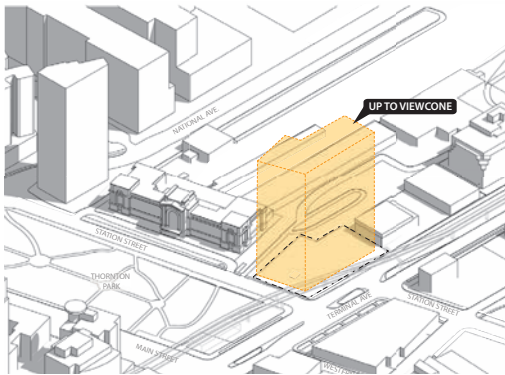
FORM & DENSITY RATIONALE



Cross-section through Station Street looking East

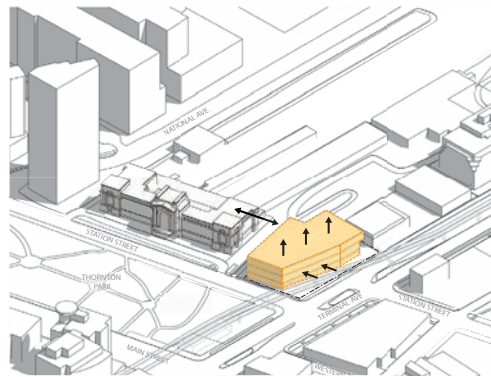
This proposed development, guided by the relevant zoning policies, also takes into consideration the neighbouring developments such as the new St. Paul's Hospital buildings, the Clinical Support and Research Center and the Indigenous Wellness and Healing Center. The proposed tower height matches the height of the Indigenous Wellness and Healing Centre, framing the Pacific Central Station. The proposed podium height matches the height of the Pacific Central Station and the podium of the Clinical Support and Research Centre.

MASSING AND FORM



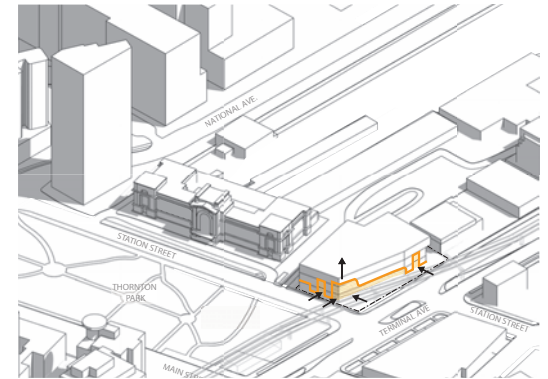
1. Identify setbacks and zoning envelope

The massing fits below the Queen Elizabeth view cone.



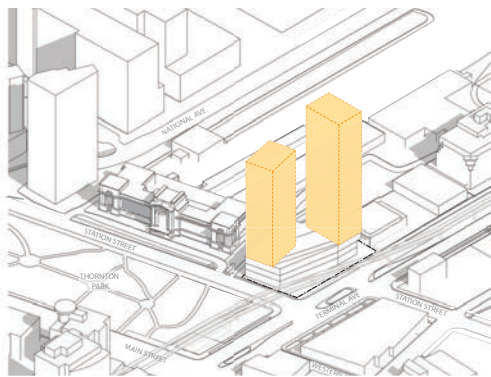
2. Form podium massing and height

A 3-storey podium is proposed, to match the height of Pacific Central Station. The South is adjusted to fit outside the site's SRWs, the 10m SkyTrain dripline and the East Easement Plan. Respecting this easement also helps to facilitate the transition to the industrial properties to the east.



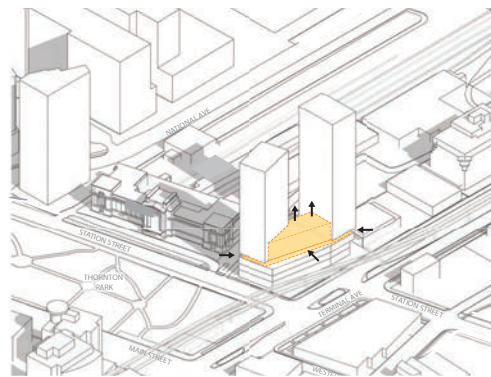
3. Breaking up massing to emphasize entrances

The two residential entrances are carved out along Station St and Terminal Ave, and the entry to the grocery store is emphasized through the raised massing and glazed corner, forming a continuous ribbon around the building.



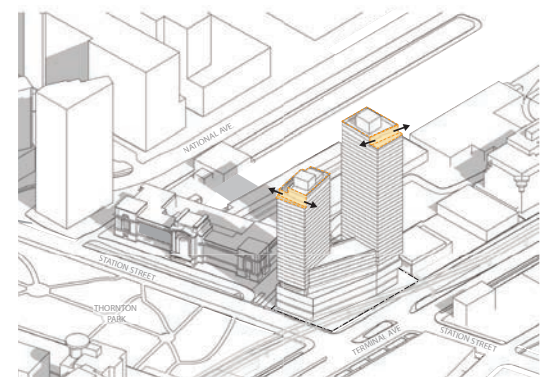
4. Two residential towers

Two slender-profiled residential towers are created instead of one, to break up the massing and reduce the shadows over the park.



5. Residential podium and towers

The residential podium is added between the two towers, both elements are raised to create a recessed level which makes the appearance of floating towers and differentiates residential and commercial uses.



6. Expressive tower articulation

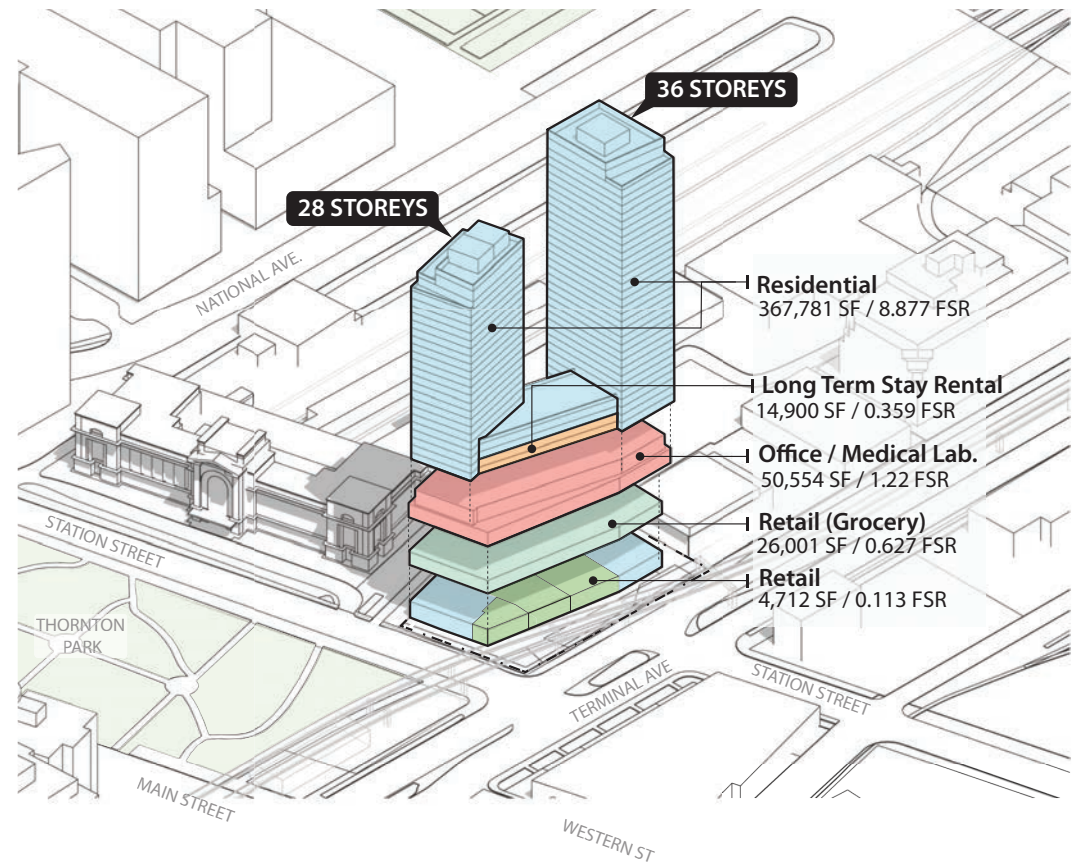
Top of towers are sculpted to slice and break the rectangular shape. West tower is sliced parallel to the view cone outline, East tower sliced parallel to skytrain drip outline.

LAND USE, DENSITY, HEIGHT

The False Creek Flats Plan envisions commercial activity and office uses in this area. For this area of the FC-2 District Schedule, the zoning seeks a minimum of 1.5 FSR of commercial density before residential uses can be considered. This proposal meets the minimum commercial expectation of the FC-2 zoning and is seeking to add residential use to the FC-2A sub-area.

The proposal includes two 28 & 36 residential towers linked together by a 8 storey podium, offering market and below-market rental homes, complemented by ground-level retail, a grocery on level 2 and office / medical laboratories on level 3 and 4.

The towers are spaced according to guidelines, with a minimum of 80 feet between residential buildings, resulting in slender tower massing when viewed from different vantage points in False Creek.



LEGEND

- RENTAL RESIDENTIAL
- OFFICE / MEDICAL LABORATORY
- RETAIL (GROCERY)
- RETAIL

LAND USE, DENSITY, HEIGHT

This application seeks to rezone 1220 Station Street in alignment with the Provincial Bill 47 and subsequent implementation of the Transit-Oriented Area (TOA) policy and regulations adopted by Vancouver City Council in 2024, with the following land use rationale:

- The site is immediately adjacent to Main Street-Science World Station and qualifies as a Tier 1 site for residential intensification within 200 metres of rapid transit.
- There are no existing buildings or residential tenants on the site, so there will be no tenant displacement.
- Full and sufficient City infrastructure and services are available to the service as a result of the area upgrades from the new St Paul's Hospital construction, and as confirmed by the approved development permit for a 12-storey office building.
- The site has been FC zoned for both residential and commercial mixed use since the zoning was adopted by City Council in 1986. The current FC-2 zoning seeks to enable the development of a high-density residential area adjacent to False Creek that transitions to the industrial/commercial lands to the east in the heart of the False Creek Flats.
- There were multiple enquiries to develop a residential building under the previous FC-1 zoning over the past decades, but City Staff did not support these residential proposals due to the 24-hour, inter-city bus terminal immediately adjacent to the site at Pacific Central. This bus function is no longer present on the site as the intensive inter-city bus services ended with the departure of Greyhound in 2018. This livability rationale for not supporting residential use on the site no longer exists.

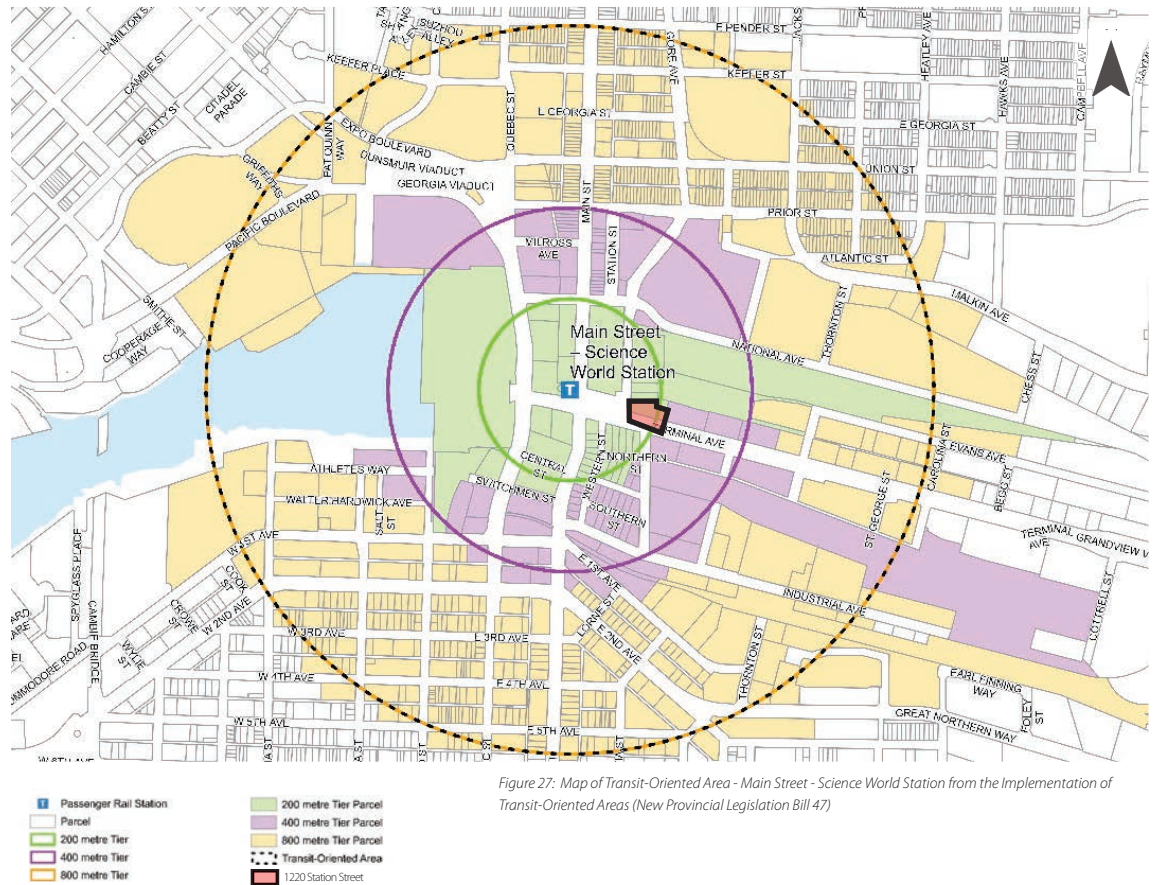


Figure 27: Map of Transit-Oriented Area - Main Street - Science World Station from the Implementation of Transit-Oriented Areas (New Provincial Legislation Bill 47)

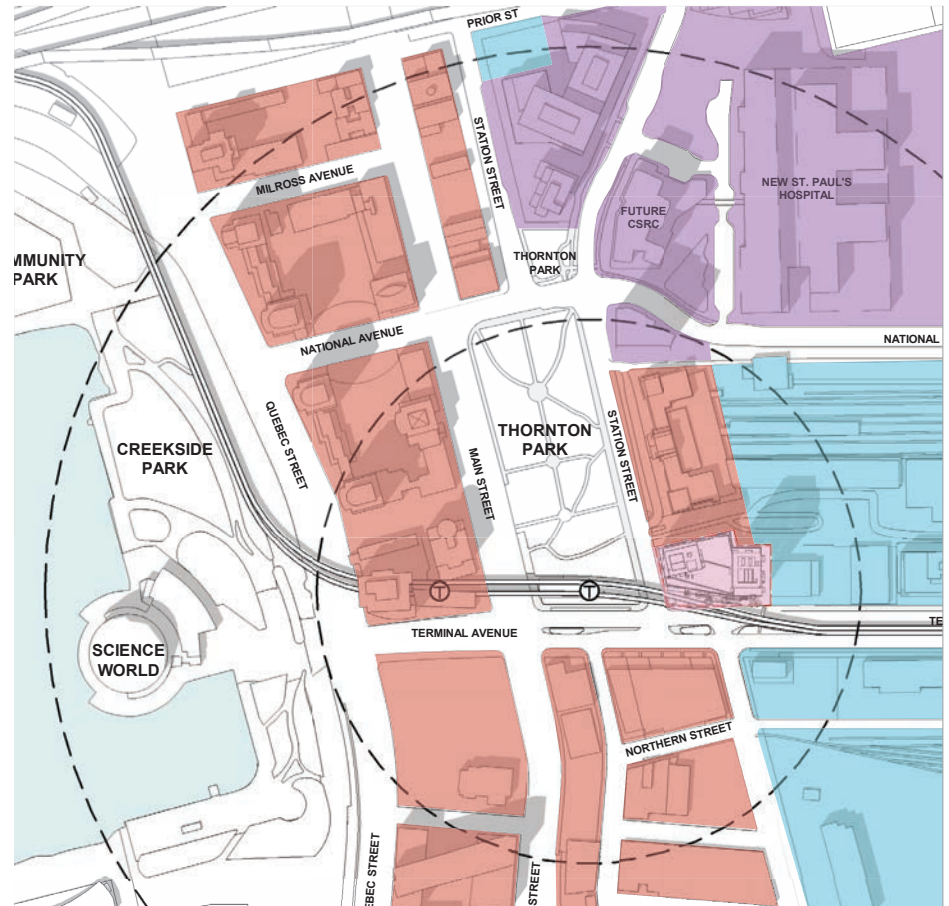
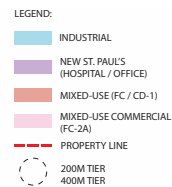
LAND USE, DENSITY, HEIGHT

- Just prior to the closure of the inter-city Bus Terminal in 2018, Staff completed the False Creek Flats Plan (adopted in 2017). As part of the Flats Plan, staff undertook a City-initiated rezoning of the site to FC-2 sub-area A, in order to support the previous landowner's then intention to build a small office building on the site (after multiple, unsuccessful requests for residential use).
- The previous owner received a development permit for an office building on the site, which is no longer viable due to market conditions for office space, and the enormous accessory office capacity (approximately 750,000 sq ft) approved in 2021 to support the financial viability of new St Paul's Hospital. Based on historic absorption rates, the significant increased office capacity created as part of the new St Paul's Hospital more than satisfies over 30 years of office demand for this area of the False Creek Flats.
- Finally, the proposed rezoning provides over 400 new rental homes (with 20% offered at below market rents), and significant commercial space that directly support the new St Paul's Hospital by providing desperately needed food and retail services to the area, and providing rental housing for hospital staff and emergency responders in direct proximity to the hospital function. The need for medical and emergency responders' housing near the new hospital was a critical discussion in considering the

new St Paul's Hospital in the Flats, especially in the event of a seismic event or other regional emergency.

Although the application seeks to rezone the FC-2A to re-instate residential use and requires a minor, consequential amendment to the Flats Plan, the proposed development is consistent with the area zoning and the overall intent of the Flats Plan.

The existing Council-approved False Creek Flats Plan, policy and zoning (FC-1 and FC-2) calls for the gradual transition in this area of the intense mixed-use, urban character of the False Creek Basin to the industrial/employment districts of the False Creek Flats. The Flats Plan reaffirms this land use transition and seeks additional height and density on the eastern side of Main Street to intensify the rental housing, public health and economic objectives of the Creative Campus and Health Hub Districts, including the new St Paul's Hospital.

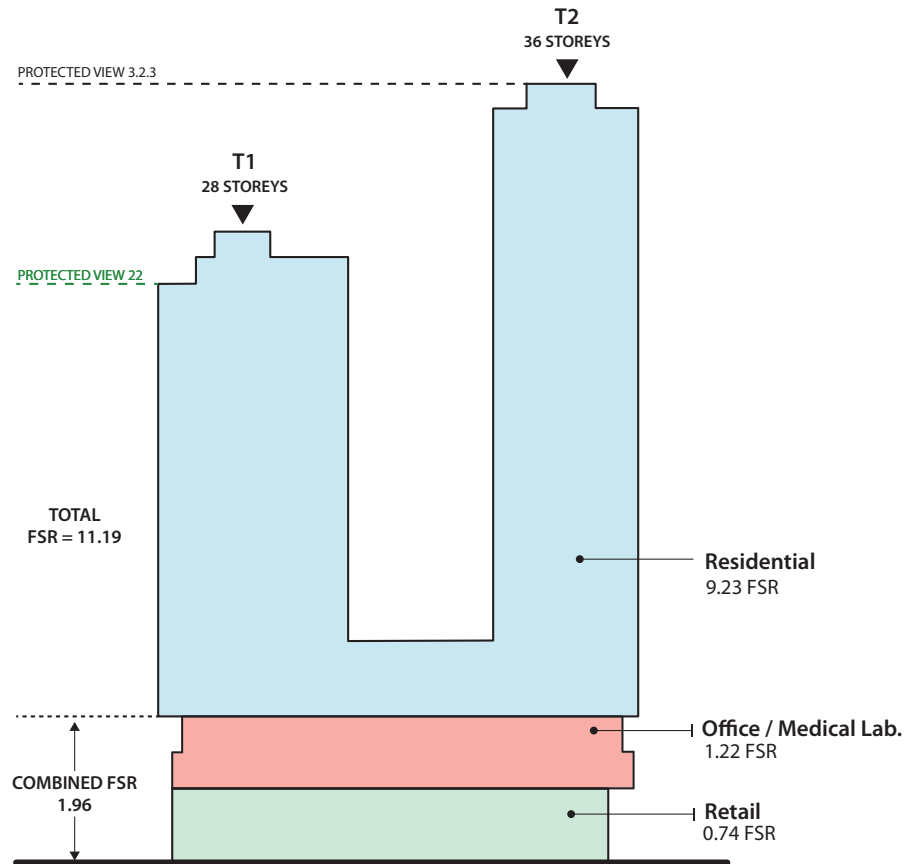
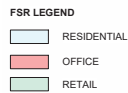


LAND USE, DENSITY, HEIGHT

This proposal for 1220 Station Street supports this land use transition envisioned in the Flats Plan and the current FC zoning. The application includes the minimum 1.5 FSR of commercial uses anticipated in the FC zoning, and elevates the residential uses well-above the noise and activity of the SkyTrain through an extended commercial podium to achieve enhanced livability.

The proposal also maintains an easement on the east of the site to contribute to the land use separation of the neighbouring industrial lands, and provide extremely important vehicular access to these lands to support their industrial function.

When the non-developable lands (lands for the easement to support the adjacent industrial and lands under the Skytrain guideway) are removed from site calculations the commercial density increases to 3.30 FSR on the net buildable area.

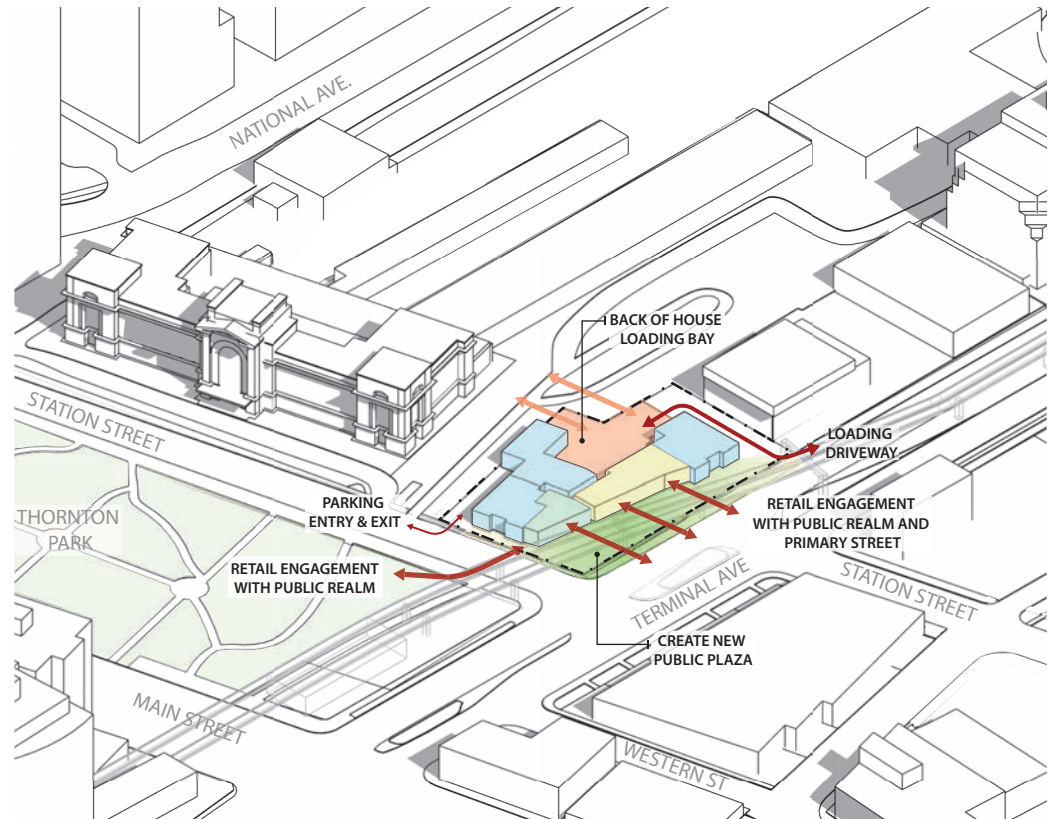


SITE DESIGN CONCEPT

Located between **the Health Hub and the Creative District**, 1220 Station Street responds to the multiple uses present. The podium of the project consists of retail, grocery, office and residential uses.

The entry to the grocery store is located at the corner of Station Street and Terminal Avenue, to activate the public realm. The residential entrances are clearly identifiable as they are located at the base of each tower. The back-of-house loading bay is located along the North side of the site and is accessed via the loading driveway from Terminal Avenue.

The proposal creates a new public plaza at the corner of Station Street and Terminal Avenue, partially covered by the SkyTrain guideway. The retail front facing Terminal Avenue creates an active edge for pedestrians as well as for staff working in the **St. Paul's Hospital and Health Hub**.



LEGEND

- RENTAL RESIDENTIAL
- BACK OF HOUSE / LOADING BAY
- OFFICE / MEDICAL LABORATORY
- RETAIL (GROCERY)

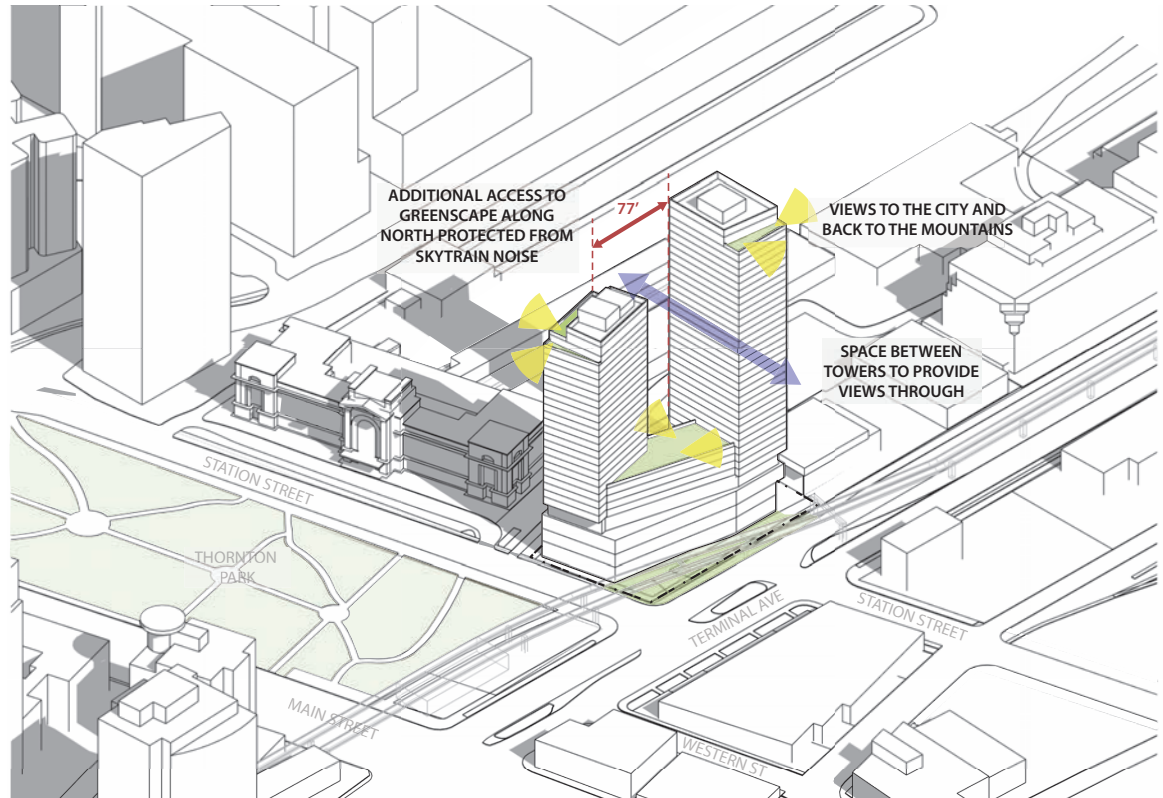
ACCESS TO OPEN SPACE AND VIEWS

Additional access to green space has been included within the proposal. Along the South of the site at grade, creating a new public plaza and activating the pedestrian activity, while mirroring the access to green space across the street from Thornton Park.

The project created additional green space on level 4, along the South and North of the residential podium. The outdoor amenity on the North side of the residential podium is protected from SkyTrain noise.

An outdoor amenity space has been created on the roof of the residential podium, providing views to the North shore mountains and the City. Additional residential amenity decks are located on the rooftops of each tower, providing outdoor South- and West-facing views and North-facing indoor amenity rooms, providing mountain views.

The towers are spaced according to guidelines, with a minimum of 80 feet between residential buildings, resulting in slender tower massing when viewed from different vantage points in False Creek.



PROJECT STATISTICS

RESIDENTIAL UNIT SUMMARY

UNIT MIX TOWER T1 & T2						
Level	STUDIO	1 BR	2 BR	3 BR	4 BR	TOTAL (PER FLOOR)

PODIUM						
LEVEL 5	10	0	0	0	0	10
LEVEL 6	11	0	0	0	0	11
LEVEL 7	11	0	0	0	0	11
LEVEL 8	11	0	0	0	0	11
TOTAL	43 (100%)	0	0	0	0	43

T1						
LEVEL 5	0	0	0	3	1	4
LEVEL 6	0	0	1	2	1	4
LEVEL 7	0	0	1	2	1	4
LEVEL 8	0	0	1	2	1	4
LEVEL 9	1	3	2	1	0	7
LEVEL 10	2	3	3	0	0	8
LEVEL 11	2	3	3	0	0	8
LEVEL 12	2	3	3	0	0	8
LEVEL 13	2	3	3	0	0	8
LEVEL 14	2	3	3	0	0	8
LEVEL 15	2	3	3	0	0	8
LEVEL 16	2	3	3	0	0	8
LEVEL 17	2	3	3	0	0	8
LEVEL 18	2	3	3	0	0	8
LEVEL 19	2	3	3	0	0	8
LEVEL 20	2	3	3	0	0	8
LEVEL 21	2	3	3	0	0	8
LEVEL 22	2	3	3	0	0	8
LEVEL 23	2	3	3	0	0	8
LEVEL 24	2	3	3	0	0	8
LEVEL 25	2	3	3	0	0	8
LEVEL 26	2	3	3	0	0	8
LEVEL 27	1	1	1	1	0	6
TOTAL	36 (21.8%)	56 (33.3%)	59 (35.8%)	11 (6.7%)	4 (2.4%)	165

PODIUM: 0% FAMILY FRIENDLY UNITS

TOWER T1: 44.8% FAMILY FRIENDLY UNITS

T2						
LEVEL 5	1	0	0	2	2	5
LEVEL 6	0	1	0	3	1	5
LEVEL 7	0	1	0	3	1	5
LEVEL 8	0	1	0	3	1	5
LEVEL 9	2	3	2	1	0	8
LEVEL 10	3	3	3	0	0	9
LEVEL 11	3	3	3	0	0	9
LEVEL 12	3	3	3	0	0	9
LEVEL 13	3	3	3	0	0	9
LEVEL 14	3	3	3	0	0	9
LEVEL 15	3	3	3	0	0	9
LEVEL 16	3	3	3	0	0	9
LEVEL 17	3	3	3	0	0	9
LEVEL 18	3	3	3	0	0	9
LEVEL 19	3	3	3	0	0	9
LEVEL 20	3	3	3	0	0	9
LEVEL 21	3	3	3	0	0	9
LEVEL 22	3	3	3	0	0	9
LEVEL 23	3	3	3	0	0	9
LEVEL 24	3	3	3	0	0	9
LEVEL 25	3	3	3	0	0	9
LEVEL 26	3	3	3	0	0	9
LEVEL 27	1	5	3	0	0	9
LEVEL 28	1	5	3	0	0	9
LEVEL 29	1	5	3	0	0	9
LEVEL 30	1	5	3	0	0	9
LEVEL 31	1	5	3	0	0	9
LEVEL 32	1	5	3	0	0	9
LEVEL 33	1	5	3	0	0	9
LEVEL 34	1	5	3	0	0	9
LEVEL 35	1	5	3	0	0	9
TOTAL	63 (24.0%)	102 (38.9%)	81 (30.5%)	12 (4.6%)	5 (1.9%)	262
TOTAL	142 (30.2%)	157 (33.4%)	139 (29.6%)	23 (4.9%)	9 (1.9%)	470

TOWER T2: 37% FAMILY FRIENDLY UNITS
PODIUM & TOWER T1 & T2:
36.4% FAMILY FRIENDLY UNITS

UNIT AREA SCHEDULE			
UNIT <= 65 m²	65 m² < UNIT < 105 m²	UNIT >= 105 SM	Count
344	117	9	470

UNIT TYPE MIX (NET AREA)			
UNIT TYPE	Area	Count	
UNIT A1	1BR	505 SF	17
UNIT A2	1BR	489 SF	30
UNIT A3	1BR	503 SF	27
UNIT A4	1BR	462 SF	9
UNIT A5	1BR	477 SF	9
UNIT A6	1BR	457 SF	10
UNIT A7	1BR	485 SF	18
UNIT A8	1BR	478 SF	18
UNIT A9	1BR	552 SF	19
UNIT B1	2BR	689 SF	27
UNIT B2	2BR	991 SF	27
UNIT B3	2BR	772 SF	17
UNIT B4	2BR	732 SF	9
UNIT B5	2BR	722 SF	19
UNIT B6	2BR	696 SF	18
UNIT B7	2BR	724 SF	17
UNIT B9	2BR	808 SF	1
UNIT B10	2BR	852 SF	3
UNIT C1	3BR	1,196 SF	1
UNIT C2	3BR	991 SF	1
UNIT C3	3BR	981 SF	4
UNIT C4	3BR	996 SF	3
UNIT C5	3BR	1,140 SF	4
UNIT C6	3BR	1,036 SF	4
UNIT C7	3BR	1,024 SF	4
UNIT C8	3BR	1,096 SF	1
UNIT C9	3BR	1,038 SF	1
UNIT C10	2BR	745 SF	1
UNIT D1	4BR	1,095 SF	4
UNIT D2	4BR	1,164 SF	4
UNIT D3	4BR	1,075 SF	1
UNIT S1	STUDIO	411 SF	18
UNIT S2	STUDIO	484 SF	17
UNIT S3	STUDIO	390 SF	27
UNIT S4	STUDIO	404 SF	18
UNIT S5	STUDIO	392 SF	18
UNIT S6	STUDIO	426 SF	34
UNIT S7	STUDIO	477 SF	4
UNIT S8	STUDIO	594 SF	4
UNIT S9	STUDIO	455 SF	1
UNIT S10	STUDIO	426 SF	1
TOTAL		470	

BELOW MARKET RENTAL COMMON AREA CALCULATIONS (MIN. 20% COMMON RES. AREA)		
ft²	m²	
16,400 SF	1,524	
TOTAL BELOW MARKET RENTAL CALCULATIONS (MIN. 20% OF TOTAL RES AREA + 20% OF COMMON AREA)		
ft²	m²	
93,482 SF	8,686 m²	

BELOW MARKET RENTAL UNIT MIX						
Level	STUDIO	1 BR	2 BR	3 BR	4 BR	TOTAL (PER FLOOR)
LEVEL 5	11	0	0	5	3	19
LEVEL 6	11	1	1	5	2	20
LEVEL 7	11	1	1	5	2	20
LEVEL 8	11	1	1	5	2	20
LEVEL 9	3	6	4	2	0	15
LEVEL 10	5	6	0	0	0	11
TOTAL	52	15	13	22	9	111
	(40.4%)	(15.2%)	(13.1%)	(22.2%)	(9.1%)	

FAMILY FRIENDLY BMR UNITS: 44.4%

ADAPTABLE UNITS						
Level	STUDIO	1 BR	2 BR	3 BR	4 BR	TOTAL (PER FLOOR)

PODIUM						
LEVEL 5	1	0	0	0	0	1
T1						
LEVEL 9	1	1	0	0	0	2
LEVEL 10	1	1	0	0	0	2
LEVEL 11	1	1	0	0	0	2
LEVEL 12	1	1	0	0	0	2
LEVEL 13	1	1	0	0	0	2
LEVEL 14	1	1	0	0	0	2
LEVEL 15	1	1	0	0	0	2
LEVEL 16	1	1	0	0	0	2
LEVEL 17	1	1	0	0	0	2
LEVEL 18	1	1	0	0	0	2
LEVEL 19	1	1	0	0	0	2
LEVEL 20	1	1	0	0	0	2
LEVEL 21	1	1	0	0	0	2
LEVEL 22	1	1	0	0	0	2
LEVEL 23	1	1	0	0	0	2
LEVEL 24	1	1	0	0	0	2
LEVEL 25	1	1	0	0	0	2
LEVEL 26	1	1	0	0	0	2
LEVEL 27	0	1	0	0	0	1
TOTAL	18	18	0	0	0	37
T2						
LEVEL 5	0	0	0	1	0	1
LEVEL 6	0	0	0	1	0	1
LEVEL 7	0	0	0	1	0	1
LEVEL 8	0	0	0	1	0	1
LEVEL 9	0	0	1	1	0	2
LEVEL 10	1	0	1	0	0	2
LEVEL 11	1	0	1	0	0	2
LEVEL 12	1	0	1	0	0	2
LEVEL 13	1	0	1	0	0	2
LEVEL 14	1	0	1	0	0	2
LEVEL 15	1	0	1	0	0	2
LEVEL 16	1	0	1	0	0	2
LEVEL 17	1	0	1	0	0	2
LEVEL 18	1	0	1	0	0	2
LEVEL 19	1	0	1	0	0	2
LEVEL 20	1	0	1	0	0	2
LEVEL 21	1	0	1	0	0	2
LEVEL 22	1	0	1	0	0	2
LEVEL 23	1	0	1	0	0	2
LEVEL 24	1	0	1	0	0	2
LEVEL 25	1	0	1	0	0	2
LEVEL 26	1	0	1	0	0	2
LEVEL 27	0	1	1	0	0	2
LEVEL 28	0	1	1	0	0	2
LEVEL 29	0	1	1	0	0	2
LEVEL 30	0	1	1	0	0	2
LEVEL 31	0	1	1	0	0	2
LEVEL 32	0	1	1	0	0	2
LEVEL 33	0	1	1	0	0	2
LEVEL 34	0	1	1	0	0	2
LEVEL 35	0	1	1	0	0	2
TOTAL	17	9	27	5	0	58
TOTAL	36	28	27	5	0	96

RESIDENTIAL STORAGE CALCULATIONS					
MARKET RENTAL		BELOW MARKET RENTAL		TOTAL	
ft²	m²	ft²	m²	ft²	m²
11,400 SF	1,059 m²	7,023 SF	653 m²	18,424 SF	1,712 m²

MIN. 20% OF STORAGE AREA TO BE BELOW MARKET RENTAL
18,424 SF * 0.2 = 3685 SF

PROJECT STATISTICS

PARKING SUMMARY

REQUIRED CAR PARKING STALLS											
RESIDENTIAL	VISITORS		RETAIL	GROCERY	OFFICE	TOTAL					
0.4 Stalls / unit	0.05 Stalls / total Units		2 Stalls / 1000 SF	2 Stalls / 1000 SF	1 Stall / 100 m ² up to 300 m ² + 1 Stall / add. 50 m ²						
188	24	13	55	51	371						
REG. EV STALLS	100% ->	188	100% ->	24	45% ->	6	45% ->	25	45% ->	41	331

REQUIRED ACCESSIBLE STALLS	
RESIDENTIAL ACCESSIBLE STALLS:	NON-RESIDENTIAL ACCESSIBLE STALLS:
7 UNITS -> 1 STALL 470 - 7 = 463 UNITS x 0.034 -> 16 STALLS	500 m ² -> 1 STALL 7550 m ² / 1000 m ² = 7.55 x 0.4 -> 3 STALLS
TOTAL: (16 REG ACCESS. STALLS / 1 VAN ACCESS. STALL)	TOTAL: 4 STALLS (3 REG ACCESS. STALLS / 1 VAN ACCESS. STALL)

LEVEL	PROVIDED CAR PARKING STALLS																				Total Parking Stalls	LOADING												
	RESIDENTIAL (100% EV STALLS)					VISITORS (100% EV STALLS)					RETAIL & GROCERY					OFFICE						Total Office Stalls	Loading Type A Stalls	Loading Type B Stalls	Loading Type C Stalls	Total Loading Stalls								
	Residential Regular Stalls	Residential Small Stalls	Residential Accessible Stalls (counted double)	Residential Van Accessible Stalls (counted double)	Res. Pass. Cl. A	Residential Total Residential Stalls	Visitor Regular Stalls	Visitor Small Stalls	Visitor Accessible Stalls (counted double)	Total Visitor Stalls	Retail Regular Stalls	Retail Small Stalls	Retail EV Stalls	Retail Accessible Stalls (counted double)	Retail Van Accessible Stalls (counted double)	Retail Pass. Cl. A	Total Retail Stalls	Office Regular Stalls	Office Small Stalls	Office EV Stalls							Office Accessible Stalls							
LEVEL 1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
LEVEL P1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	14	0	0	0	0	0	0	0	0	17	3	0	0	0	0	0	3
LEVEL P2	0	0	0	0	0	0	0	0	0	0	0	1	4	33	2	4	0	44	0	0	0	0	0	0	0	0	50	0	0	0	0	0	0	0
LEVEL P3	0	0	0	0	0	0	0	0	0	0	0	7	1	0	2	0	0	10	17	3	20	0	0	0	0	0	40	0	0	0	0	0	0	0
LEVEL P4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	38	8	0	2	46	54	0	0	0	0	0	0	0	0	0	
LEVEL P5	31	2	4	2	0	39	9	6	4	19	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	58	0	0	0	0	0	0	0	
LEVEL P6	41	8	10	0	0	59	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	59	0	0	0	0	0	0	0	
LEVEL P7	38	9	10	0	0	57	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	57	0	0	0	0	0	0	0	
LEVEL P8	23	6	4	0	0	33	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	33	0	0	0	0	0	0	0	
	133	25	28	2	3	191	17	6	4	27	8	5	46	4	4	1	68	55	9	26	2	92	378	3	6	0	0	0	9					

EACH 2 CLASS A BICYCLE SPACES WILL HAVE AN ELECTRICAL OUTLET

REQUIRED BIKE STALLS - CLASS A				
RESIDENTIAL	RETAIL	OFFICE	TOTAL	
1.5 Stalls for Units < 65 m ²	2.5 Stalls for Units between 65 m ² and 105 m ²	3 Stalls for Units > 105 m ²	Total Residential	1 Stall for each 340 m ²
515	293	27	835	8
				1 Stall for each 170 m ²
				28
				871

REQUIRED BIKE STALLS - CLASS B				
RESIDENTIAL	RETAIL	OFFICE	TOTAL	
2 Stalls for first 20 units + 1 Stall for each add. 20 Unit	6 Stalls for more than 1000 m ²	6 Stalls for more than 2000 m ²		
25	6	6	37	

Level	PROVIDED BIKE STALLS							Total	CLASS B Stalls
	Horizontal Bike Stalls (2' x 6')	Vertical Bike Stalls (2' x 3'-6") max. 30% of Class A Stalls = max. 291 Stalls	Slack Bike Stalls (x2) (2' x 6") max. 60% of Class A Stalls may be vertical and stacked = max. 581 Stalls	Bike Lockers (2' x 6") min. 10% of Class A Stalls = min. 95	Over-size Bike Stalls (3' x 6") min. 5% of req. Class A Stalls = min. 48	Exterior Visitor Bike Stalls			
LEVEL P2	26	9	0	22	0	0	57	21	
LEVEL P3	35	1	62	24	22	0	144	0	
LEVEL P4	37	40	60	11	0	0	148	16	
LEVEL P5	36	38	48	20	9	0	149	0	
LEVEL P6	46	36	54	7	9	0	152	0	
LEVEL P7	44	39	54	7	9	0	153	0	
LEVEL P8	39	30	54	7	0	0	130	0	
	263	191	332	98	49	0	933	37	

RESIDENTIAL STORAGE LOCKERS PROVIDED		
LEVEL	COUNT	
LEVEL 5	22	
LEVEL 6	22	
LEVEL 7	22	
LEVEL 8	22	
	88	

EOT FACILITY		
FIXTURE	REQUIRED	PROVIDED
Grooming Station	3	4
Locker	50	50
Shower	4	4
Wash Basin	3	4
Water Closet	4	4

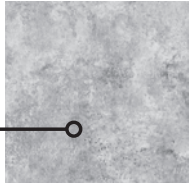
NOTE:
DEDICATED BULK STORAGE IS PROVIDED BELOW GRADE FOR UNITS WITH NO-IN-SUITE STORAGE ROOMS

RESIDENTIAL STORAGE LOCKERS REQUIRED	
REQUIRED	
	20

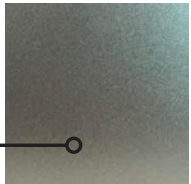
BUILDING MATERIAL & PATTERNING



DARK GREEN METAL PANEL
LOCATION - WINDOW FRAMES, MULLIONS, LINTELS, AND CANOPY STRUCTURE AT LEVEL 1



CONCRETE
LOCATION - RESIDENTIAL AND COMMERCIAL ENTRIES, FASCIA.

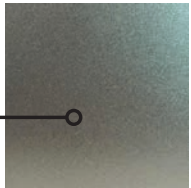


ALUMINIUM SIGNAGE
LOCATION - RESIDENTIAL AND COMMERCIAL ENTRY SIGNAGE



DARK BRICK
LOCATION - THROUGH PODIUM LEVELS 1 TO 3
REFERENCE - OBSIDEAN FROM MUTUAL MATERIALS

BUILDING MATERIAL & PATTERNING



SILVER PANEL
LOCATION - VERTICAL STRIPS
THROUGH TOWERS AND
RESIDENTIAL PODIUM



DARK GREY METAL PANEL
LOCATION - VERTICAL ALIGNMENT
WITH WINDOWS AND OPENINGS



DARK BRICK
LOCATION - THROUGH PODIUM
LEVELS 1 TO 3
REFERENCE - OBSIDEAN FROM
MUTUAL MATERIALS

3D VIEW



View from False Creek looking East

3D VIEW



Cityscape View looking West

3D VIEW



South Elevation

3D VIEW



Southwest View

3D VIEW



Southwest View of Plaza (no SkyTrain for visibility)

3D VIEW



Northwest View

SOLAR ACCESS AND SHADOW ANALYSIS

On July 10, 2024, City Council approved Solar Access Guidelines for the areas of Vancouver, outside of the Downtown. These guidelines acknowledge the need to provide sunlight to parks and open spaces for all Vancouverites to enjoy the public realm at all times of the year.

At that time, City Council also acknowledged the unique urban form of the Downtown Core and that the same guidelines approved for the areas of the City generally south of Broadway do not reflect the complexity in managing growth and solar access in the Downtown and overall Metro Core.

As such, Council approved the following motion: "THAT Council direct staff to initiate a comprehensive review of solar access in public spaces in downtown neighbourhoods and assess the impacts of shadow policies on the delivery of housing and jobs space."

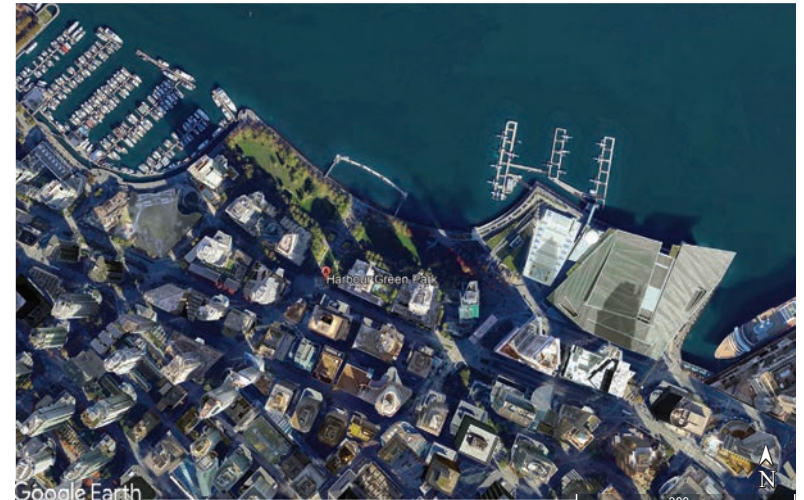
The policy changes to protected views and building heights approved in July 2024 by Council, in addition to the recent TOA-regulations by the Province, provide an excellent opportunity to inform and quantify the solar access analysis to an overall "solar budget" approach to managing shadows. Given these policy changes all converge on 1220 Station Street, this application provides an opportunity to advance the City Council direction to balance City Council's affordable housing objectives and

solar access considerations on this Downtown site.

In some ways, this approach has been used for decades in the Downtown Core where shadow impacts from new development were rarely prohibited on parks and open spaces, but rather were carefully managed through taller, slender towers, careful tower separation, and reasonable attention to the time and intensity of use of the public space.

This current approach has proved effective in achieving housing and employment growth in areas such as Coal Harbour and its relationship to the waterfront seawall and park space to the north of this new neighbourhood. This approach has also been used at City Gate, a neighbourhood immediately adjacent to 1220 Station that manages inevitable shadow impacts on the park and seawall to the west of that new neighbourhood.

However, this approach is difficult to quantify in a meaningful and consistent way. The conventional methodology of using "snap shot" studies at moments in time (10:00/12:00/2:00 on the Solstice and the Equinox) does not fully capture the dynamic use of the public space as people and shadows move constantly throughout the day.



Google Earth view of Harbour Green Park in Coal Harbour, showing the shadow impacts of nearby developments

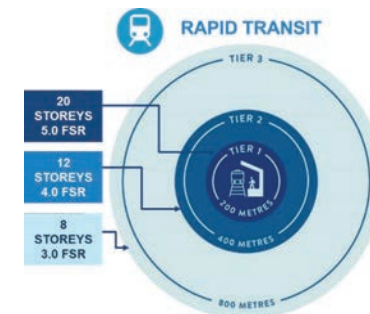
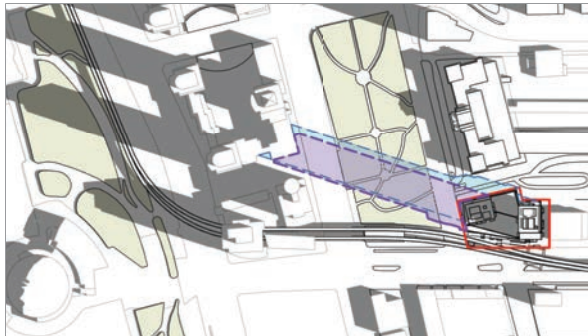
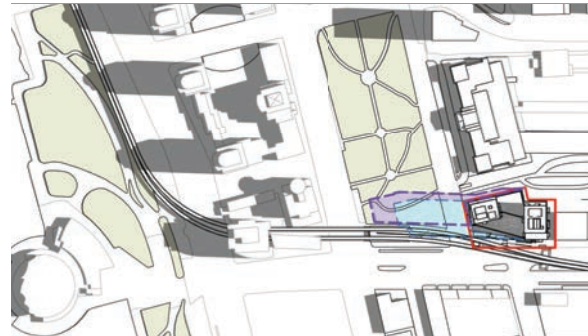


Diagram taken from Implementation of Transit-Oriented Areas (New Provincial Legislation: Bill 47)

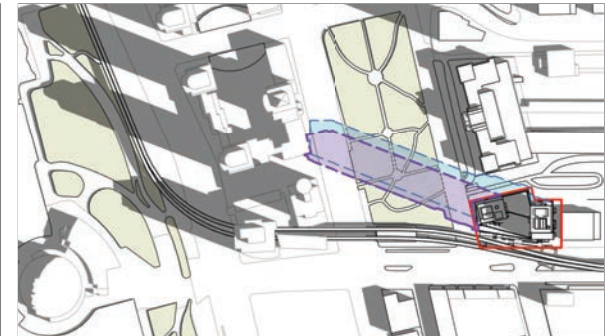
SHADOW STUDIES - 10AM



SPRING EQUINOX - MARCH 21 10 AM - SHADOW
SCALE: 1:1000



SOLSTICE - JUNE 21 10 AM - SHADOW
SCALE: 1:1000



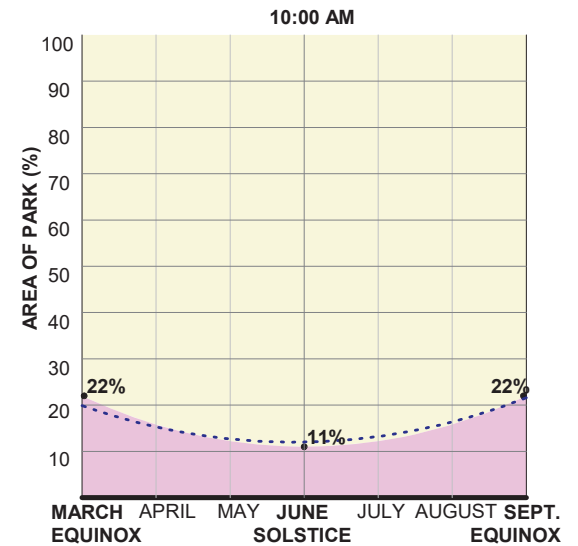
FALL EQUINOX - SEPTEMBER 22 10 AM - SHADOW
SCALE: 1:1000

- PROPOSED BUILDING (28 STOREYS RESIDENTIAL)
- PROPOSED BUILDING (36 STOREYS RESIDENTIAL)
- ORIGINAL OFFICE BUILDING

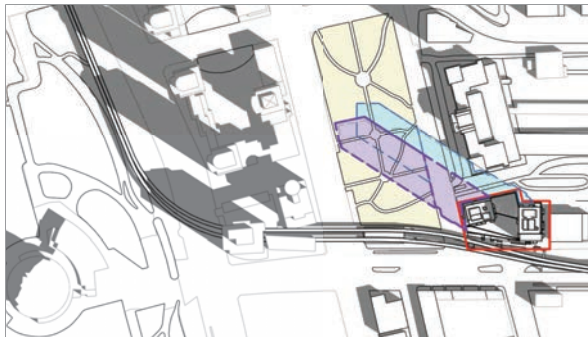
As such, many cities around the world have chosen to pursue Solar Budget Analysis as a way to quantify the full potential opportunity of a person to access sunshine in a park or open space during any point in the day. In effect, a solar budget speaks to the probability that someone will find a sunny space in a park or open space at any given time of day or year. It is in effect a useful measure of the degree of solar access, and the impact of surrounding development.

The following solar access and shadow analysis seeks to provide additional quantitative and qualitative rigour to analyzing solar access opportunities in Thornton Park after the development of 1220 Station in the following manner:

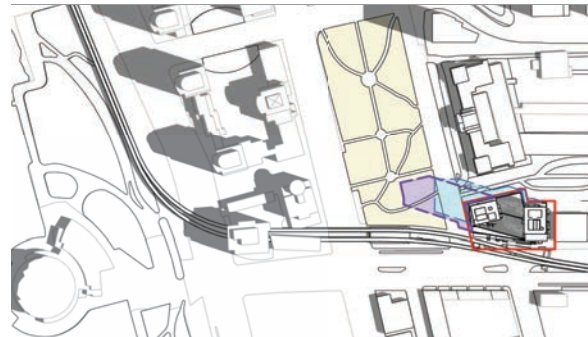
- The analysis provides three build form reference points to show the incremental shadow impact on adjacent Thornton Park for three development forms supported by Council policy and direction including:
 - o The existing development permit approval for a 13-storey office building
 - o The potential 20-storey office building that may be considered as a Tier 1 site within the Province/City regulations for Transit-Oriented Areas within 200 m of Main Street/Science World SkyTrain Station
 - o The proposed two tower, 28 & 36 storey rental development that is the subject of this rezoning application.



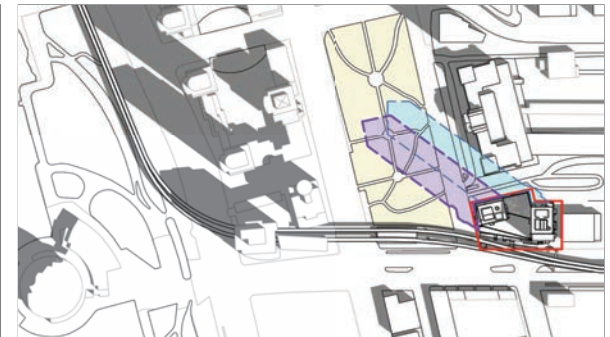
SHADOW STUDIES - 11 AM



○ SPRING - EQUINOX - MARCH 21 11 AM - SHADOW
SCALE: 1"=100'



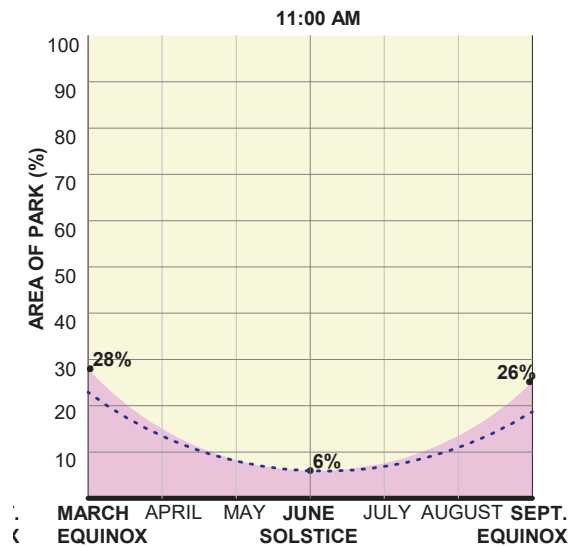
○ SOLSTICE - JUNE 21 11 AM - SHADOW
SCALE: 1"=100'



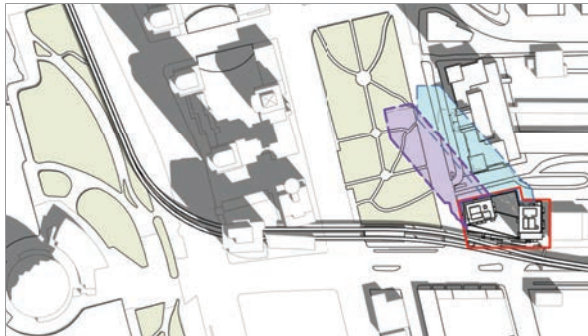
○ FALL EQUINOX - SEPTEMBER 22 11 AM - SHADOW
SCALE: 1"=100'

- PROPOSED BUILDING (36 STOREYS RESIDENTIAL)
- PROPOSED BUILDING (38 STOREYS RESIDENTIAL)
- ORIGINAL OFFICE BUILDING

- 1220 Station Street has been designed with taller, slender residential floorplates of approximately 6,600/6,800 sq ft to minimize shadows cast, in contrast to the 6,490 SF - 12,400 SF (603 m² - 1,152 m²) floorplates of adjacent City Gate.
- The Solar Budget Analysis shows all three development schemes during the morning hours where there is some shadow impact on adjacent Thornton Park at 12:00 noon, 11:00am and 10:00am at all times of year, and the resulting proportion of the overall park and public space that experiences reduced solar access.



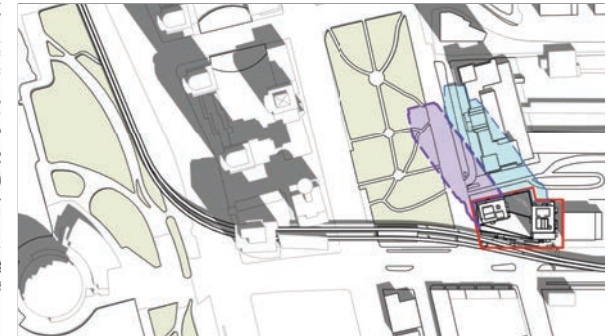
SHADOW STUDIES - 12PM



○ SPRING EQUINOX - MARCH 21 12 PM - SHADOW
SCALE: 1:1500



○ SOLSTICE - JUNE 21 12 PM - SHADOW
SCALE: 1:1500



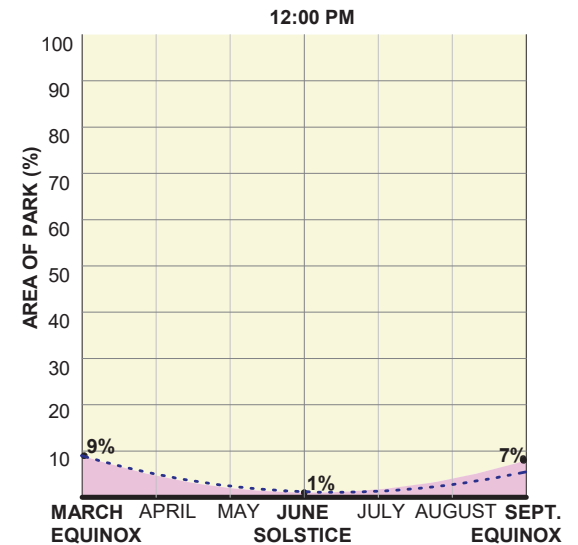
○ FALL EQUINOX - SEPTEMBER 22 12 PM - SHADOW
SCALE: 1:1500

- PROPOSED BUILDING (28 STOREYS RESIDENTIAL)
- PROPOSED BUILDING (36 STOREYS RESIDENTIAL)
- ORIGINAL OFFICE BUILDING

The results of the Solar Budget Analysis provide some key findings for the proposed development and its impact on solar access in the surrounding public space:

- All three development schemes have some morning shadow impact given the location of Thornton park to the west of the site, including the approved development permit for an office building.
- The shadow impact of the proposed rezoning to a two-tower rental scheme provides only marginal additional shadow impact from a TOA-supported office development.
- The maximum impact on the Solar Access Budget of the proposed rental towers is 25% shadow impact (at 11:00am on the Vernal Equinox) meaning that more than 75% of the park remains free of shadow impact at the moment of maximum shadow.

- Any solar access and shadow impact of the proposed rental towers is clear from the park by lunchtime, which is advantageous for adjacent hospital employees and visitors.
- The approximately 5,320 SF / 494 m² public plaza proposed as part of the development creates additional public space to augment the overall solar budget, resulting in the project having a maximum solar impact of 20% during the time of peak shadow impact (at 10:00am on the Vernal Equinox), meaning 80% of the Thornton Park and new plaza is without shadow.

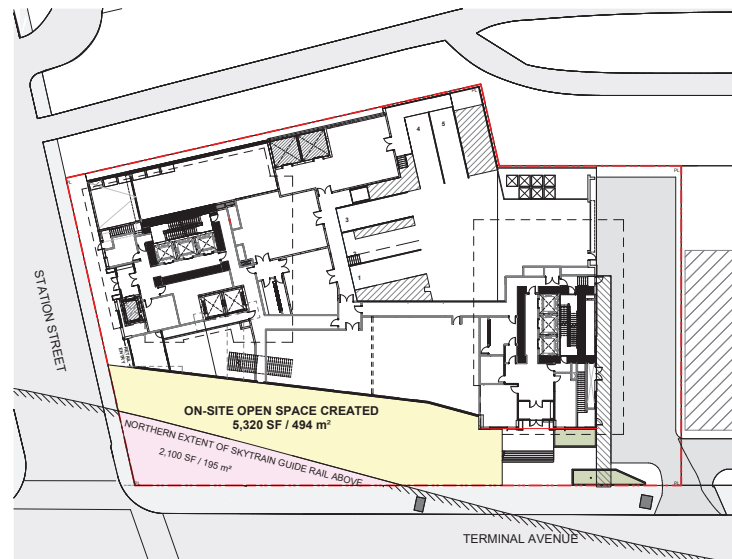


SHADOW STUDIES - MITIGATION

The shadows have been mapped on the graph, showing the area in the park that is covered by the proposed building since the approved office DP proposal, over the total area of the park (173,541 SF / 16,122 m²).

At 10am, the area ranges from approximately 10,000 SF to 17,000 SF covering less than 10% of the park's total area. At 11am, the shadows cover between 10,000 SF and 30,000 SF of the park, up to a maximum of 17% of the park's total area. By 12pm, the shadows have almost left the park entirely and represent less than the area of the on-site open space created.

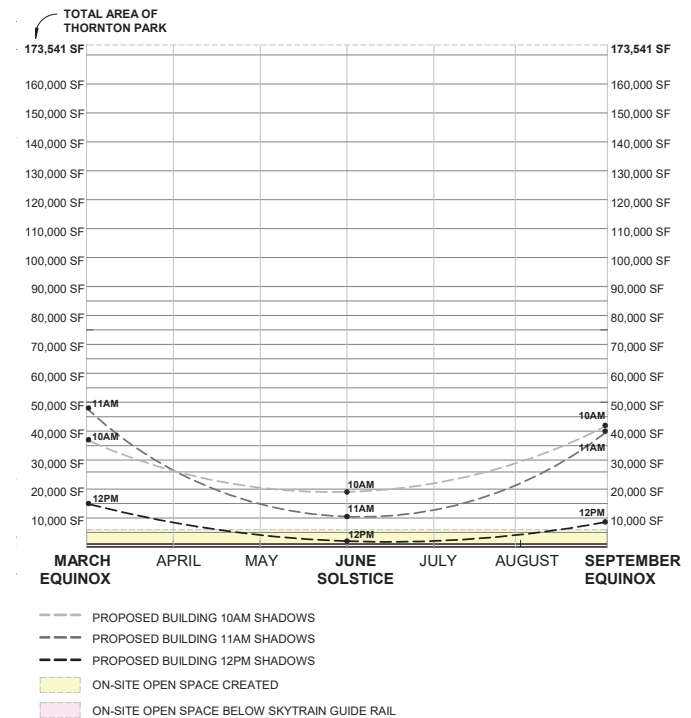
As a **mitigation measure**, the proposal includes an **additional 5,320 SF / 494 m² of open space** at grade along the South of the site. This area is in addition to the open space below the SkyTrain guide rail. The project provides public access to a new plaza space beyond the current public space of Thornton Park to provide additional solar access opportunities for citizens to benefit from solar access during the morning hours where this is some shadow impact on the park.



4 LEVEL 1 - FLOOR PLAN

The **new plaza** includes outdoor seating along the retail front of the building, spilling out into the public plaza, bordered by planters and benches, activating the local pedestrian activity along Terminal Avenue.

AREA OF ADDITIONAL SHADOW AREA IN THORNTON PARK SINCE OFFICE DP



SOLAR ACCESS AND SHADOW ANALYSIS

In summary, the proposal has some early morning shadow impact on Thornton Park in a similar manner to adjacent City Gate on Creekside Park, but to a lesser extent given the urban design strategy of significantly more slender towers.

The shadow impacts from the proposal clear Thornton Park by lunchtime at all times of year, and never impact more than ~20% of park area so solar access is always maintained across 80% of the park for park users. The proposed public plaza also creates additional opportunities for public enjoyment of the public space when morning shadows are present in the park.

The applicant will work with staff to manage shadows in a manner consistent with previous Downtown practice, including providing all technical methodology of the attached Solar Budgeting Analysis to support staff to fulfill the July 2024 Council directive to modernize solar access management in the Downtown.



View of Thornton Park looking East



Shadow Study over context plan taken on March 21st at 10am

Chapter 4: Landscape

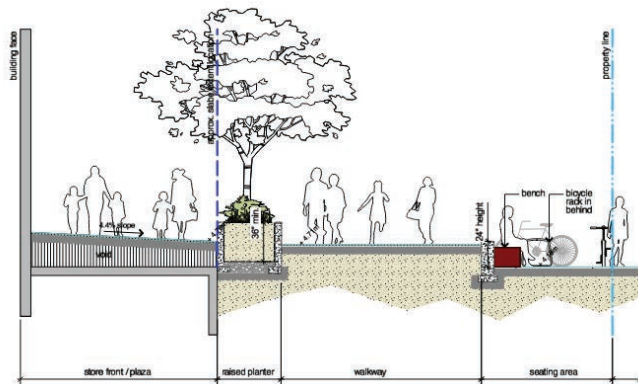
LEVEL 1 PLAZA

CONCEPT

The southern streetscape along Station Street features pedestrian-focused retail frontages with spill-out areas, enhanced by planting and seating that create opportunities to pause. A tree-lined promenade-inspired walkway, flanked by retail patios and outdoor seating, is complemented by West Coast-inspired elements like grizzly granite slabs and a public art feature at the plaza's western corner.

The plaza is designed as a vibrant, accessible space that encourages community interaction and enriches the urban landscape. It combines functionality and aesthetics, offering seating, greenery, and open areas for events. The design prioritizes pedestrian flow, safety, and inclusivity to meet diverse needs.

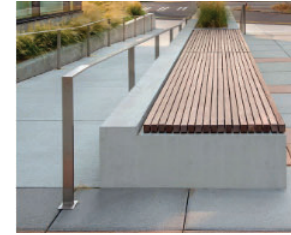
Public art, natural features, and thoughtful lighting add character, creating a welcoming ambiance and boosting foot traffic for nearby businesses. Enhanced lighting also improves security and ensures visitors feel safe at night. This new plaza aspires to become a beloved gathering place reflecting local culture and identity.



Section through level 1



Outdoor patios



Concrete benches with wood tops



Open plaza with seating



Feature Pots at retail entry



Paving Pattern



Granite Slabs



Seating around planters



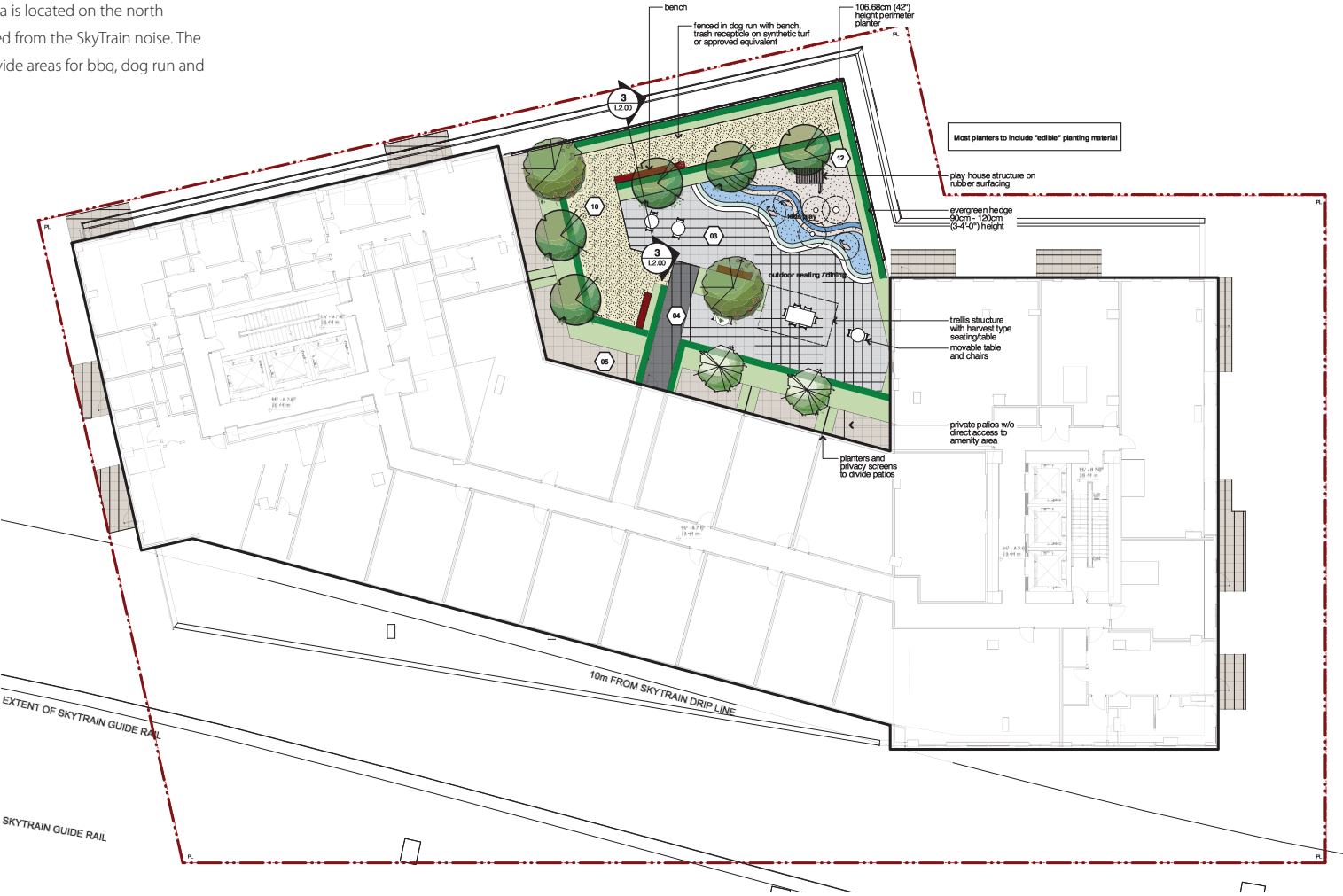
"Soft" concrete seating



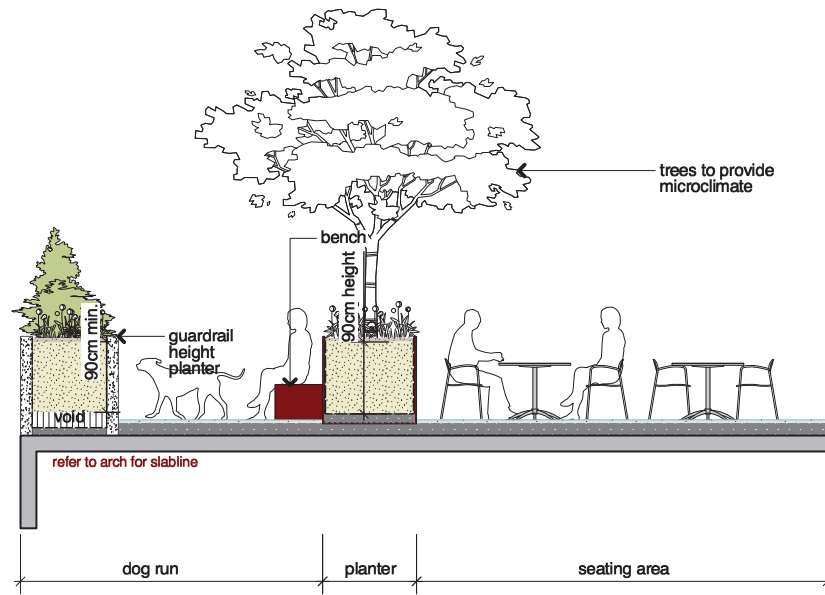
Illumination

LEVEL 5 - OUTDOOR AMENITY

The family-friendly area is located on the north amenity deck, sheltered from the SkyTrain noise. The amenity deck will provide areas for bbq, dog run and children's playground.



LEVEL 5 - OUTDOOR AMENITY



Section through dog run



Seating around planters



Seating around planters



Seating around planters



Seating around planters



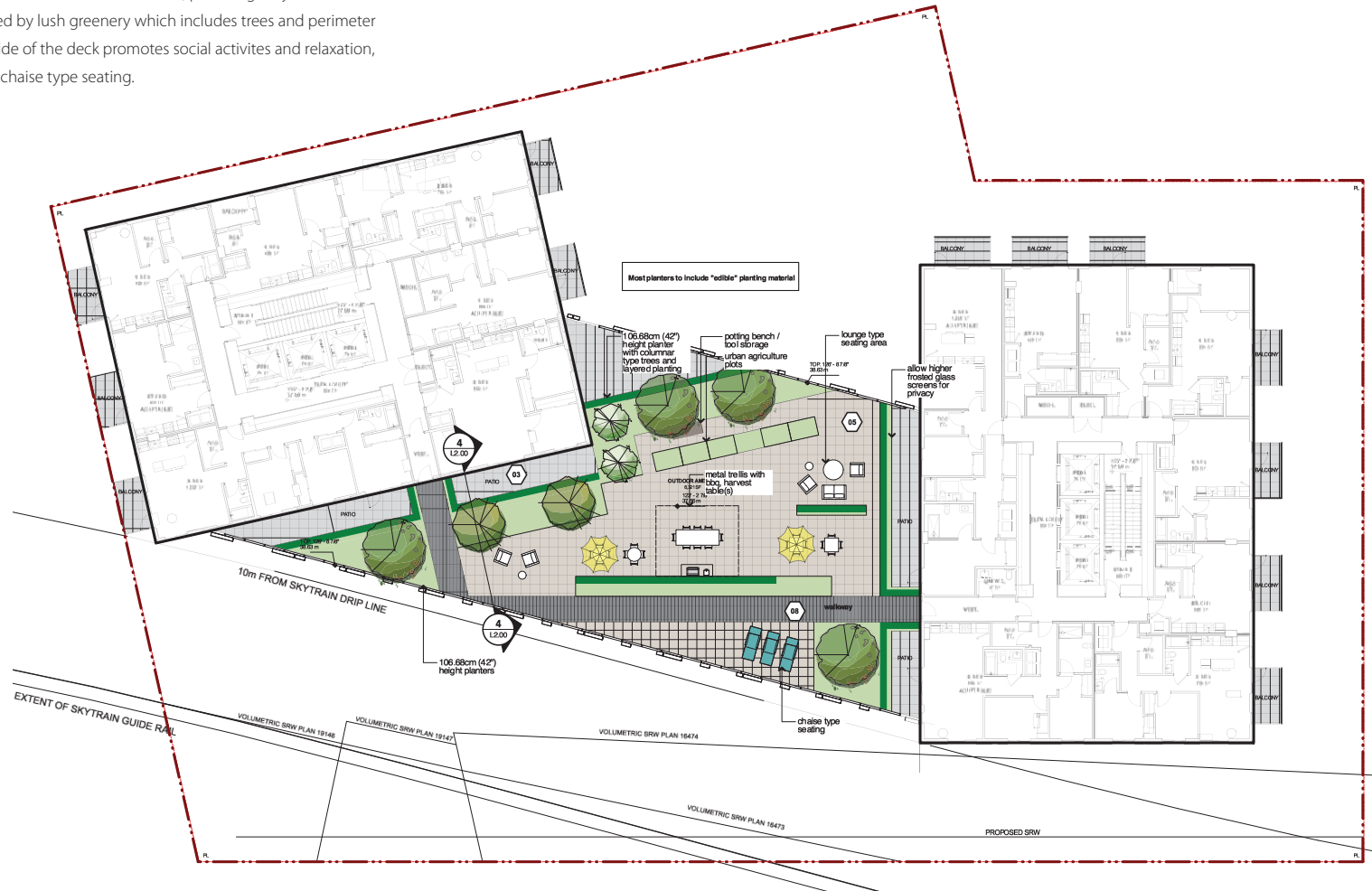
Seating around planters



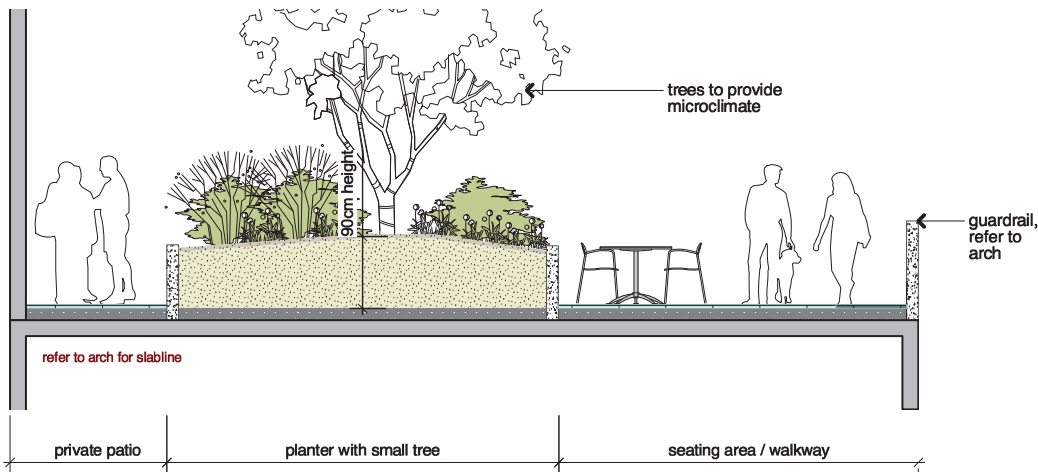
Seating around planters

LEVEL 9 - OUTDOOR AMENITY

Urban agriculture plots are located on this level, providing easy access from both towers. Bordered by lush greenery which includes trees and perimeter planters, the south side of the deck promotes social activities and relaxation, with a bbq area and chaise type seating.



LEVEL 9 - OUTDOOR AMENITY



Section through Level 9 Amenity Level



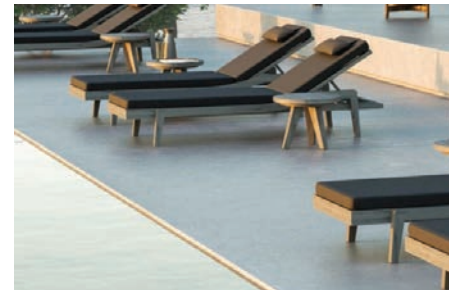
Outdoor Amenity / Seating



Board walk



Material Transition



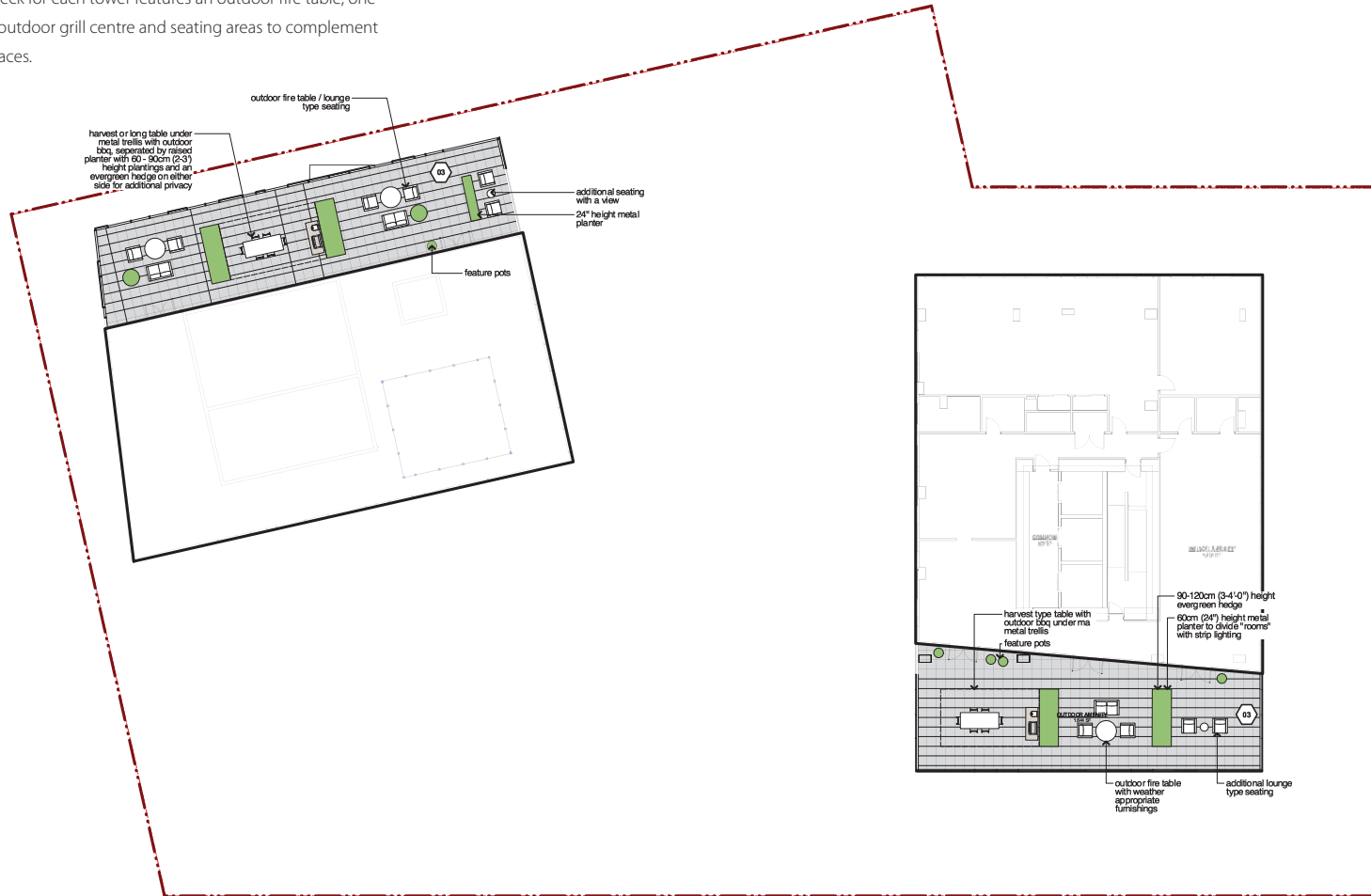
Chaise Type Seating



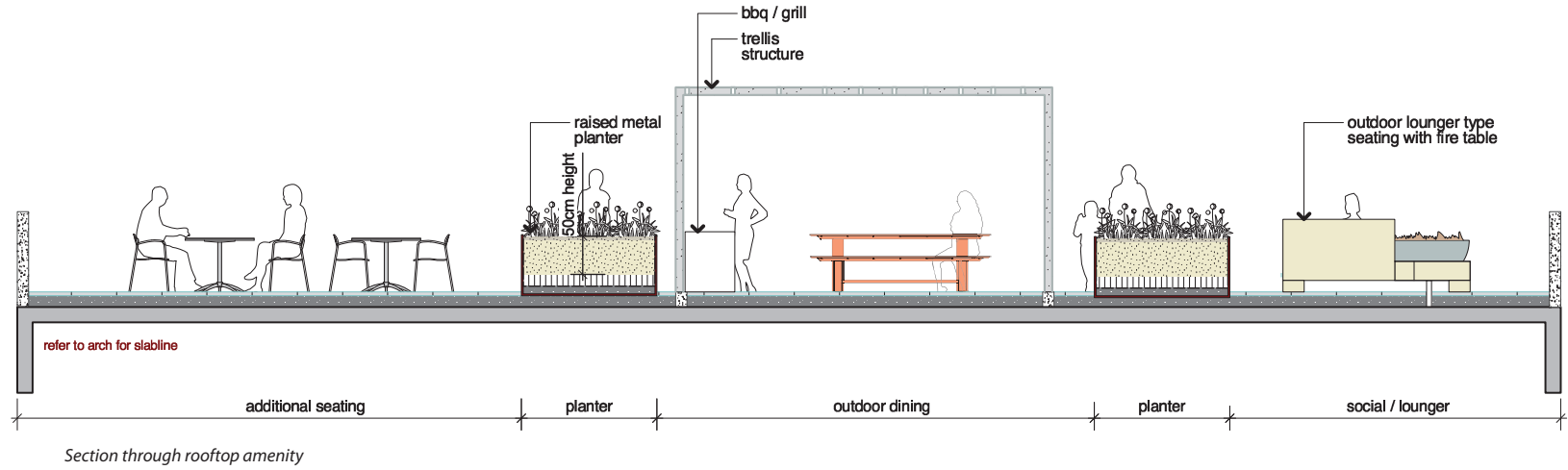
Outdoor Amenity Seating

ROOFTOP AMENITIES

The rooftop amenity deck for each tower features an outdoor fire table, one long harvest table, an outdoor grill centre and seating areas to complement the indoor amenity spaces.



ROOFTOP AMENITIES



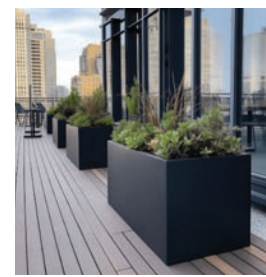
Metal Benches



Fire Table



Grill Centre



Metal Planters

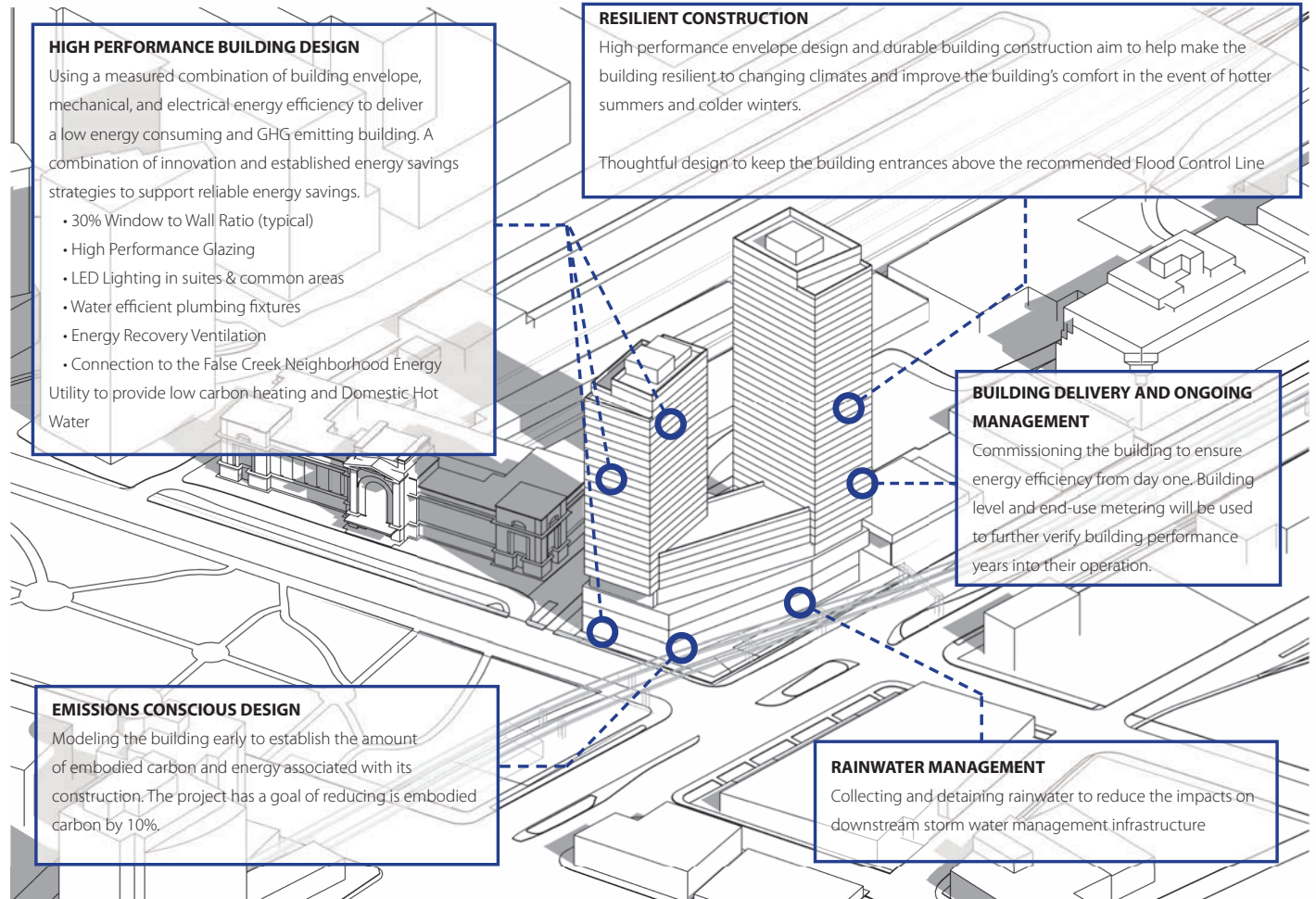


Harvest Table

Chapter 5: Sustainability

GREEN BUILDINGS POLICY

The **Green Buildings Policy** for Rezoning supports the creation of high-performance resilient buildings. 1220 Station Street will meet the City of Vancouver's industry-lead sustainable building policies and by-laws. The development has undertaken extensive analysis to understand the ways in which the building will perform, the impacts it will have on climate change and the impacts climate change will have on it.



Appendix A: Architectural Drawings



ISSUED FOR REZONING

ARCHITECTURAL

PROJECT DIRECTORY

CLIENT



Great West Life Realty Advisors
1600-650 West Georgia Street
Vancouver, BC, Canada, V6B 4N7
Tel: 604-713-6450

ARCHITECT

Musson Cattell Mackey Partnership
Architects Designers Planners
Oceanic Plaza
1066 West Hastings Street
Suite 1900
Vancouver, BC V6E 3X1
Tel: 604-687-2990

STRUCTURAL

Giotman Simpson
1661 W 5th Ave
Vancouver, BC V6J 1N5
Tel: 604-734-8822

MECHANICAL

Burt & Associates
180 - 200 Granville St
Vancouver, BC V6C 1S4
Tel: 604-687-1800

ELECTRICAL

Gunn Concessions Ltd.
2009 W 4th Ave W
Vancouver, BC V6J 1N3
Tel: 604-736-6562

TRAFFIC

Burt & Associates
1550 - 1060 W Pender St
Vancouver, BC V6J 1N3
Tel: 604-685-6427

ELEVATOR

Gunn Concessions Inc.
166 - 1901 Rossar Ave
North Vancouver, BC V7P 3T5
Tel: 604-630-2276

LANDSCAPE

Durante Kreuk Ltd.
102 - 1637 W 5th Ave
Vancouver, BC V6J 1N3
Tel: 604-684-4611

SURVEYOR

Bennett Surveys Ltd.
201 - 275 Fall Avenue
North Vancouver, BC V7P 3T5
Tel: 604-980-5856

GEOTECHNICAL

GeoPacific Consultants Ltd.
1776 W 75th Ave
Vancouver, BC V6P 3T1
Tel: 604-439-0922

CODE & C.P.

Jensen Hughes
500 - 1901 Rossar Ave
North Vancouver, BC V7P 3T5
Tel: 604-684-2384

SUSTAINABILITY

Introba
180 - 200 Granville St
Vancouver, BC V6C 1S4
Tel: 604-687-1800

CIVIL

Cross Engineering
610 - East Tower, 221 Esplanade West
North Vancouver, BC V7M 3J3
Tel: 604-738-2500

BUILDING ENVELOPE

BC Building Science Ltd.
611 Bent Ct
New Westminster, BC V3M 1V3
Tel: 604-520-6456

ENVIRONMENTAL

SLR Consulting Ltd.
200 - 887 Great Northern Way
Vancouver, BC V5T 4T5
Tel: 604-738-2500

ACOUSTIC

Rowan Williams Davies & Irwin Inc.
280 - 1385 W 8th Ave
Vancouver, BC V6E 3V9
Tel: 604-730-5688

ARBORIST

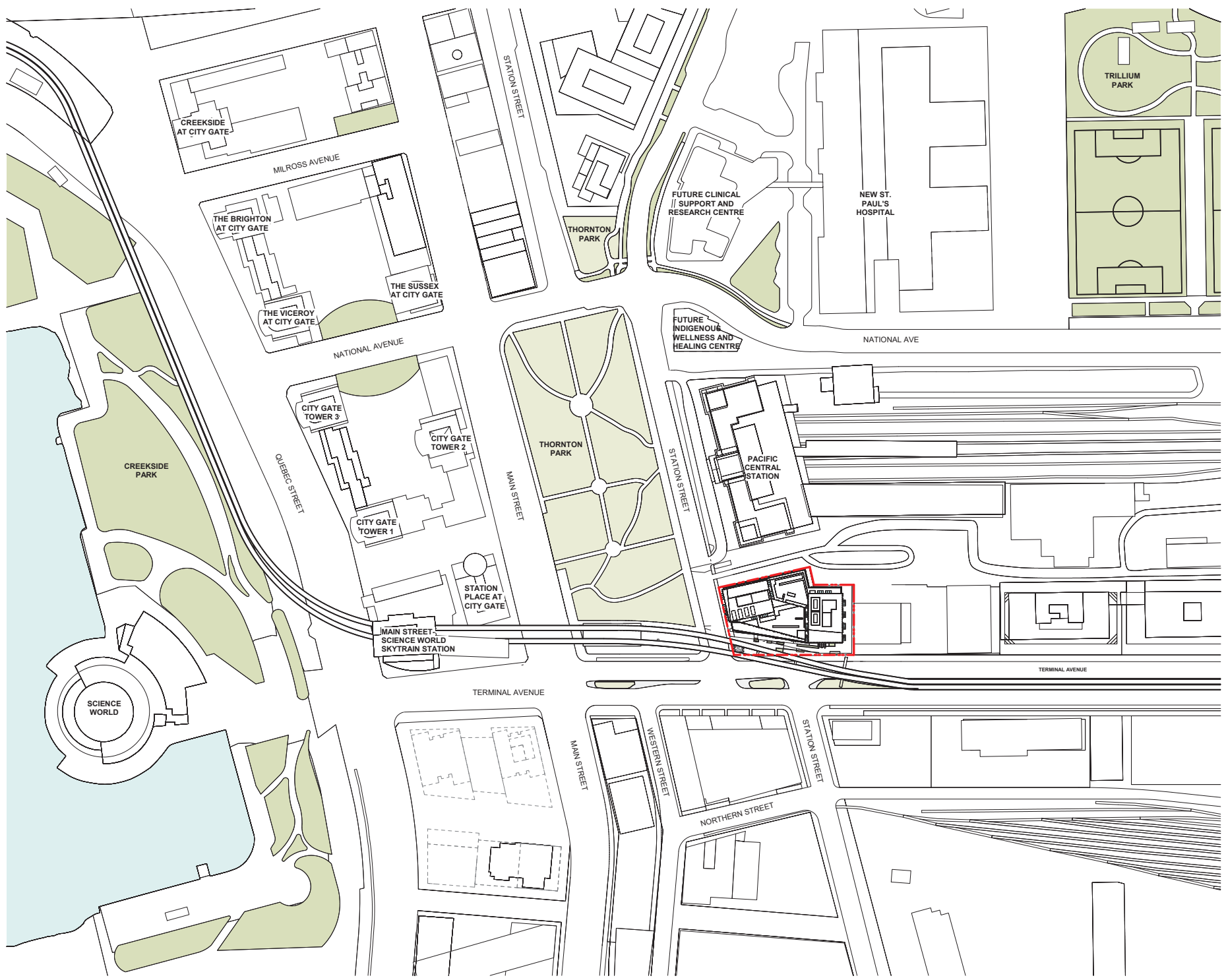
Durante Kreuk Ltd.
100 - 1637 W 5th Ave
Vancouver, BC V6J 1N5
Tel: 604-684-4611

PLANNING

Pooni Group
200 - 1055 West Hastings
Vancouver, BC V6E 2G9
Tel: 604-731-9053

1220 STATION STREET

224004



1 CONTEXT PLAN
SCALE: 1" = 100'



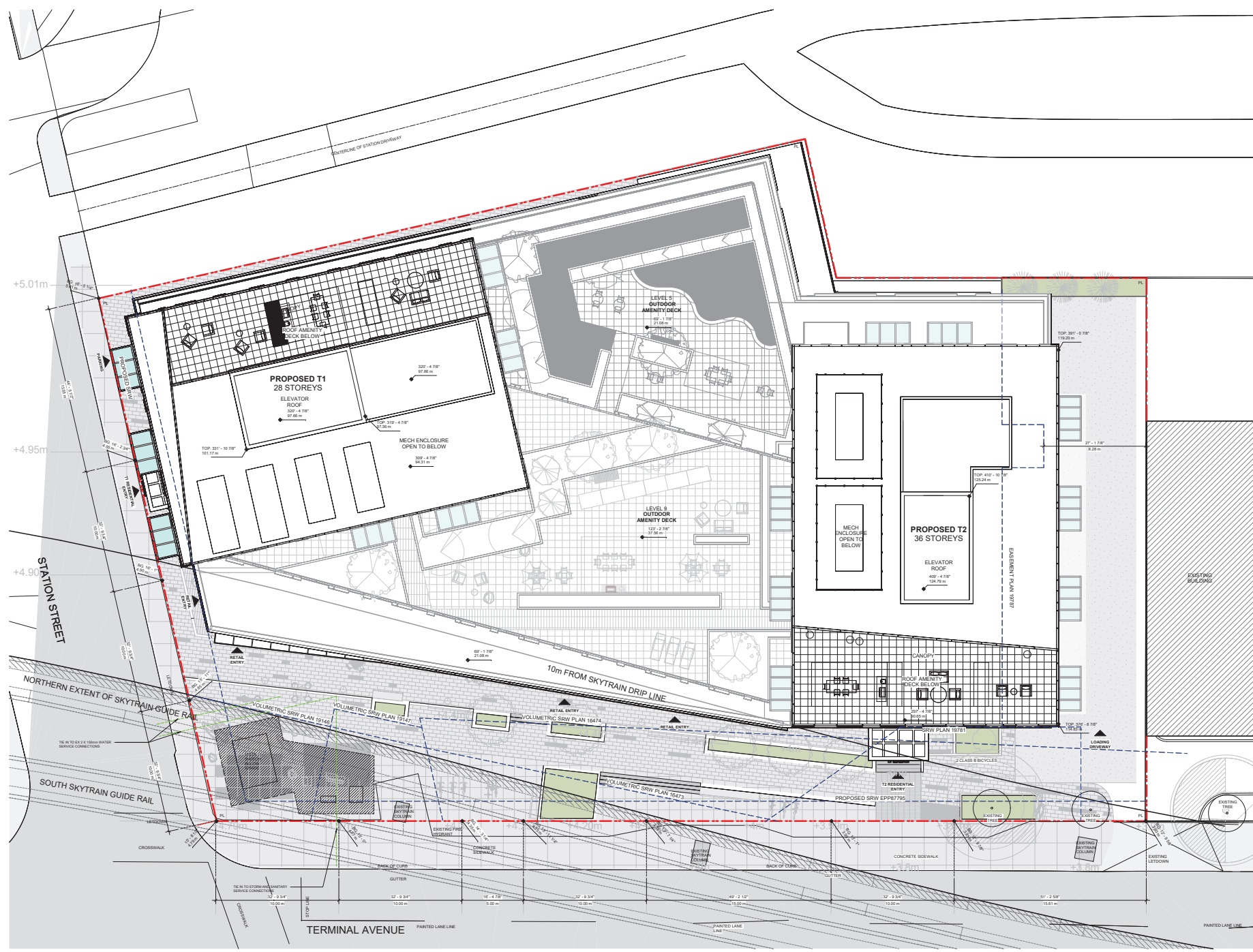
1220 STATION STREET

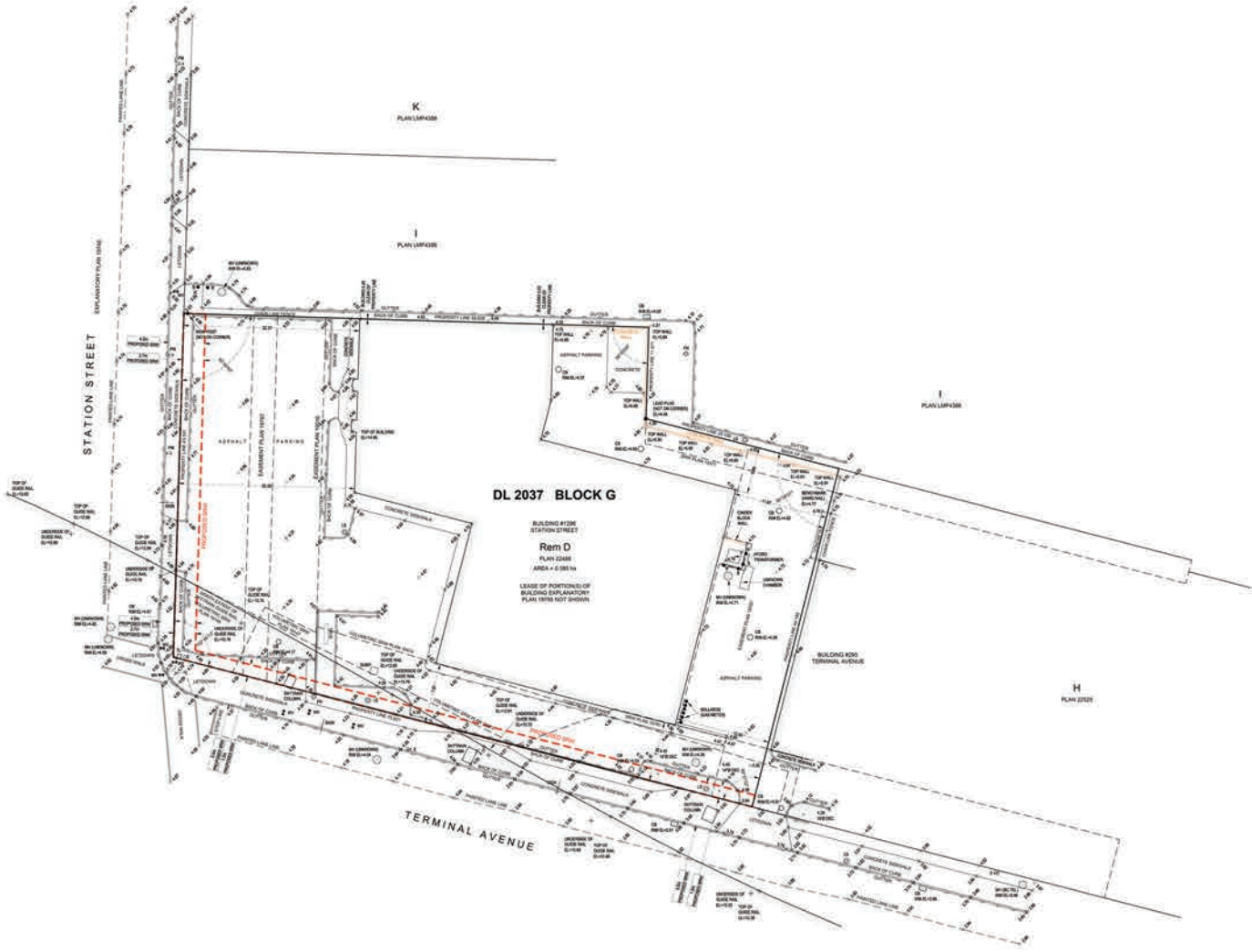


1220 Station Street
Vancouver, BC
Project
CONTEXT PLAN

Sheet

A101





**TOPOGRAPHIC SURVEY OF LOT D,
EXCEPT PART IN PLAN 22525,
Bk G, DL 2037, G 1, NWD,
PLAN 22488**
CITY OF VANCOUVER
INTEGRATED SURVEY AREA #31
CIVIC ADDRESS: 1226 STATION STREET
PID: 015-061-075



- LEGEND:**
- 1/4" IDENTIFIED ELECTRICAL SERVICE BOX
 - 1/4" IDENTIFIED PLUMBING UNIT
 - 1/4" IDENTIFIED FIRE ALARM
 - 1/4" IDENTIFIED FIRE EXTINGUISHER
 - 1/4" IDENTIFIED STREET SIGN
 - 1/4" IDENTIFIED SIGNAGE
 - 1/4" IDENTIFIED CAPTOP MARK
 - 1/4" IDENTIFIED WATER VALVE
 - 1/4" IDENTIFIED FUSE MOUNTING
 - 1/4" IDENTIFIED METER
 - 1/4" IDENTIFIED PROFESSIONAL

STATUS:
SURVEYING AND PHOTOGRAPHY OF THE SURVEY AREA AND THE SURVEYING INSTRUMENTS AND METHODS USED IN THE SURVEY ARE DESCRIBED IN THE SURVEY REPORT. ALL SURVEYING INSTRUMENTS AND METHODS USED IN THE SURVEY ARE DESCRIBED IN THE SURVEY REPORT.

PROPERTY:
PROPERTY BOUNDARIES SHOWN ARE DERIVED FROM PREVIOUS PLANS AND SURVEYS. THE PROPERTY LINES ARE NOT TO BE USED TO DETERMINE BOUNDARIES.

GENERAL:
THIS PLAN SHOWS THE LOCATION OF THE SURVEY AREA ONLY AND DOES NOT SHOW THE LOCATION OF THE SURVEY AREA OR THE SURVEY AREA. THE SURVEY AREA IS NOT TO BE USED TO DETERMINE BOUNDARIES.

ADDITIONAL INFORMATION:
THIS SURVEY AND INSTRUMENTS WERE CONDUCTED BY A QUALIFIED SURVEYOR AND INSTRUMENTS WERE CALIBRATED AND CHECKED AT THE TIME OF THE SURVEY. THE SURVEYOR IS NOT RESPONSIBLE FOR ANY ERRORS OR OMISSIONS IN THIS SURVEY.



DATE	BY
2024-01-11	DL
2024-08-14	DL
2024-09-04	DL

REVISIONS:

THIS PLAN HAS BEEN PREPARED FOR THE PURPOSES OF THE SURVEY. IT IS NOT TO BE USED FOR ANY OTHER PURPOSES. ALL RIGHTS RESERVED. ALL RIGHTS RESERVED. ALL RIGHTS RESERVED. ALL RIGHTS RESERVED. ALL RIGHTS RESERVED. ALL RIGHTS RESERVED. ALL RIGHTS RESERVED. ALL RIGHTS RESERVED. ALL RIGHTS RESERVED. ALL RIGHTS RESERVED.

PROPERTY OWNER:
MUSSON CATTELL MACKAY PARTNERSHIP
1226 STATION STREET
VANCOUVER, BC
V6E 3E1
TEL: 604.687.2080
FAX: 604.687.2772
WWW.MCM.COM

PEDESTRIAN STATUTORY RIGHT OF WAY REVIEW

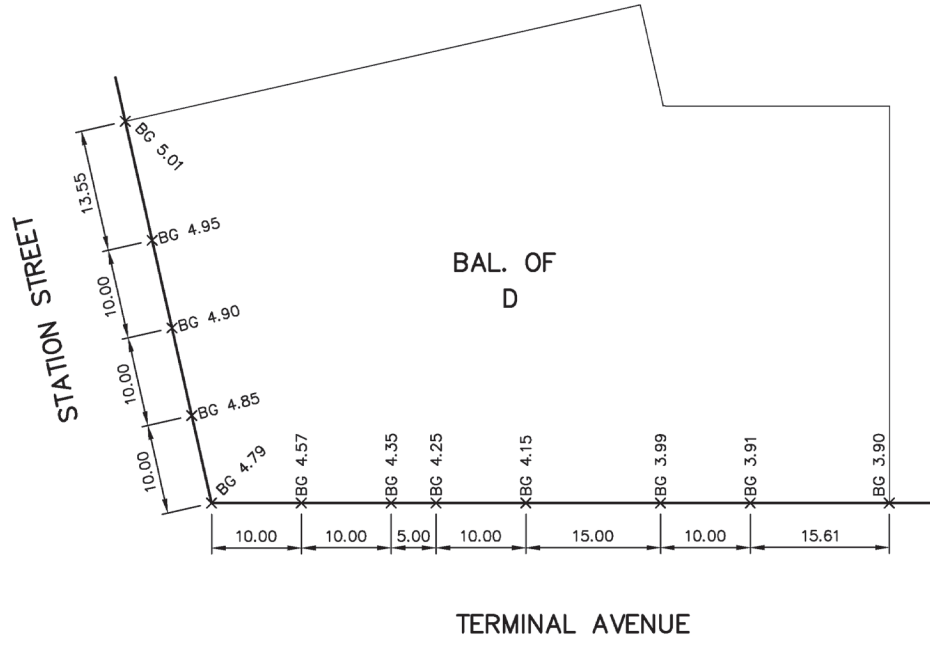
1220 STATION STREET
VANCOUVER, BC
V6E 3E1
TEL: 604.687.2080
FAX: 604.687.2772
WWW.MCM.COM

SITE SURVEY





Revision: YYYYMMDD
 1 ISSUED FOR PERMITS 2018-01-11
 2 ISSUED FOR PERMITS 2018-04-14
 3 ISSUED FOR PERMITS 2018-04-14



NOTE: THIS AREA MAY BE SUBJECT TO THE CITY OF VANCOUVER FLOODPROOFING POLICIES.

BENCH MARK: ELEVATION: 3.813 DESCRIPTION: SURVEY MONUMENT MARKED V-2123 AT THE NORTH EAST CORNER OF TERMINAL AVENUE AND QUEBEC STREET.
 THE CITY OF VANCOUVER ASSUMES NO RESPONSIBILITY FOR PROPERTY DIMENSIONS ON THIS PLAN.

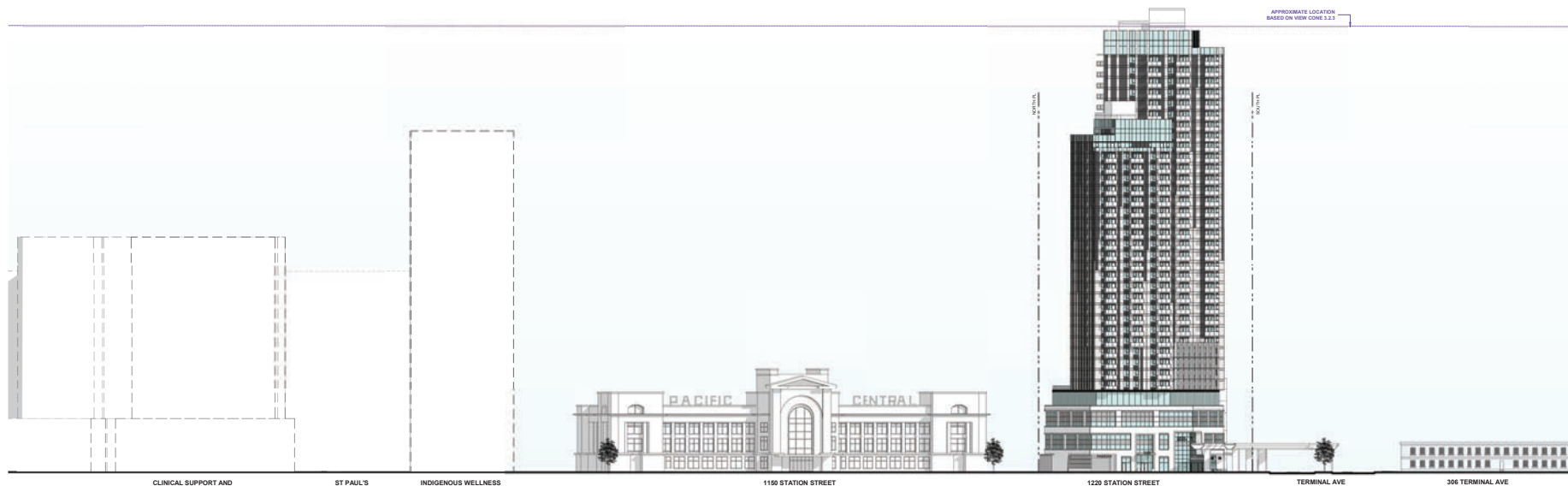
ATTENTION
 ELEVATIONS SHOWN ON THIS PLAN ARE IN METRES BASED ON GVRD DATUM (ISSUED MARCH 31, 2005). DIMENSIONS ARE ALSO IN METRES.

NO.	DATE	REVISION	BY	CHK	CITY OF VANCOUVER ENGINEERING SERVICES		
					BUILDING GRADE ELEVATIONS FOR LOT D, BLK. G, D.L. 2037, PLAN 22488.		
							SCALE: 1:500
							DWG. NO. BG-2017-00133
						SHEET / OF REVISION:	

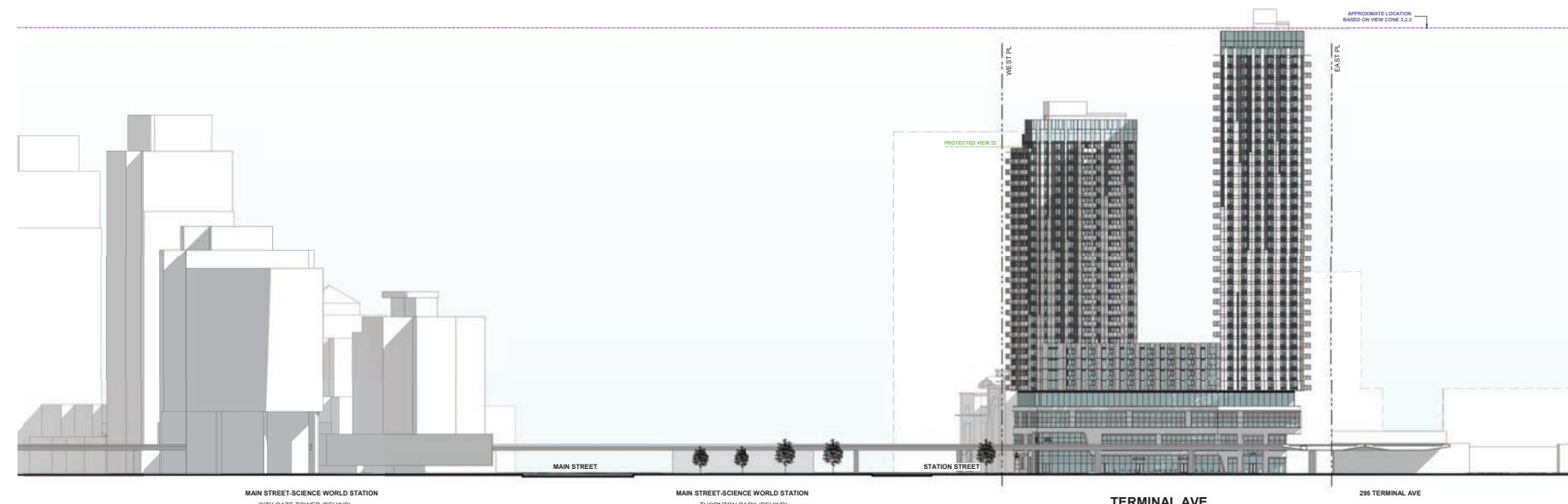
1 **BUILDING GRADES**
 SCALE: 1/8" = 1'-0"



1220 STATION STREET
 1220 Station Street
 Vancouver, BC
 Project
 BUILDING GRADES
 Date: 1/11/17
 Scale: 1/8" = 1'-0"
 Project: 2017-00133
 Sheet: **A104**

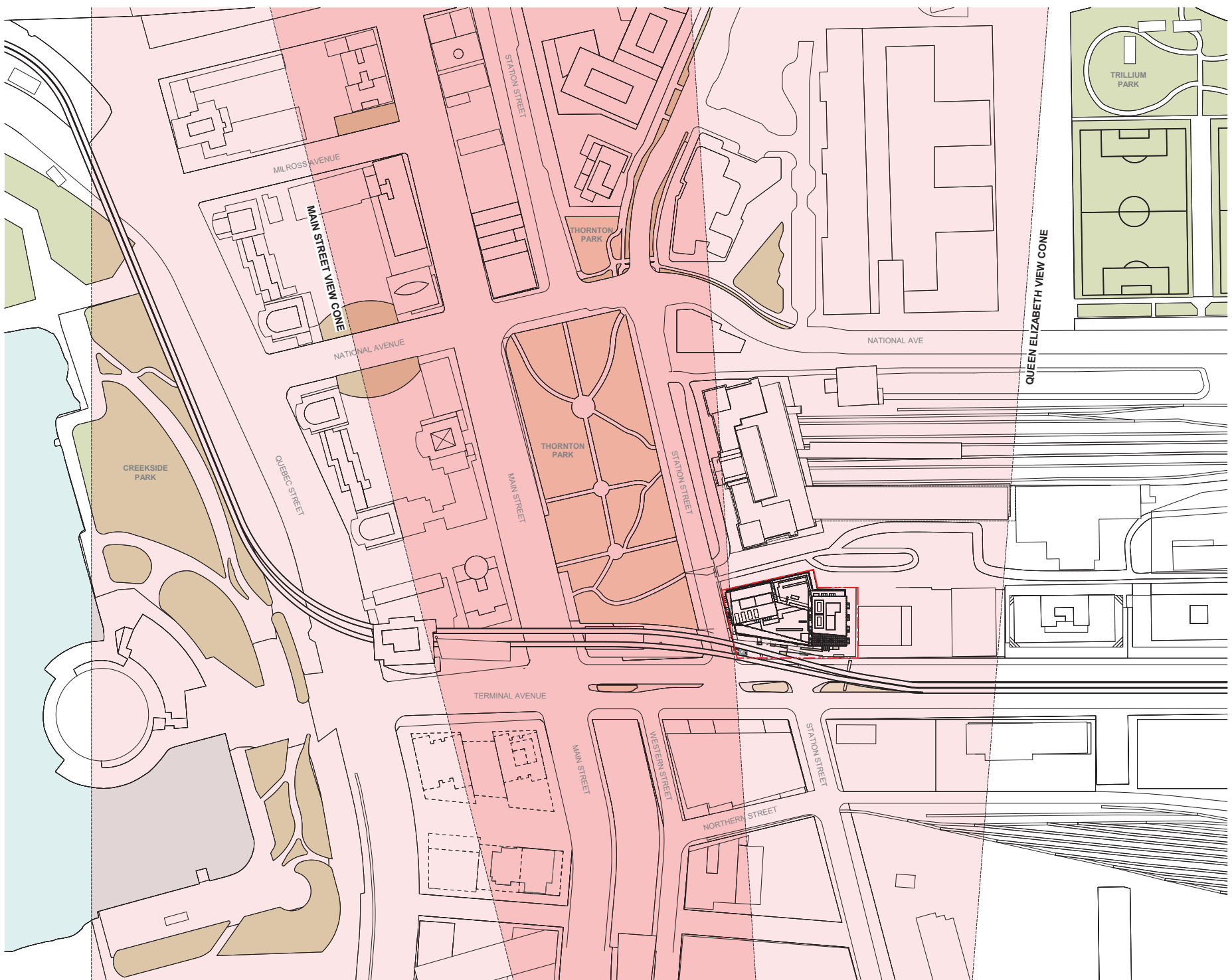


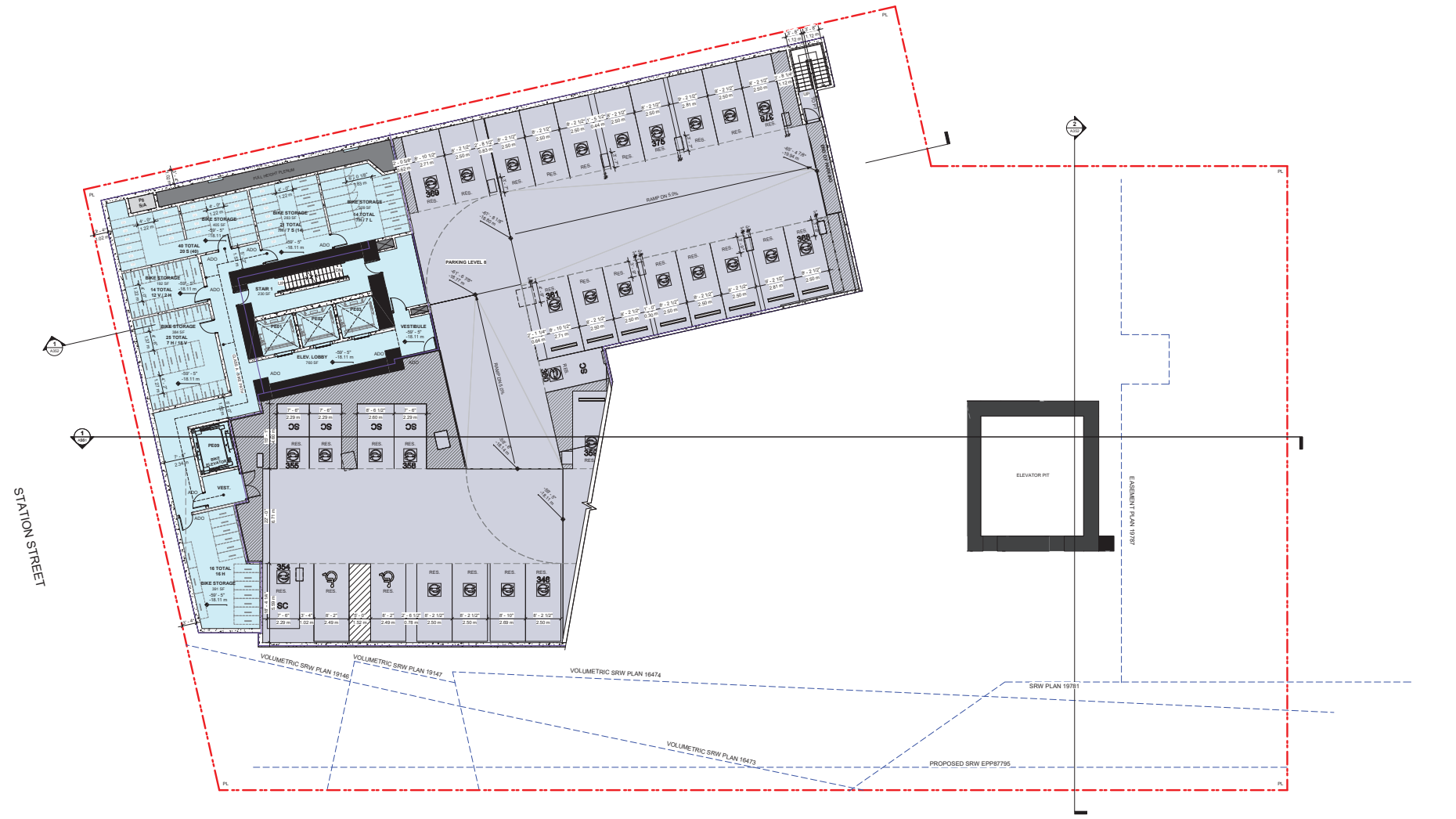
1 STREETScape ELEVATION - STATION STREET
SCALE: 1/32" = 1'-0"



2 STREETScape ELEVATION - TERMINAL AVE
SCALE: 1/32" = 1'-0"







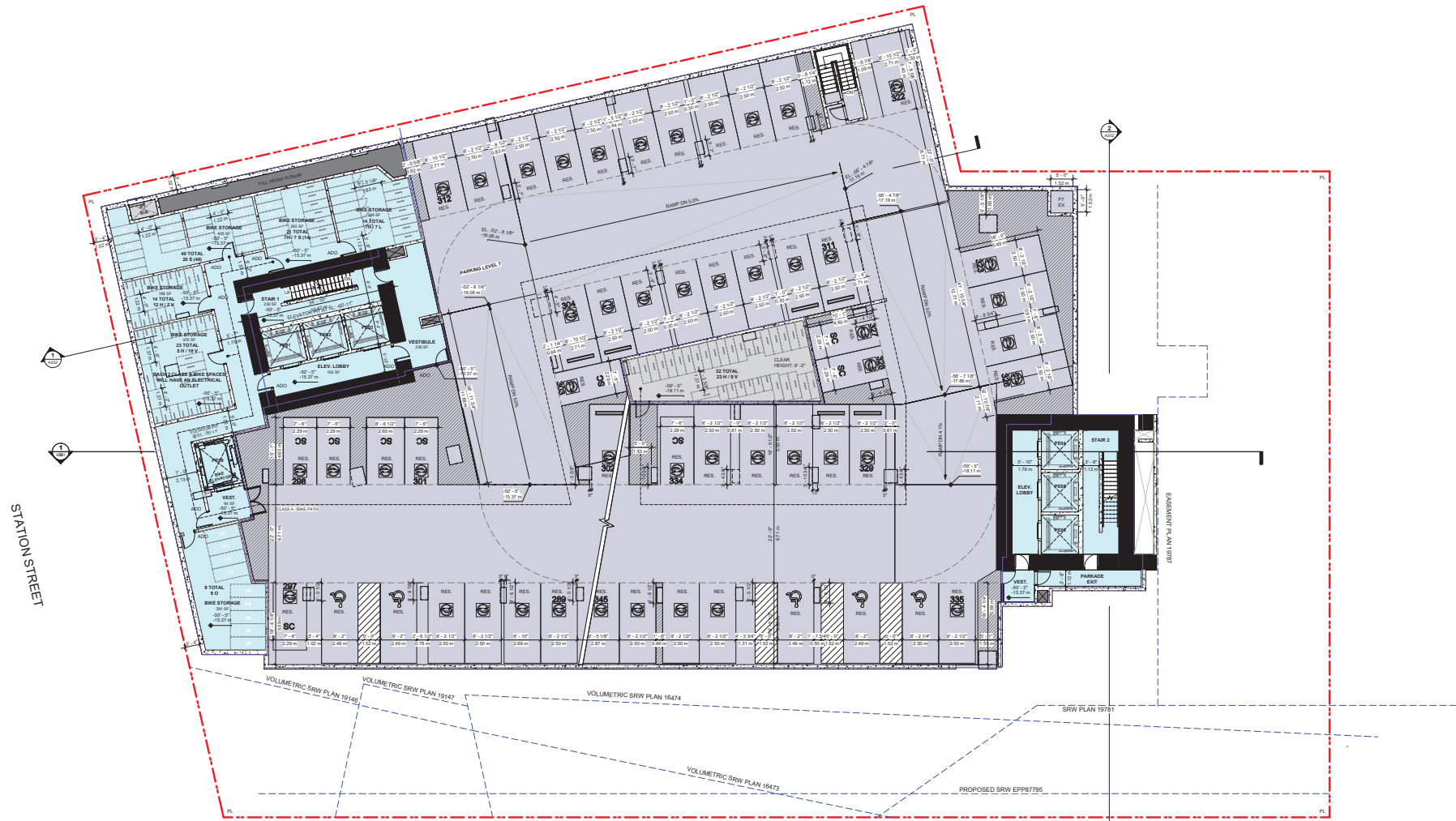
TERMINAL AVENUE

1 LEVEL P8 - FLOOR PLAN
 SCALE: 1/8" = 1'-0"

PARKING STALL SCHEDULE LEVEL P8				
Level	Total Stalls	Total Reserved Stalls	Total Access Stalls	Total Parking Stalls
LEVEL P8	32	0	0	32

PROVIDED BIKE STALLS						
Level	CLASS A Stalls		CLASS B Stalls		CLASS C Stalls	
	Horizontal Bike Stalls (2' x 5')	Vertical Bike Stalls (2' x 5')	Stack Bike Stalls (2' x 5')	Bike Lockers (2' x 6')	Overseer Bike Stalls (2' x 6')	Exterior Visitor Bike Stalls
LEVEL P8	20	1	62	24	22	0
LEVEL P9	32	0	62	24	22	0
LEVEL P10	32	0	62	24	22	0
LEVEL P11	44	0	62	24	22	0
LEVEL P12	32	0	62	24	22	0
LEVEL P13	20	0	62	24	22	0
TOTAL	200	1	372	96	110	0





1 LEVEL P7 - FLOOR PLAN
 SCALE: 1/8" = 1'-0"

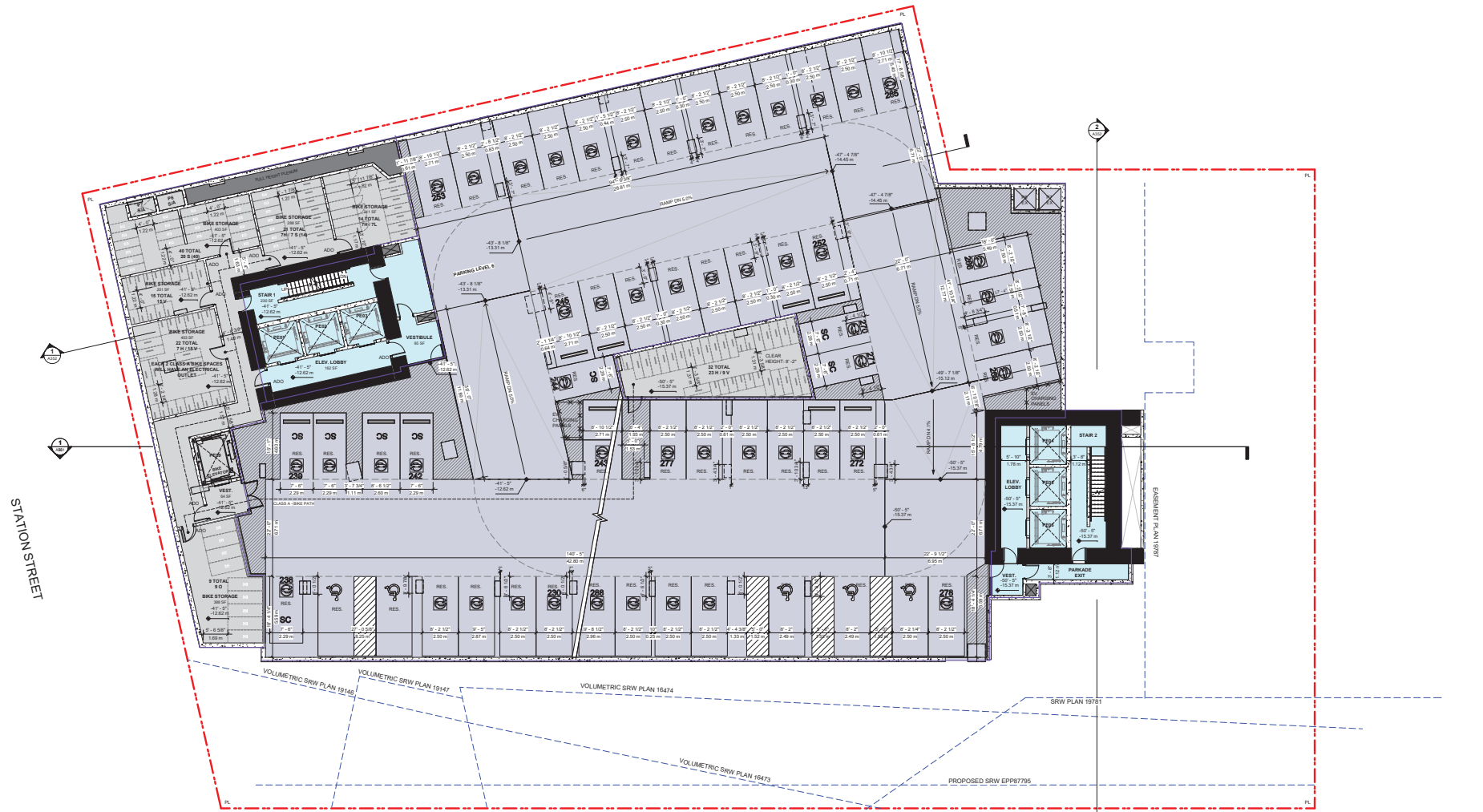
TERMINAL AVENUE

PARKING STALL SCHEDULE LEVEL P7				
Level	Class A Stalls	Class B Stalls	Class C Stalls	Total Parking Stalls
LEVEL P7	31	0	0	31

Level	PROVIDED BIKE STALLS				
	Horizontal Bike Stalls (2' x 5')	Vertical Bike Stalls (2' x 5')	Stack Bike Stalls (2' x 5')	CLASS A Stalls	CLASS B Stalls
LEVEL P7	20	0	0	0	0
LEVEL P5	33	1	0	24	0
LEVEL P4	40	0	0	22	0
LEVEL P3	38	0	0	0	0
LEVEL P2	40	0	0	0	0
LEVEL P1	44	0	0	0	0
LEVEL P0	30	0	0	0	0
LEVEL P7	200	101	330	50	0



LEGEND
 COMMON
 PARKING
 SERVICE



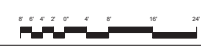
PARKING STALL SCHEDULE LEVEL P6

Level	Total Required Stalls	Total Provided Stalls	Total Service Stalls	Total Parking Stalls
LEVEL P6	33	33	0	33

PROVIDED BIKE STALLS

Level	CLASS A Stalls		CLASS B Stalls		Total
	Horizontal Bike Stalls (2' x 6')	Vertical Bike Stalls (2' x 6')	Overhead Bike Stalls (2' x 6')	Exterior Vertical Bike Stalls	
LEVEL P6	20	0	0	0	20
LEVEL P7	33	0	0	0	33
LEVEL P8	33	0	0	0	33
LEVEL P9	33	0	0	0	33
LEVEL P10	33	0	0	0	33
LEVEL P11	33	0	0	0	33
LEVEL P12	33	0	0	0	33
LEVEL P13	33	0	0	0	33
LEVEL P14	33	0	0	0	33
LEVEL P15	33	0	0	0	33
LEVEL P16	33	0	0	0	33
LEVEL P17	33	0	0	0	33
LEVEL P18	33	0	0	0	33
LEVEL P19	33	0	0	0	33
LEVEL P20	33	0	0	0	33
LEVEL P21	33	0	0	0	33
LEVEL P22	33	0	0	0	33
LEVEL P23	33	0	0	0	33
LEVEL P24	33	0	0	0	33
LEVEL P25	33	0	0	0	33
LEVEL P26	33	0	0	0	33
LEVEL P27	33	0	0	0	33
LEVEL P28	33	0	0	0	33
LEVEL P29	33	0	0	0	33
LEVEL P30	33	0	0	0	33
LEVEL P31	33	0	0	0	33
LEVEL P32	33	0	0	0	33
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LEVEL P35	33	0	0	0	33
LEVEL P36	33	0	0	0	33
LEVEL P37	33	0	0	0	33
LEVEL P38	33	0	0	0	33
LEVEL P39	33	0	0	0	33
LEVEL P40	33	0	0	0	33
LEVEL P41	33	0	0	0	33
LEVEL P42	33	0	0	0	33
LEVEL P43	33	0	0	0	33
LEVEL P44	33	0	0	0	33
LEVEL P45	33	0	0	0	33
LEVEL P46	33	0	0	0	33
LEVEL P47	33	0	0	0	33
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LEVEL P86	33	0	0	0	33
LEVEL P87	33	0	0	0	33
LEVEL P88	33	0	0	0	33
LEVEL P89	33	0	0	0	33
LEVEL P90	33	0	0	0	33
LEVEL P91	33	0	0	0	33
LEVEL P92	33	0	0	0	33
LEVEL P93	33	0	0	0	33
LEVEL P94	33	0	0	0	33
LEVEL P95	33	0	0	0	33
LEVEL P96	33	0	0	0	33
LEVEL P97	33	0	0	0	33
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LEVEL P99	33	0	0	0	33
LEVEL P100	33	0	0	0	33

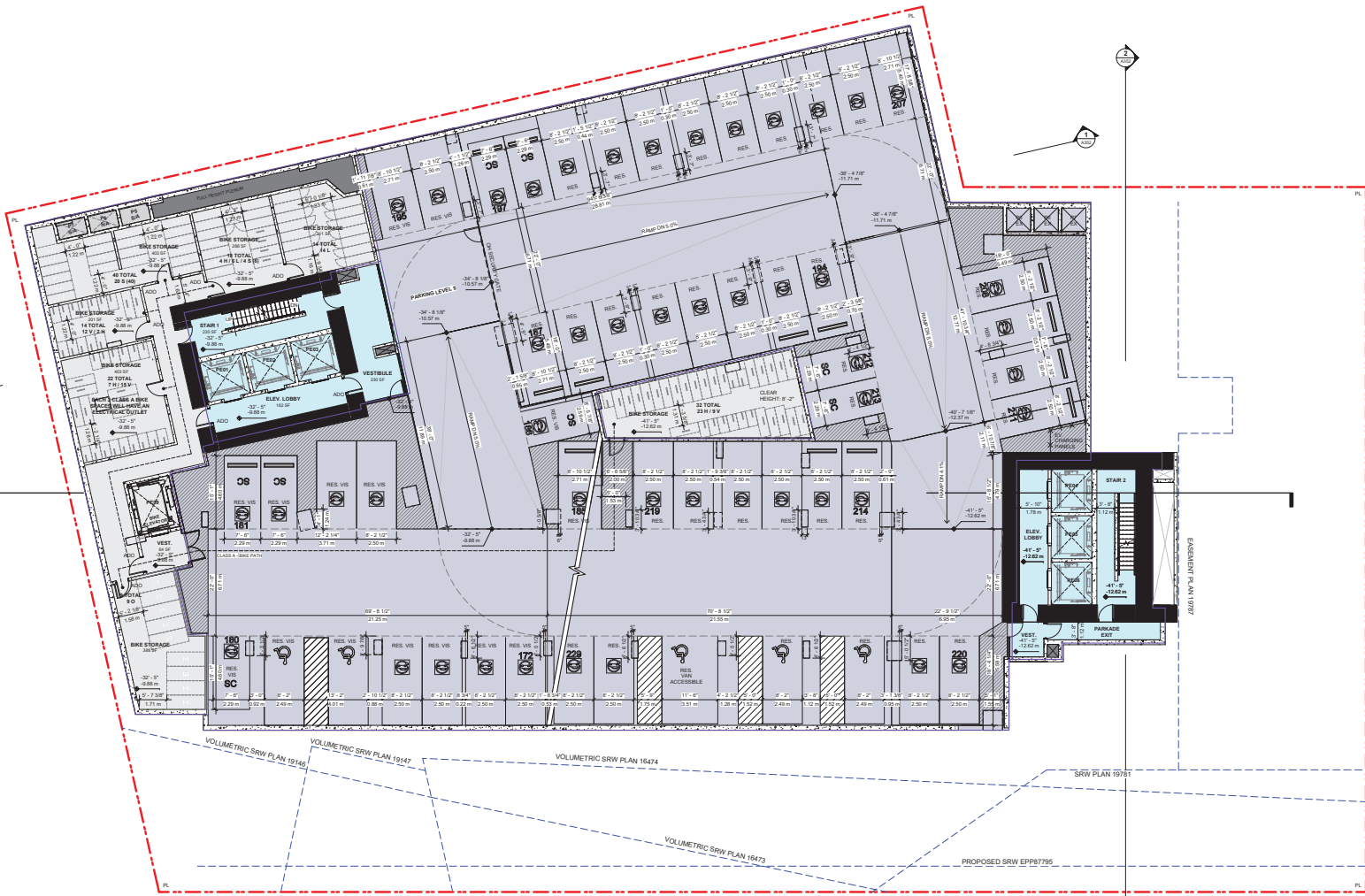
1 LEVEL P6 - FLOOR PLAN
 SCALE: 1/8" = 1'-0"



1220 STATION STREET

FLOOR PLAN LEVEL P6

LEGEND
 COMMON
 PARKING
 STORAGE



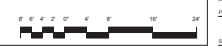
1 LEVEL P5 - FLOOR PLAN
 SCALE: 1/8" = 1'-0"

PARKING STALL SCHEDULE LEVEL P5

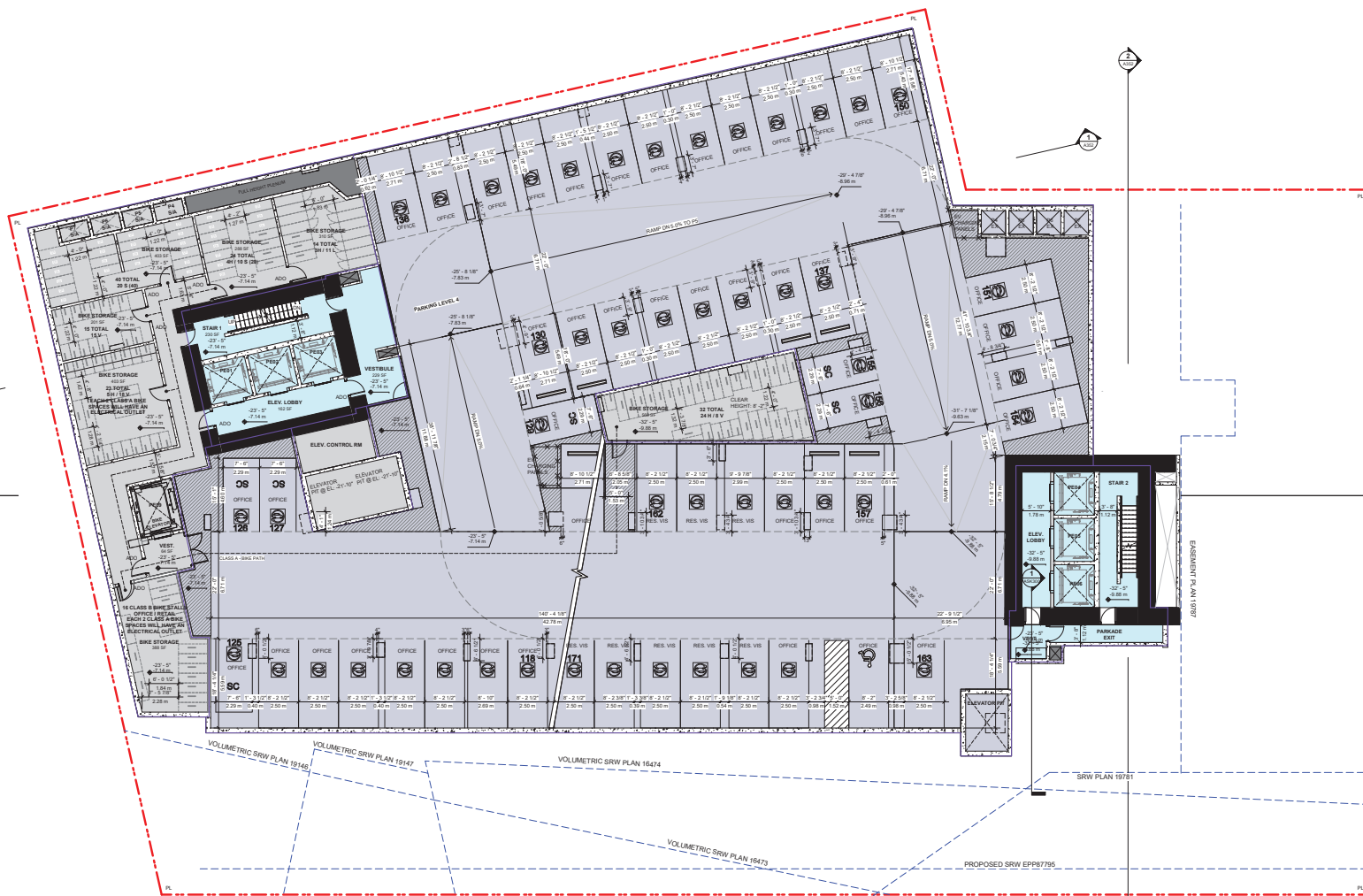
Level	Class A Stalls	Class B Stalls	Class C Stalls	Class D Stalls	Total Parking Stalls
LEVEL P5	30	30	30	30	120

PROVIDED BIKE STALLS

Level	Horizontal Bike Stalls (2' x 5')		Vertical Bike Stalls (2' x 5' 4")		CLASS A Stalls		CLASS B Stalls		Total
	Horizontal	Vertical	Horizontal	Vertical	Class A	Class B	Class C		
LEVEL P5	30	30	30	30	30	30	30	120	



LEGEND
 SOCCOMM
 PARKING
 SERVICE



STATION STREET

TERMINAL AVENUE

1 LEVEL P4 - FLOOR PLAN
 SCALE: 1/8" = 1'-0"

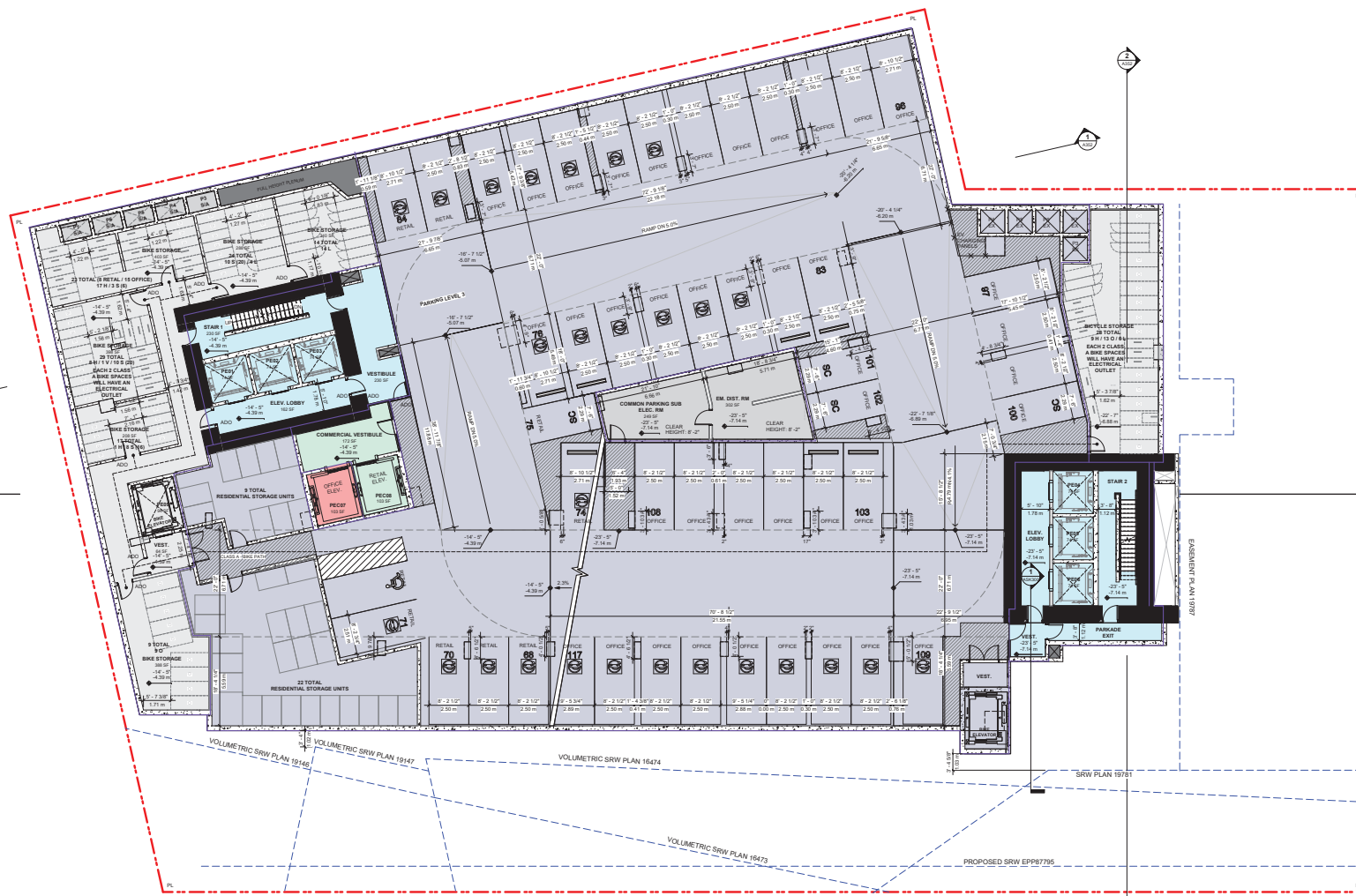
Level	Class A Stalls	Class B Stalls	Class C Stalls	Class D Stalls	Total Parking Stalls
LEVEL P4	0	0	0	40	40

Level	CLASS A STALLS		CLASS B STALLS		Total
	Horizontal Bike Stalls (2' x 6')	Vertical Bike Stalls (2' x 6')	Stack Bike Stalls (2' x 6')	Overhead Bike Stalls (2' x 6')	
LEVEL P4	0	0	0	0	0
LEVEL P3	0	0	0	0	0
LEVEL P2	0	0	0	0	0
LEVEL P1	0	0	0	0	0
LEVEL P0	0	0	0	0	0
LEVEL P-1	0	0	0	0	0
LEVEL P-2	0	0	0	0	0
LEVEL P-3	0	0	0	0	0
LEVEL P-4	0	0	0	0	0
LEVEL P-5	0	0	0	0	0
LEVEL P-6	0	0	0	0	0
LEVEL P-7	0	0	0	0	0
LEVEL P-8	0	0	0	0	0
LEVEL P-9	0	0	0	0	0
LEVEL P-10	0	0	0	0	0
LEVEL P-11	0	0	0	0	0
LEVEL P-12	0	0	0	0	0
LEVEL P-13	0	0	0	0	0
LEVEL P-14	0	0	0	0	0
LEVEL P-15	0	0	0	0	0
LEVEL P-16	0	0	0	0	0
LEVEL P-17	0	0	0	0	0
LEVEL P-18	0	0	0	0	0
LEVEL P-19	0	0	0	0	0
LEVEL P-20	0	0	0	0	0
LEVEL P-21	0	0	0	0	0
LEVEL P-22	0	0	0	0	0
LEVEL P-23	0	0	0	0	0
LEVEL P-24	0	0	0	0	0
LEVEL P-25	0	0	0	0	0
LEVEL P-26	0	0	0	0	0
LEVEL P-27	0	0	0	0	0
LEVEL P-28	0	0	0	0	0
LEVEL P-29	0	0	0	0	0
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LEVEL P-35	0	0	0	0	0
LEVEL P-36	0	0	0	0	0
LEVEL P-37	0	0	0	0	0
LEVEL P-38	0	0	0	0	0
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LEVEL P-59	0	0	0	0	0
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LEVEL P-80	0	0	0	0	0
LEVEL P-81	0	0	0	0	0
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LEVEL P-83	0	0	0	0	0
LEVEL P-84	0	0	0	0	0
LEVEL P-85	0	0	0	0	0
LEVEL P-86	0	0	0	0	0
LEVEL P-87	0	0	0	0	0
LEVEL P-88	0	0	0	0	0
LEVEL P-89	0	0	0	0	0
LEVEL P-90	0	0	0	0	0
LEVEL P-91	0	0	0	0	0
LEVEL P-92	0	0	0	0	0
LEVEL P-93	0	0	0	0	0
LEVEL P-94	0	0	0	0	0
LEVEL P-95	0	0	0	0	0
LEVEL P-96	0	0	0	0	0
LEVEL P-97	0	0	0	0	0
LEVEL P-98	0	0	0	0	0
LEVEL P-99	0	0	0	0	0
LEVEL P-100	0	0	0	0	0



LEGEND

COMMON
OFFICE
PARKING
RETAIL
SERVICE
STORAGE



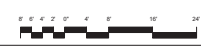
LEVEL P3 - FLOOR PLAN
 SCALE: 1/8" = 1'-0"

PARKING STALL SCHEDULE LEVEL P3

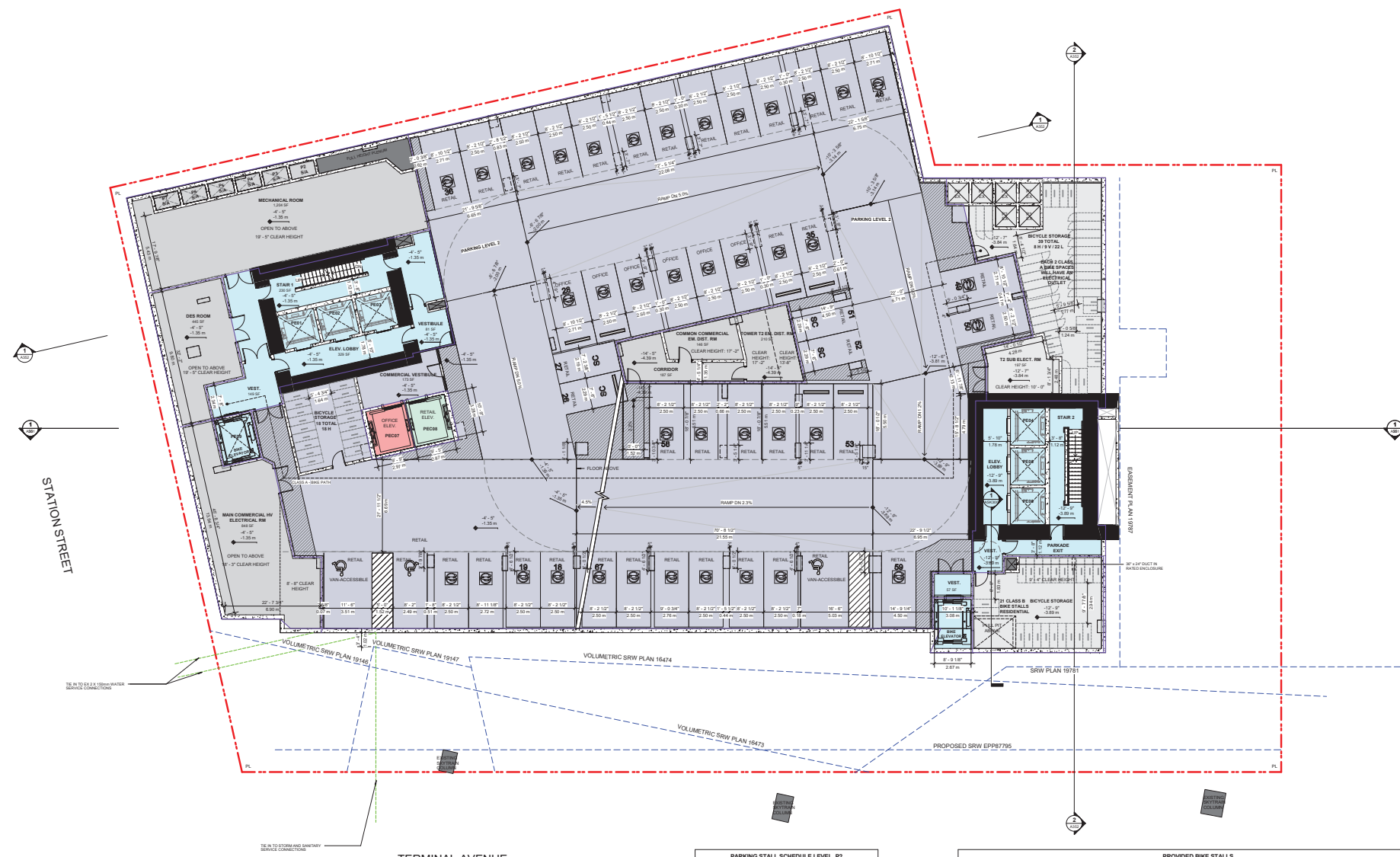
Level	Total Stalls	Class A Stalls	Class B Stalls	Class C Stalls	Total Parking Stalls
LEVEL P3	263	191	13	59	363

PROVIDED BIKE STALLS

Level	Horizontal Bike Stalls (2' x 6')	Vertical Bike Stalls (2' x 5'-0") min. 30% of Class A Stalls = max. 211 Stalls	CLASS A Stalls			CLASS B Stalls	
			Stalls (2' x 6') min. 10% of Class A Stalls = max. 561 Stalls	Stalls (2' x 6') min. 10% of Class A Stalls = min. 35	Overlaps Bike Stalls (2' x 6') min. 2% of req. Class A Stalls = min. 41	External Visitor Bike Stalls	Total
LEVEL P3	20	191	62	24	22	0	277
LEVEL P4	20	191	62	24	22	0	277
LEVEL P5	20	191	62	24	22	0	277
LEVEL P6	20	191	62	24	22	0	277
LEVEL P7	20	191	62	24	22	0	277
LEVEL P8	20	191	62	24	22	0	277
LEVEL P9	20	191	62	24	22	0	277
LEVEL P10	20	191	62	24	22	0	277
LEVEL P11	20	191	62	24	22	0	277
LEVEL P12	20	191	62	24	22	0	277
LEVEL P13	20	191	62	24	22	0	277
LEVEL P14	20	191	62	24	22	0	277
LEVEL P15	20	191	62	24	22	0	277
LEVEL P16	20	191	62	24	22	0	277
LEVEL P17	20	191	62	24	22	0	277
LEVEL P18	20	191	62	24	22	0	277
LEVEL P19	20	191	62	24	22	0	277
LEVEL P20	20	191	62	24	22	0	277
LEVEL P21	20	191	62	24	22	0	277
LEVEL P22	20	191	62	24	22	0	277
LEVEL P23	20	191	62	24	22	0	277
LEVEL P24	20	191	62	24	22	0	277
LEVEL P25	20	191	62	24	22	0	277
LEVEL P26	20	191	62	24	22	0	277
LEVEL P27	20	191	62	24	22	0	277
LEVEL P28	20	191	62	24	22	0	277
LEVEL P29	20	191	62	24	22	0	277
LEVEL P30	20	191	62	24	22	0	277
LEVEL P31	20	191	62	24	22	0	277
LEVEL P32	20	191	62	24	22	0	277
LEVEL P33	20	191	62	24	22	0	277
LEVEL P34	20	191	62	24	22	0	277
LEVEL P35	20	191	62	24	22	0	277
LEVEL P36	20	191	62	24	22	0	277
LEVEL P37	20	191	62	24	22	0	277
LEVEL P38	20	191	62	24	22	0	277
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LEVEL P46	20	191	62	24	22	0	277
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LEVEL P50	20	191	62	24	22	0	277
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LEVEL P52	20	191	62	24	22	0	277
LEVEL P53	20	191	62	24	22	0	277
LEVEL P54	20	191	62	24	22	0	277
LEVEL P55	20	191	62	24	22	0	277
LEVEL P56	20	191	62	24	22	0	277
LEVEL P57	20	191	62	24	22	0	277
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LEVEL P59	20	191	62	24	22	0	277
LEVEL P60	20	191	62	24	22	0	277
LEVEL P61	20	191	62	24	22	0	277
LEVEL P62	20	191	62	24	22	0	277
LEVEL P63	20	191	62	24	22	0	277
LEVEL P64	20	191	62	24	22	0	277
LEVEL P65	20	191	62	24	22	0	277
LEVEL P66	20	191	62	24	22	0	277
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LEVEL P68	20	191	62	24	22	0	277
LEVEL P69	20	191	62	24	22	0	277
LEVEL P70	20	191	62	24	22	0	277
LEVEL P71	20	191	62	24	22	0	277
LEVEL P72	20	191	62	24	22	0	277
LEVEL P73	20	191	62	24	22	0	277
LEVEL P74	20	191	62	24	22	0	277
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LEVEL P81	20	191	62	24	22	0	277
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LEVEL P84	20	191	62	24	22	0	277
LEVEL P85	20	191	62	24	22	0	277
LEVEL P86	20	191	62	24	22	0	277
LEVEL P87	20	191	62	24	22	0	277
LEVEL P88	20	191	62	24	22	0	277
LEVEL P89	20	191	62	24	22	0	277
LEVEL P90	20	191	62	24	22	0	277
LEVEL P91	20	191	62	24	22	0	277
LEVEL P92	20	191	62	24	22	0	277
LEVEL P93	20	191	62	24	22	0	277
LEVEL P94	20	191	62	24	22	0	277
LEVEL P95	20	191	62	24	22	0	277
LEVEL P96	20	191	62	24	22	0	277
LEVEL P97	20	191	62	24	22	0	277
LEVEL P98	20	191	62	24	22	0	277
LEVEL P99	20	191	62	24	22	0	277
LEVEL P100	20	191	62	24	22	0	277



- LEGEND
- COMMON
 - OFFICE
 - PARKING
 - RETAIL
 - SERVICE
 - STORAGE



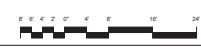
LEVEL P2 - FLOOR PLAN
 SCALE: 1/8"=1'-0"

PARKING STALL SCHEDULE LEVEL P2

Level	Total Required Stalls (2' x 8')	Total Required Stalls (2' x 10')	Total Required Stalls (2' x 12')	Total Required Stalls (2' x 14')	Total Parking Stalls
LEVEL P2	191	191	191	191	764

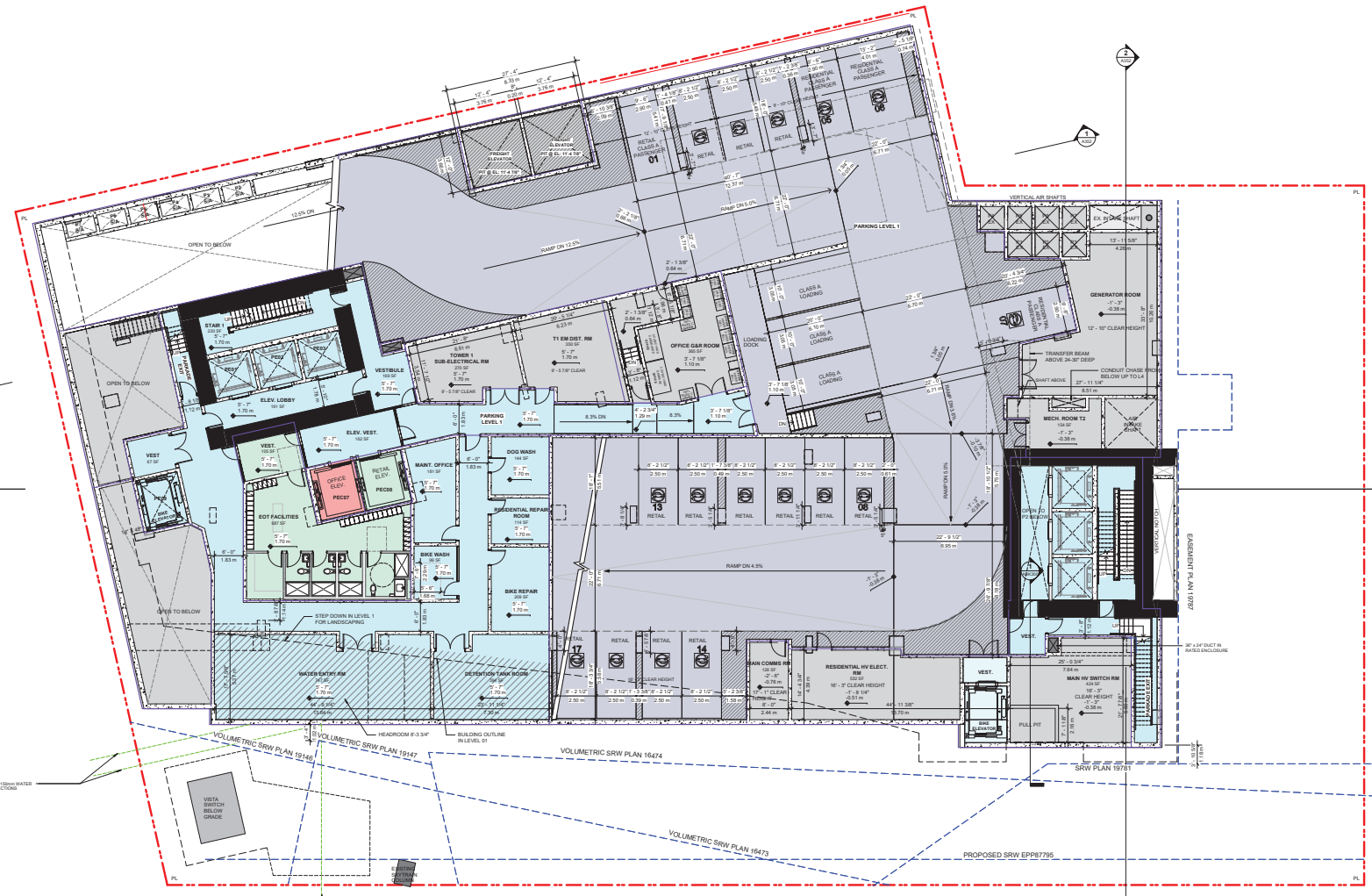
PROVIDED BIKE STALLS

Level	CLASS A Stalls				Total	CLASS B Stalls
	Horizontal Bike Stalls (2' x 8')	Vertical Bike Stalls (2' x 8')	Stack Bike Stalls (2' x 8')	Other		
LEVEL P2	203	191	33	58	485	37
LEVEL P3	33	1	63	24	121	0
LEVEL P4	33	1	63	24	121	0
LEVEL P5	33	1	63	24	121	0
LEVEL P6	33	1	63	24	121	0
LEVEL P7	33	1	63	24	121	0
LEVEL P8	33	1	63	24	121	0
LEVEL P9	33	1	63	24	121	0
LEVEL P10	33	1	63	24	121	0
LEVEL P11	33	1	63	24	121	0
LEVEL P12	33	1	63	24	121	0
LEVEL P13	33	1	63	24	121	0
LEVEL P14	33	1	63	24	121	0
LEVEL P15	33	1	63	24	121	0
LEVEL P16	33	1	63	24	121	0
LEVEL P17	33	1	63	24	121	0
LEVEL P18	33	1	63	24	121	0
LEVEL P19	33	1	63	24	121	0
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LEVEL P21	33	1	63	24	121	0
LEVEL P22	33	1	63	24	121	0
LEVEL P23	33	1	63	24	121	0
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LEVEL P25	33	1	63	24	121	0
LEVEL P26	33	1	63	24	121	0
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LEVEL P30	33	1	63	24	121	0
LEVEL P31	33	1	63	24	121	0
LEVEL P32	33	1	63	24	121	0
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LEVEL P35	33	1	63	24	121	0
LEVEL P36	33	1	63	24	121	0
LEVEL P37	33	1	63	24	121	0
LEVEL P38	33	1	63	24	121	0
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LEVEL P41	33	1	63	24	121	0
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LEVEL P61	33	1	63	24	121	0
LEVEL P62	33	1	63	24	121	0
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LEVEL P64	33	1	63	24	121	0
LEVEL P65	33	1	63	24	121	0
LEVEL P66	33	1	63	24	121	0
LEVEL P67	33	1	63	24	121	0
LEVEL P68	33	1	63	24	121	0
LEVEL P69	33	1	63	24	121	0
LEVEL P70	33	1	63	24	121	0
LEVEL P71	33	1	63	24	121	0
LEVEL P72	33	1	63	24	121	0
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LEVEL P95	33	1	63	24	121	0
LEVEL P96	33	1	63	24	121	0
LEVEL P97	33	1	63	24	121	0
LEVEL P98	33	1	63	24	121	0
LEVEL P99	33	1	63	24	121	0
LEVEL P100	33	1	63	24	121	0



LEGEND

COMMON
OFFICE
PARKING
RETAIL
SERVICE



STATION STREET

TERMINAL AVENUE

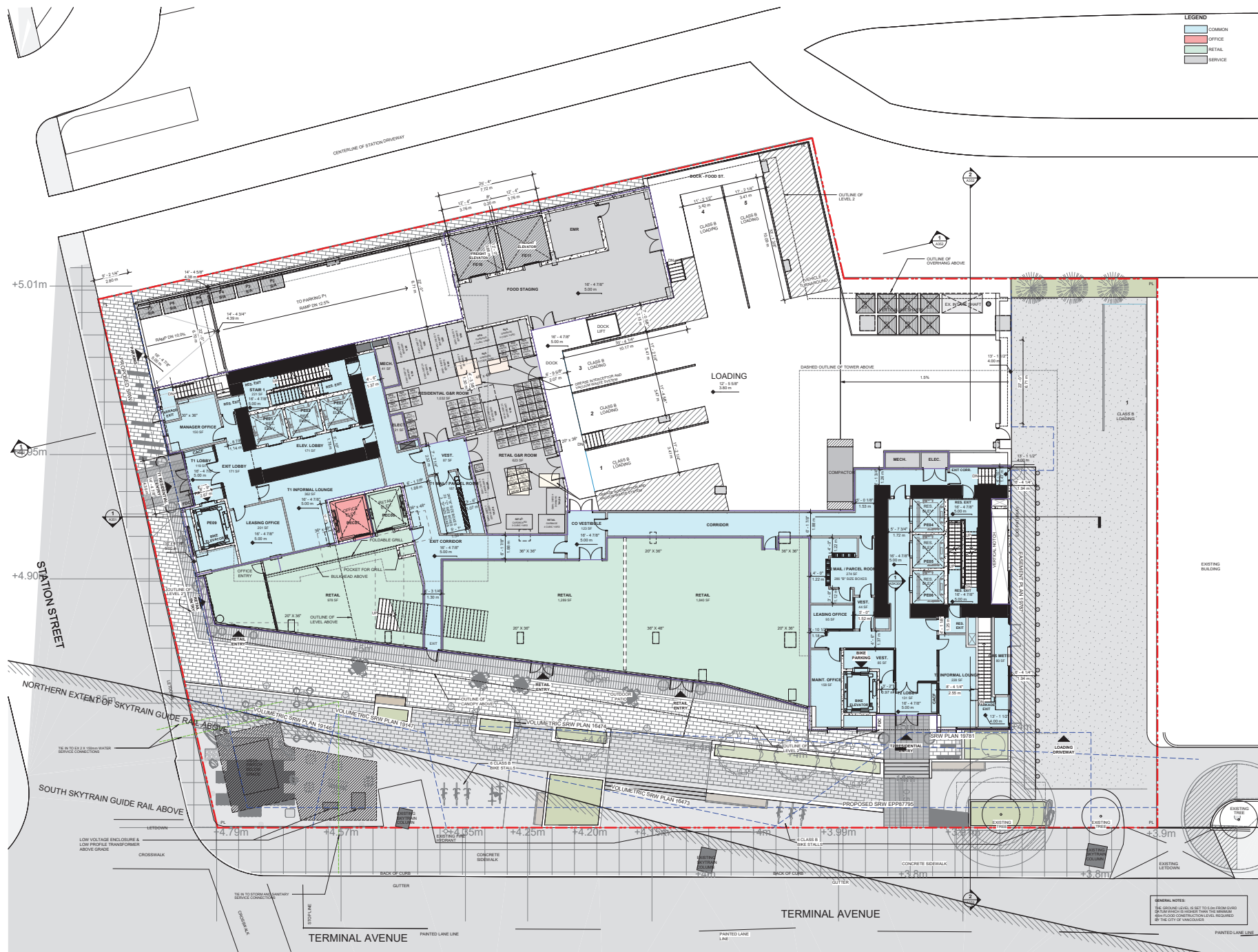
LEVEL P1 - FLOOR PLAN
 SCALE: 1/8" = 1'-0"

PARKING STALL SCHEDULE LEVEL P1

Level	Total Reserved Stalls	Total Visitor Stalls	Total Office Stalls	Transit Loading Stalls	Total Office Stalls	Total Parking Stalls	Loading Stalls
LEVEL P1	203	191	332	58	192	247	24

PROVIDED BIKE STALLS

Level	CLASS A Stalls		CLASS B Stalls		Total
	Horizontal Bike Stalls (2' x 4')	Vertical Bike Stalls (2' x 4')	Horizontal Bike Stalls (2' x 4')	Vertical Bike Stalls (2' x 4')	
LEVEL P1	30	30	30	30	120
LEVEL P2	30	30	30	30	120
LEVEL P3	30	30	30	30	120
LEVEL P4	30	30	30	30	120
LEVEL P5	30	30	30	30	120
LEVEL P6	30	30	30	30	120
LEVEL P7	30	30	30	30	120
LEVEL P8	30	30	30	30	120
LEVEL P9	30	30	30	30	120
LEVEL P10	30	30	30	30	120
LEVEL P11	30	30	30	30	120
LEVEL P12	30	30	30	30	120
LEVEL P13	30	30	30	30	120
LEVEL P14	30	30	30	30	120
LEVEL P15	30	30	30	30	120
LEVEL P16	30	30	30	30	120
LEVEL P17	30	30	30	30	120
LEVEL P18	30	30	30	30	120
LEVEL P19	30	30	30	30	120
LEVEL P20	30	30	30	30	120
LEVEL P21	30	30	30	30	120
LEVEL P22	30	30	30	30	120
LEVEL P23	30	30	30	30	120
LEVEL P24	30	30	30	30	120
LEVEL P25	30	30	30	30	120
LEVEL P26	30	30	30	30	120
LEVEL P27	30	30	30	30	120
LEVEL P28	30	30	30	30	120
LEVEL P29	30	30	30	30	120
LEVEL P30	30	30	30	30	120
LEVEL P31	30	30	30	30	120
LEVEL P32	30	30	30	30	120
LEVEL P33	30	30	30	30	120
LEVEL P34	30	30	30	30	120
LEVEL P35	30	30	30	30	120
LEVEL P36	30	30	30	30	120
LEVEL P37	30	30	30	30	120
LEVEL P38	30	30	30	30	120
LEVEL P39	30	30	30	30	120
LEVEL P40	30	30	30	30	120
LEVEL P41	30	30	30	30	120
LEVEL P42	30	30	30	30	120
LEVEL P43	30	30	30	30	120
LEVEL P44	30	30	30	30	120
LEVEL P45	30	30	30	30	120
LEVEL P46	30	30	30	30	120
LEVEL P47	30	30	30	30	120
LEVEL P48	30	30	30	30	120
LEVEL P49	30	30	30	30	120
LEVEL P50	30	30	30	30	120
LEVEL P51	30	30	30	30	120
LEVEL P52	30	30	30	30	120
LEVEL P53	30	30	30	30	120
LEVEL P54	30	30	30	30	120
LEVEL P55	30	30	30	30	120
LEVEL P56	30	30	30	30	120
LEVEL P57	30	30	30	30	120
LEVEL P58	30	30	30	30	120
LEVEL P59	30	30	30	30	120
LEVEL P60	30	30	30	30	120
LEVEL P61	30	30	30	30	120
LEVEL P62	30	30	30	30	120
LEVEL P63	30	30	30	30	120
LEVEL P64	30	30	30	30	120
LEVEL P65	30	30	30	30	120
LEVEL P66	30	30	30	30	120
LEVEL P67	30	30	30	30	120
LEVEL P68	30	30	30	30	120
LEVEL P69	30	30	30	30	120
LEVEL P70	30	30	30	30	120
LEVEL P71	30	30	30	30	120
LEVEL P72	30	30	30	30	120
LEVEL P73	30	30	30	30	120
LEVEL P74	30	30	30	30	120
LEVEL P75	30	30	30	30	120
LEVEL P76	30	30	30	30	120
LEVEL P77	30	30	30	30	120
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LEVEL P79	30	30	30	30	120
LEVEL P80	30	30	30	30	120
LEVEL P81	30	30	30	30	120
LEVEL P82	30	30	30	30	120
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LEVEL P89	30	30	30	30	120
LEVEL P90	30	30	30	30	120
LEVEL P91	30	30	30	30	120
LEVEL P92	30	30	30	30	120
LEVEL P93	30	30	30	30	120
LEVEL P94	30	30	30	30	120
LEVEL P95	30	30	30	30	120
LEVEL P96	30	30	30	30	120
LEVEL P97	30	30	30	30	120
LEVEL P98	30	30	30	30	120
LEVEL P99	30	30	30	30	120
LEVEL P100	30	30	30	30	120

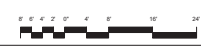


LEGEND

- COMMON
- OFFICE
- RETAIL
- SERVICE

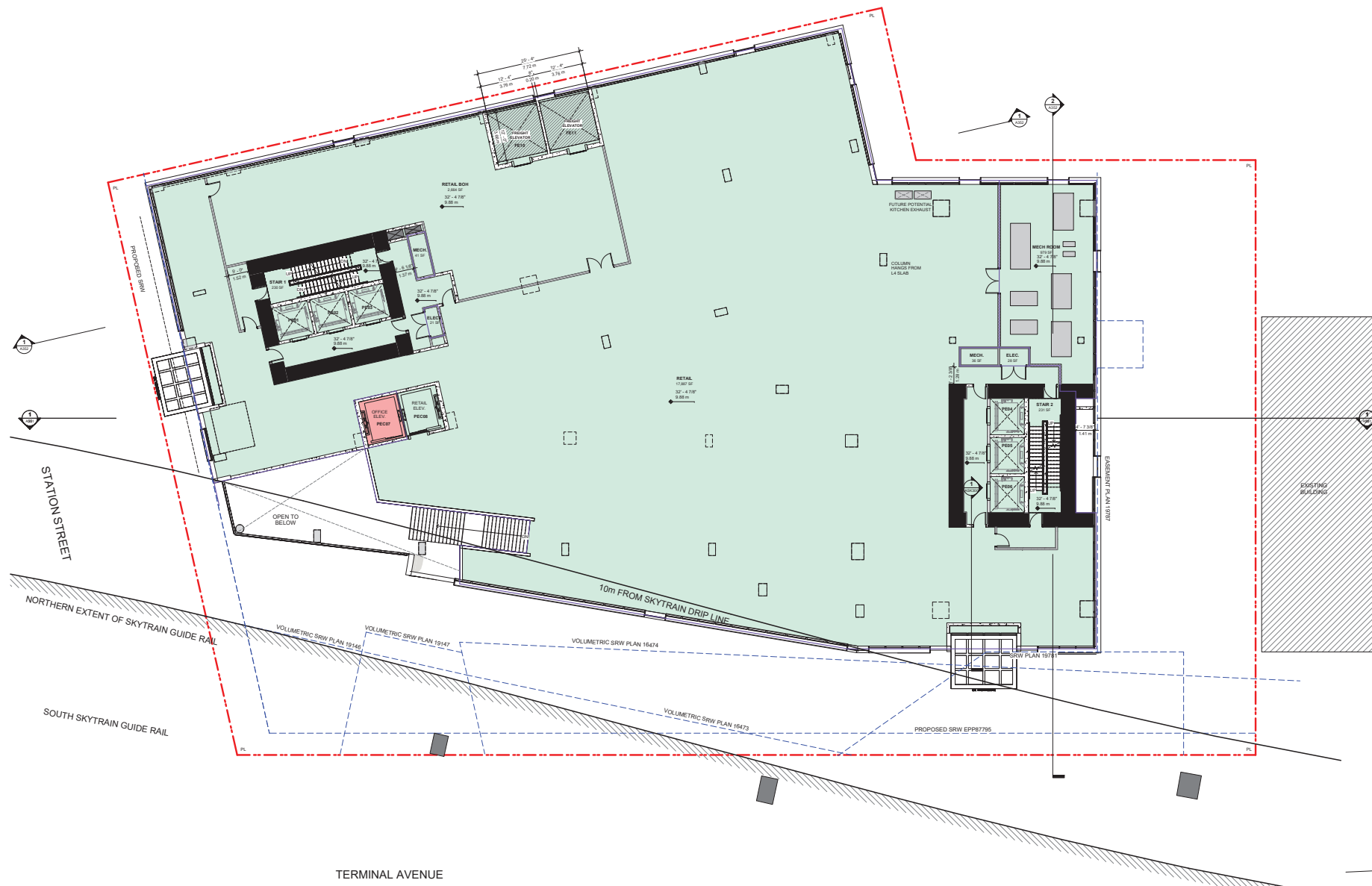
LEVEL 1 - FLOOR PLAN
SCALE: 1/8" = 1'-0"

GENERAL NOTES:
1. THE FINISH LEVEL IS SET TO 5.0 FROM GRID
2. ALL FINISHES TO BE SHOWN WITH THE FINISH
3. ALL FLOOR CONSTRUCTION LEVELS REQUIRED
BY THE CITY OF VANCOUVER.

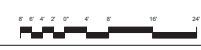


1220 STATION STREET
1220 Station Street
Vancouver, BC
Project
FLOOR PLAN LEVEL 1

LEGEND
OFFICE
RETAIL



LEVEL 2 - FLOOR PLAN
SCALE: 1/8"=1'-0"



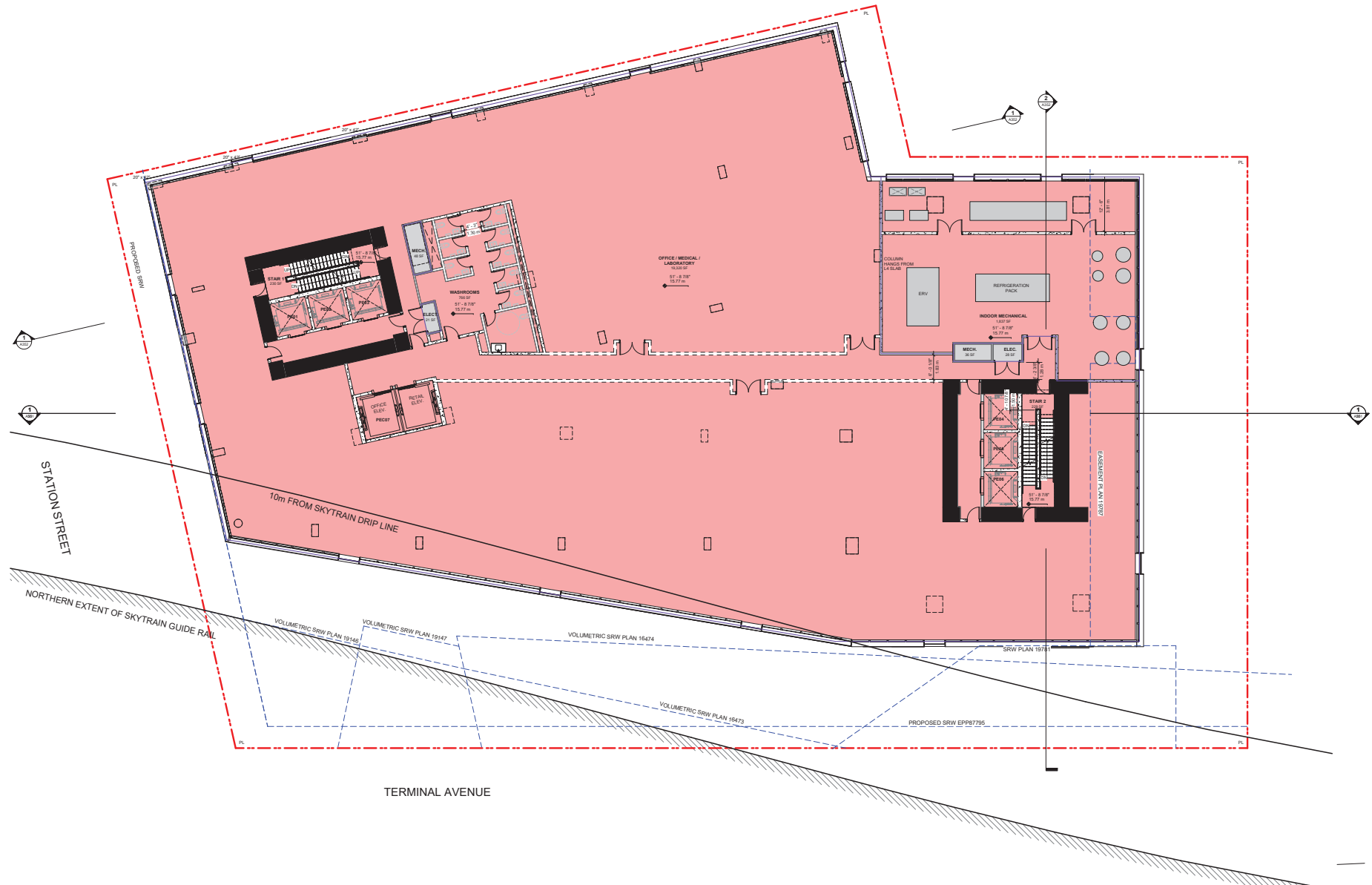
1220 STATION STREET
1220 Station Street
Vancouver, BC
Project
FLOOR PLAN LEVEL 2

Sheet: 1220-02
Date: 2024-04-14
A210



Revision: YYYYMMDD
 1 ISSUED FOR PERMITTING 2024-03-11
 2 ISSUED FOR PERMITTING 2024-06-14
 3 ISSUED FOR PERMITTING 2024-06-14

LEGEND
 OFFICE
 SERVICE

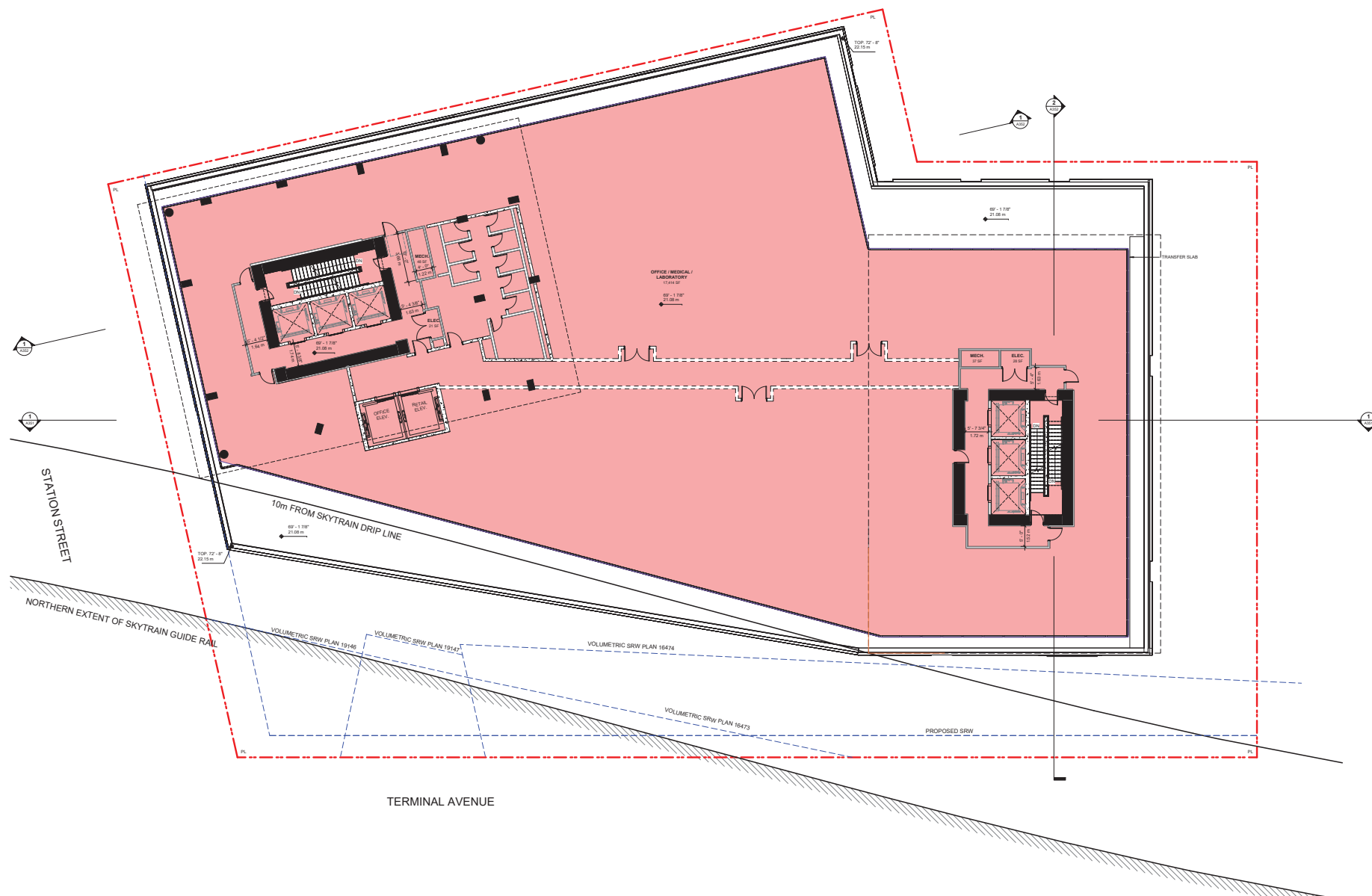


LEVEL 3 - FLOOR PLAN
 SCALE: 1/8" = 1'-0"

1220 STATION STREET
 1220 Station Street
 Vancouver, BC
 Project
 FLOOR PLAN LEVEL 3
 Date: 2024-03-11
 Scale: 1/8" = 1'-0"
 Project: 202403
 Sheet: **A211**



LEGEND
OFFICE



LEVEL 4 - FLOOR PLAN
SCALE: 1/8" = 1'-0"

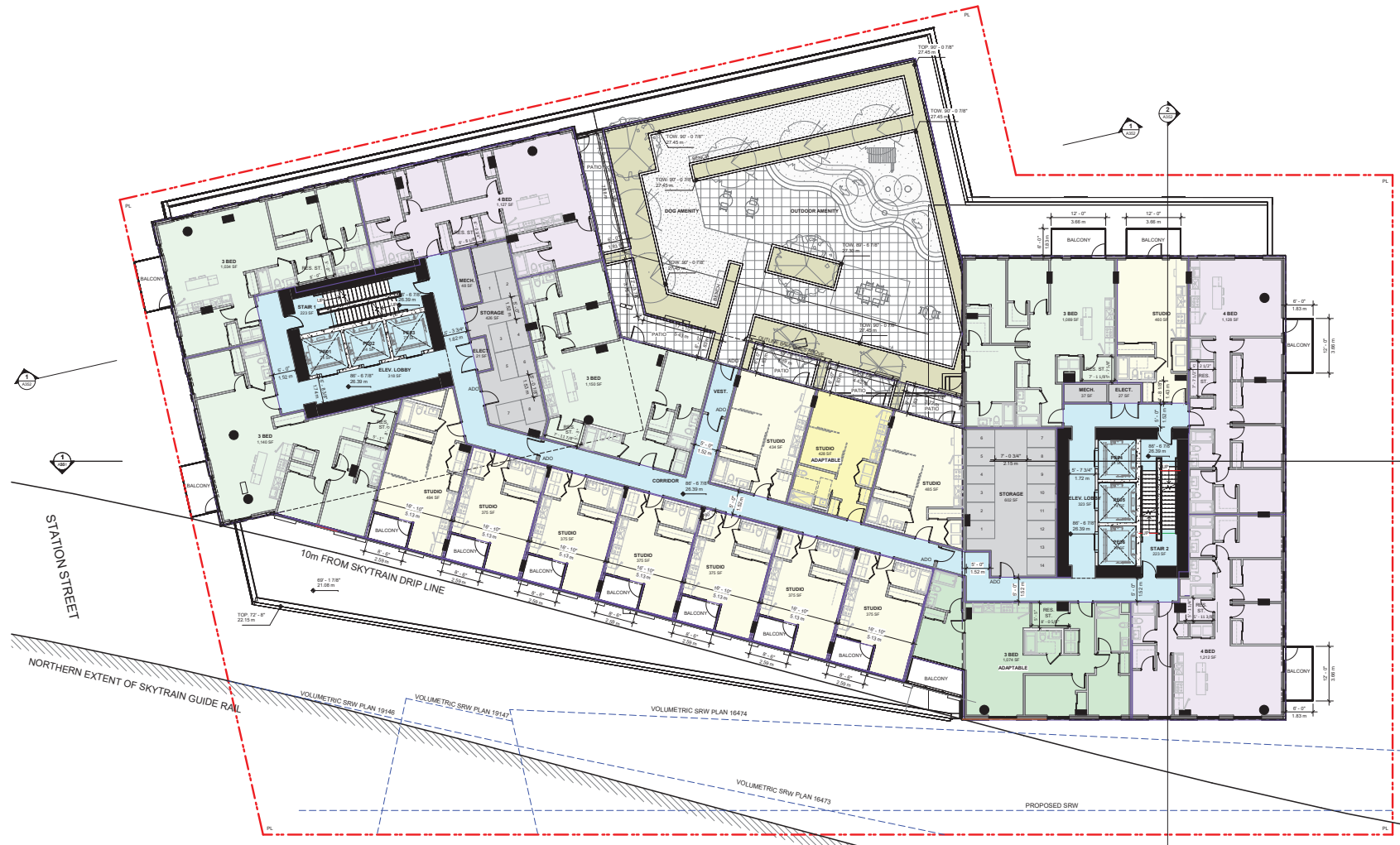
1220 STATION STREET
Vancouver, BC
Project
FLOOR PLAN LEVEL 4



Sheet: A212

NOTE:
ALL UNITS FROM LEVELS 6-10
ARE BELOW MARKET RENTAL

- LEGEND
- 3 BED
 - 4 BED
 - ADAPTABLE 3 BED
 - ADAPTABLE STUDIO
 - COMMON
 - SERVICE
 - STUDIO



STATION STREET

TERMINAL AVENUE

LEVEL 5
SCALE: 1/8" = 1'-0"



1220 STATION STREET

 1220 Station Street
 Vancouver, BC
 Project:
 FLOOR PLAN LEVEL 5

Sheet: A213
 Date: 2024-08-14
 Author: [Name]
 Check: [Name]

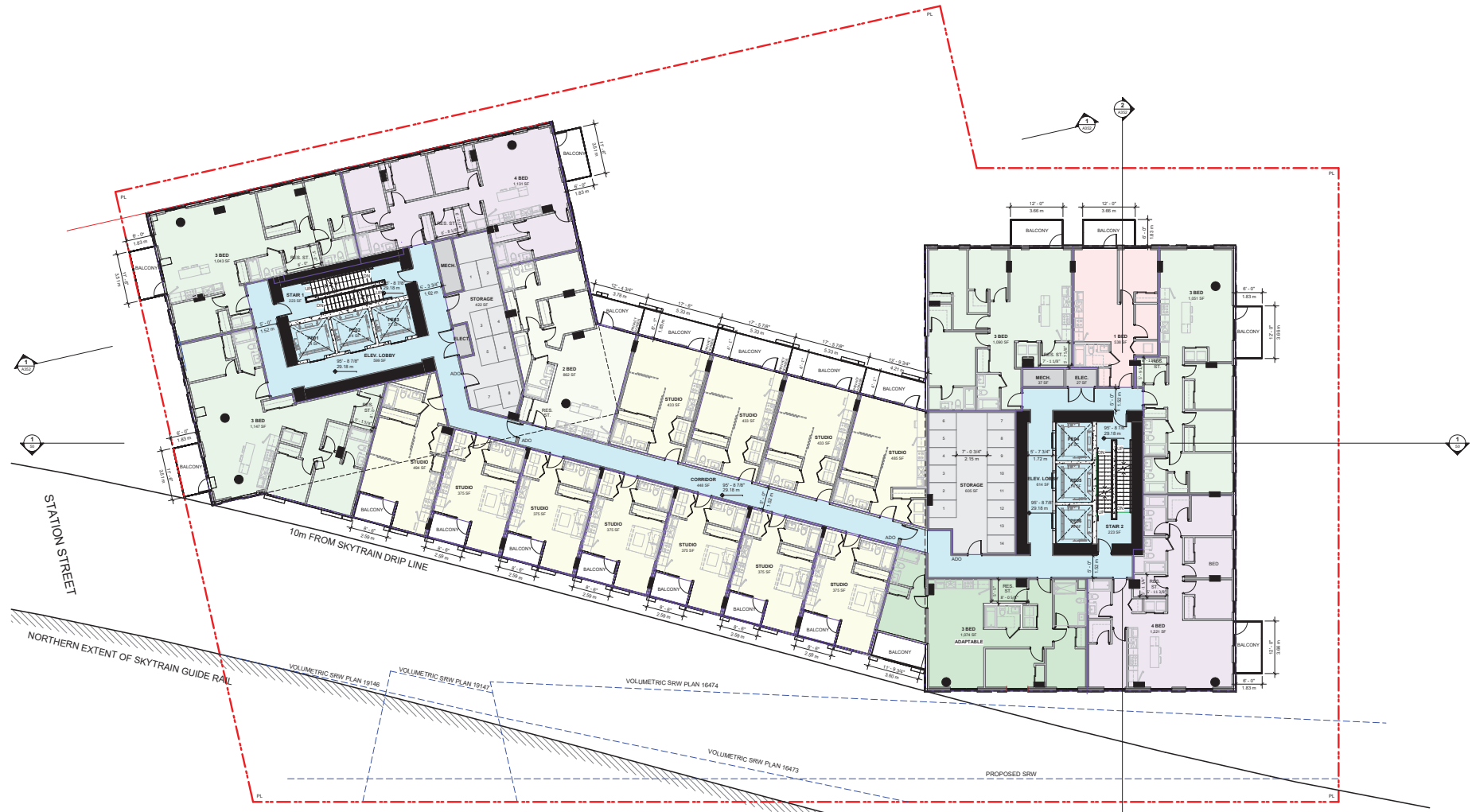


Revision: YYYYMMDD
 1 ISSUED FOR PERMITS 2024-01-11
 2 ISSUED FOR PERMITS 2024-08-14
 3 ISSUED FOR PERMITS 2025-04-04

NOTE:
 ALL UNITS FROM LEVELS 6-10
 ARE BELOW MARKET RENTAL

LEGEND

[Light Green Box]	3 BED
[Light Blue Box]	2 BED
[Light Purple Box]	3 BED
[Light Yellow Box]	2 BED
[Light Green Box]	ADAPTABLE 3 BED
[Light Blue Box]	COMMON
[Light Purple Box]	SERVICE
[Light Yellow Box]	STORAGE
[Light Green Box]	STUDIO



1 LEVEL 6-8 - FLOOR PLAN
 SCALE: 1/8" = 1'-0"



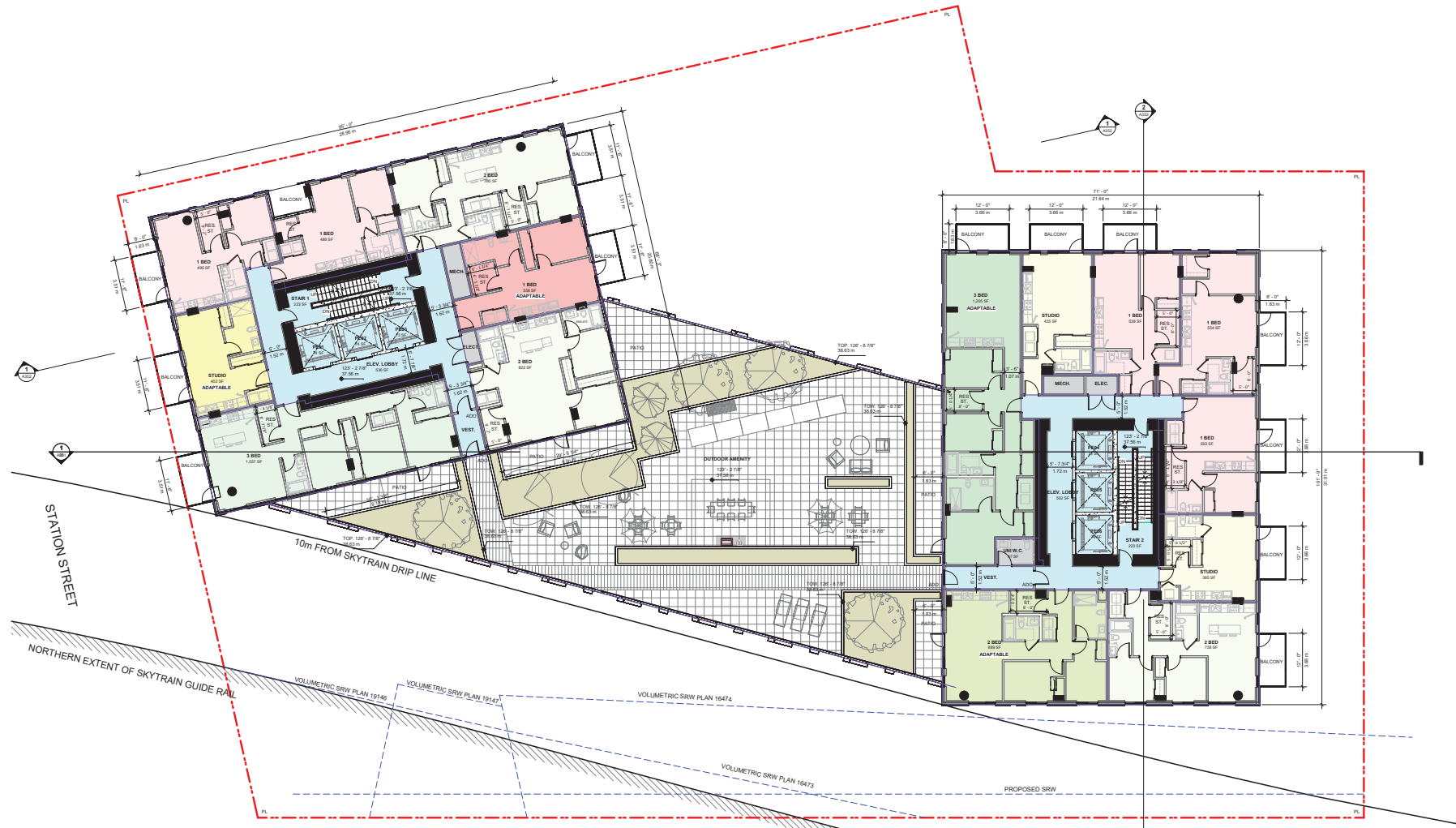


Revision: YYYYMMDD
 1 ISSUED FOR PERMITS 2024-01-11
 2 ISSUED FOR PERMITS 2024-04-14
 3 ISSUED FOR PERMITS 2024-04-14

NOTE:
 ALL UNITS FROM LEVELS #1-10
 ARE BELOW MARKET RENTAL

LEGEND

[Light Green]	1 BED
[Light Blue]	2 BED
[Light Yellow]	3 BED
[Light Purple]	ADAPTABLE 1 BED
[Light Orange]	ADAPTABLE 2 BED
[Light Red]	ADAPTABLE 3 BED
[Light Green]	ADAPTABLE STUDIO
[Light Blue]	COMMON
[Light Purple]	SERVICE
[Light Yellow]	STUDIO



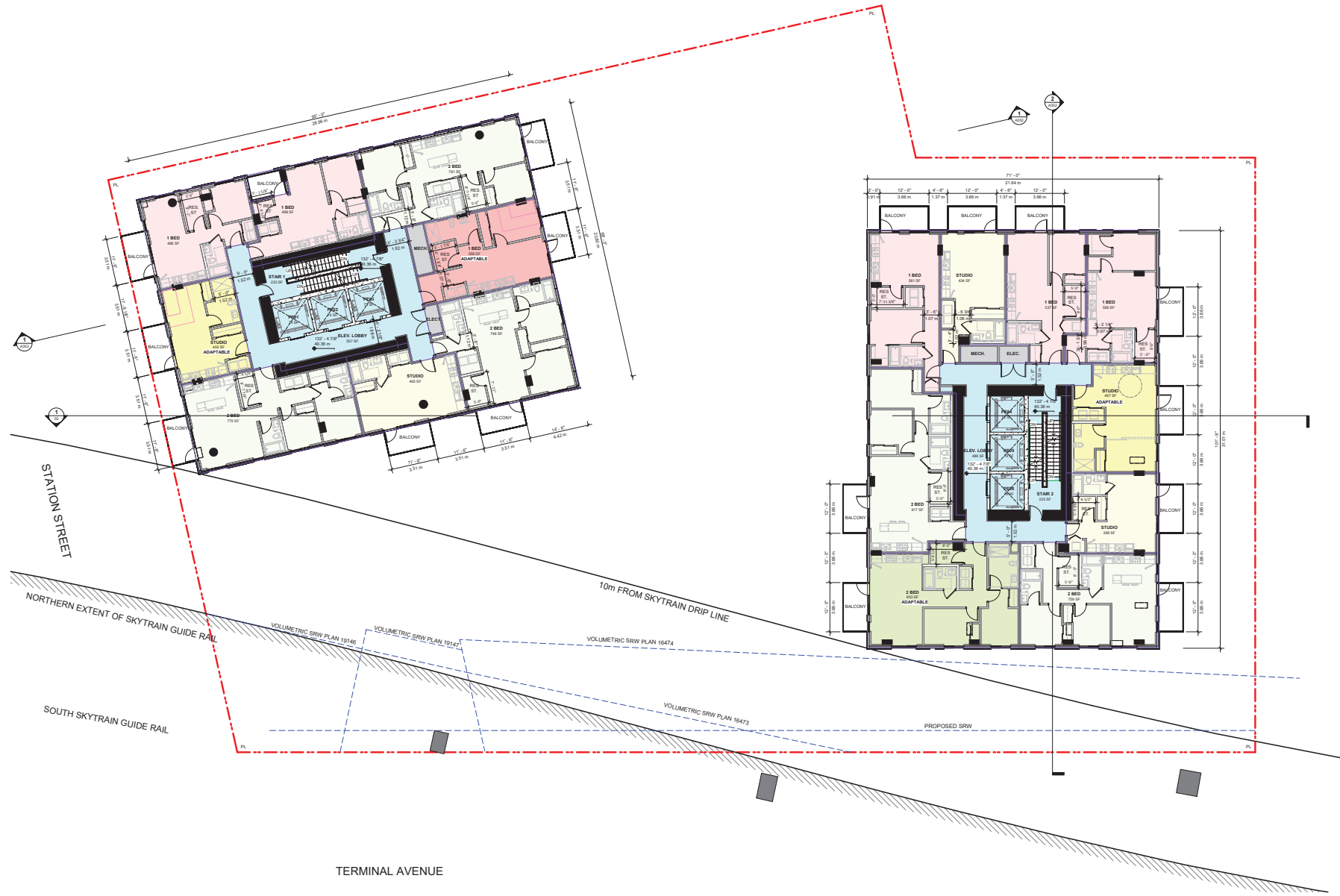
2 LEVEL 9 - FLOOR PLAN
 SCALE: 1/8" = 1'-0"



NOTE:
 ALL UNITS FROM LEVELS #10
 ARE BELOW MARKET RENTAL

LEGEND

[Pink Box]	1 BED
[Light Pink Box]	2 BED
[Red Box]	ADAPTABLE 1 BED
[Light Green Box]	ADAPTABLE 2 BED
[Yellow Box]	ADAPTABLE STUDIO
[Blue Box]	COMMON
[Light Blue Box]	SERVICE
[Green Box]	STUDIO



1 LEVELS 10
 SCALE: 1/8" = 1'-0"



1220 STATION STREET

1220 Station Street
 Vancouver, BC

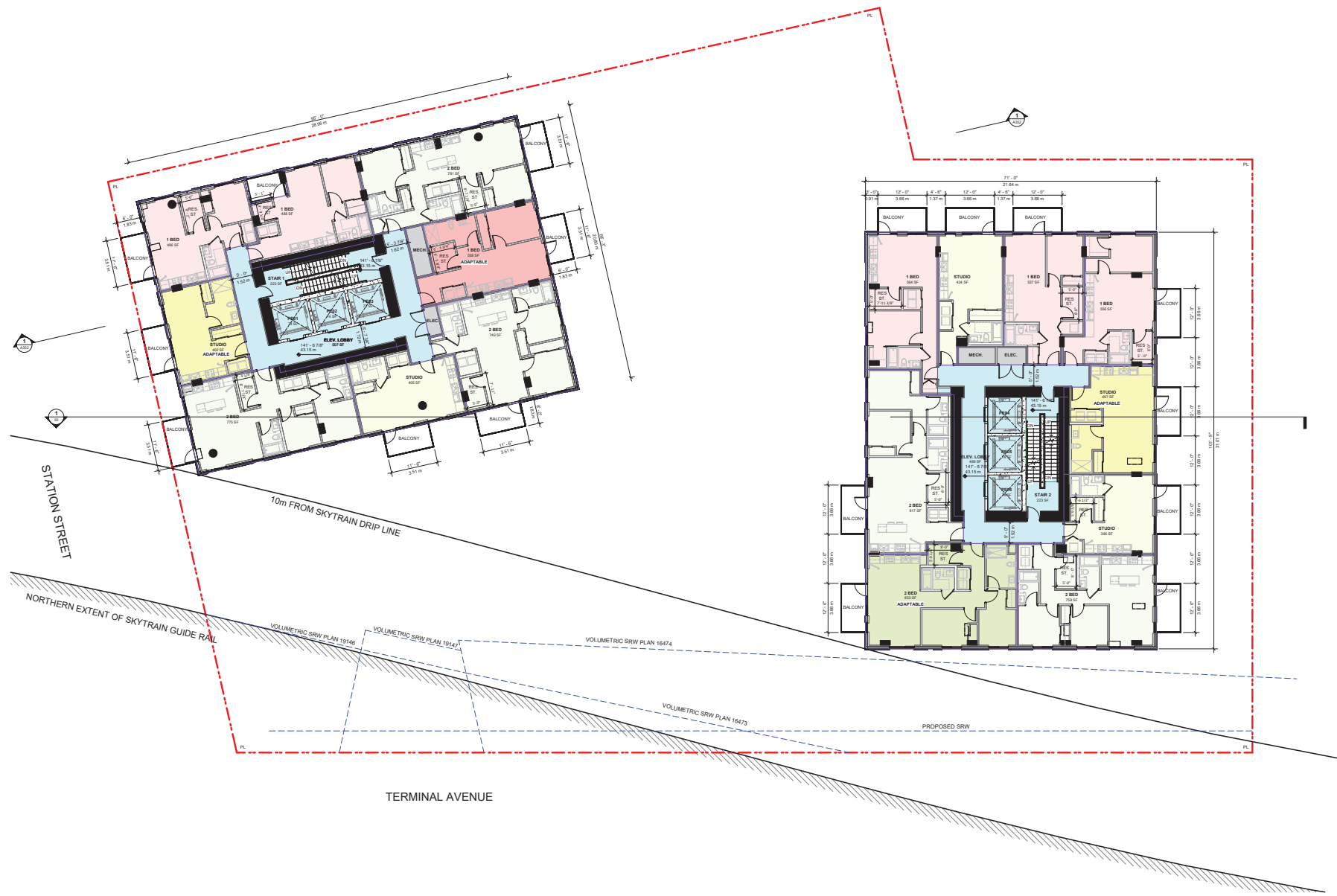
FLOOR PLAN LEVELS
 - TYP. LEVELS 10

DATE: 2025-04-02

A216

NOTE:
 ALL UNITS FROM LEVELS #10
 ARE BELOW MARKET RENTAL

- LEGEND
- 1 BED
 - 2 BED
 - ADAPTABLE 1 BED
 - ADAPTABLE 2 BED
 - ADAPTABLE STUDIO
 - COMMON
 - SERVICE
 - STUDIO

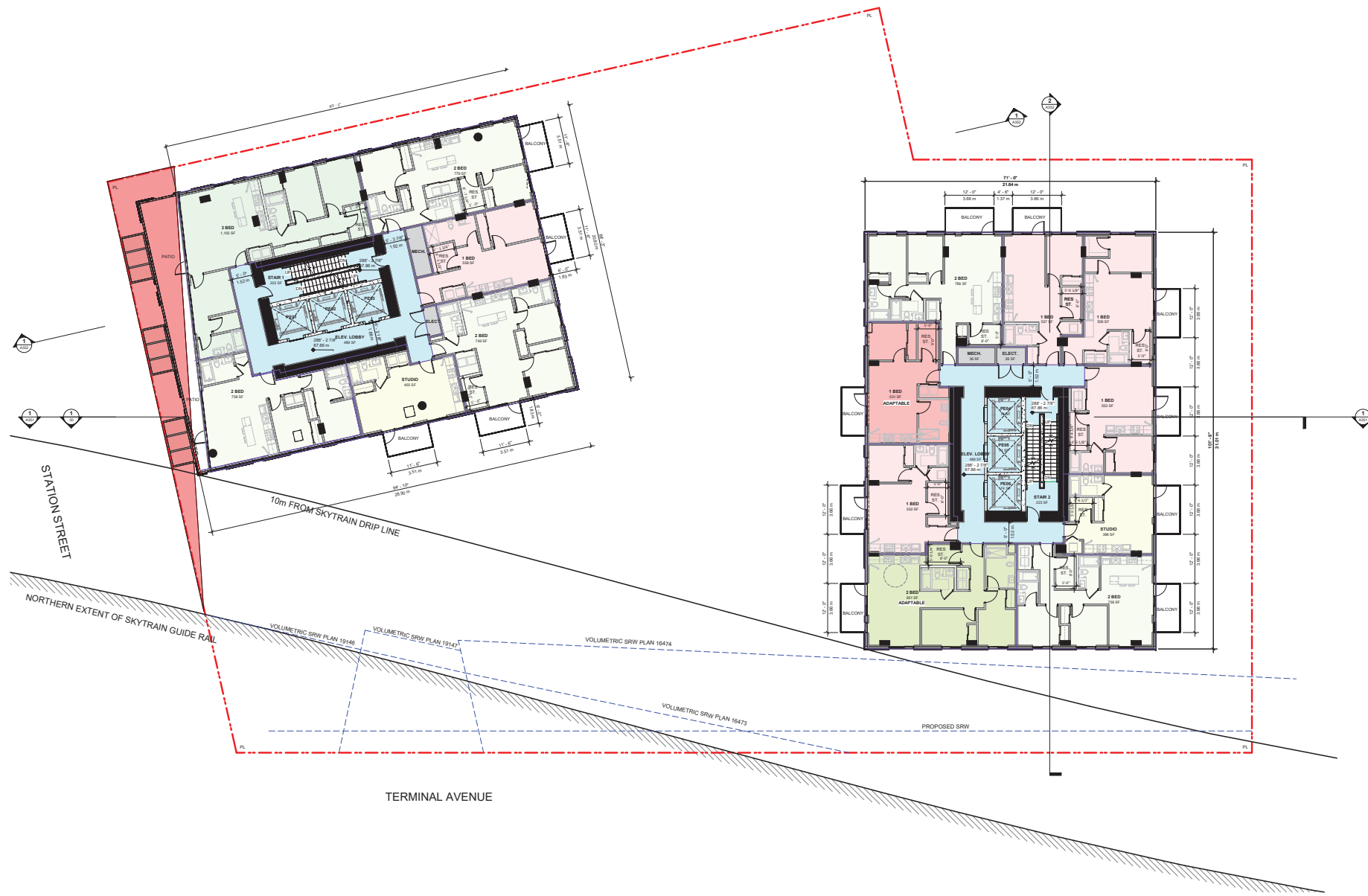


1 LEVEL 11 TO 26 FLOOR PLAN
 SCALE: 1/8" = 1'-0"

NOTE:
 ALL UNITS FROM LEVELS #10
 ARE BELOW MARKET RENTAL

LEGEND

[Light Green Box]	1 BED
[Light Blue Box]	2 BED
[Light Yellow Box]	3 BED
[Pink Box]	ADAPTABLE 1 BED
[Light Green Box]	ADAPTABLE 2 BED
[Light Blue Box]	COMMON
[Light Yellow Box]	SERVICE
[Light Green Box]	STUDIO



1 LEVEL 27 - FLOOR PLAN
 SCALE: 1/8" = 1'-0"



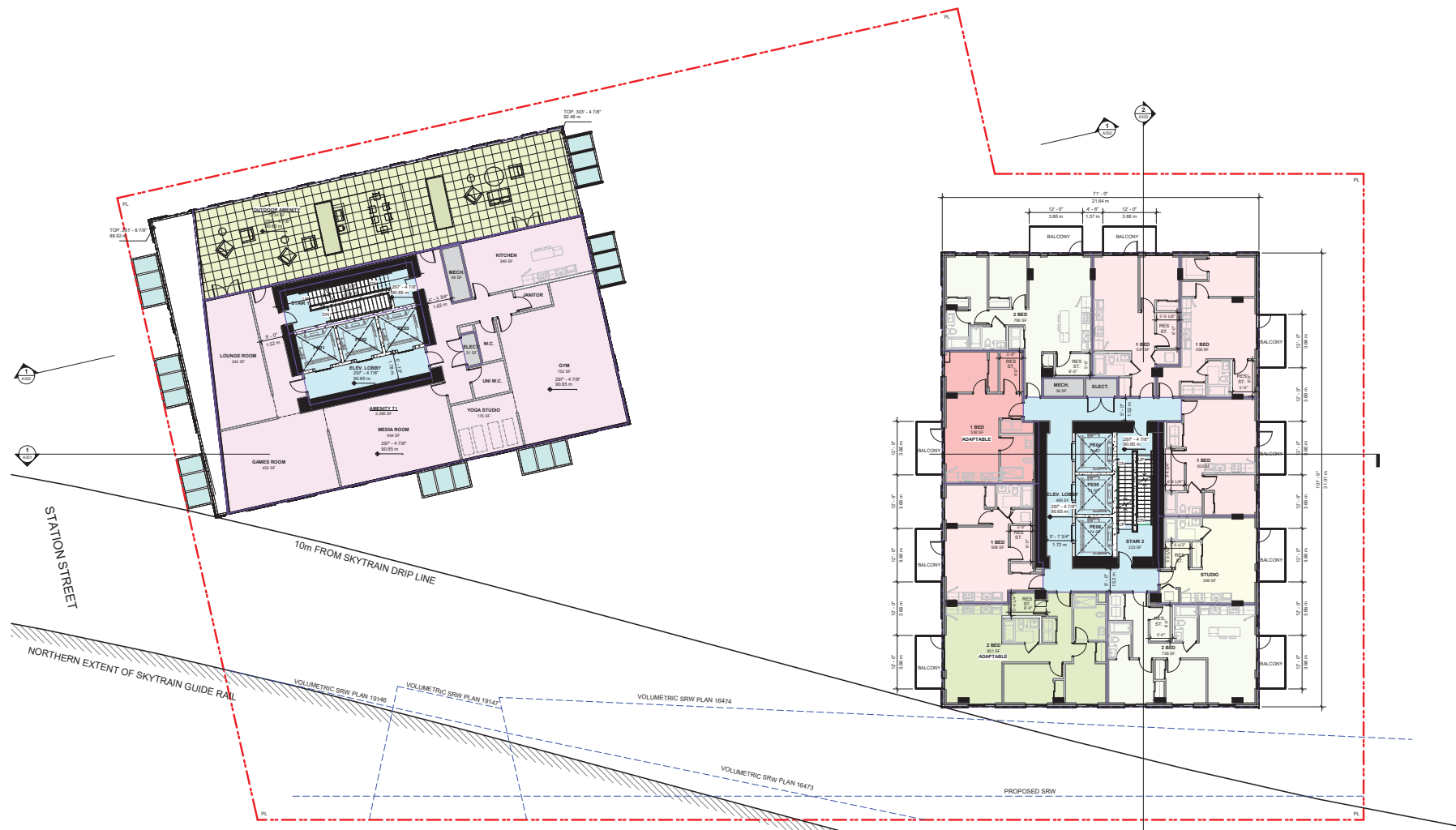
1220 STATION STREET
 1220 Station Street
 Vancouver, BC
 Project
 FLOOR PLAN LEVELS
 - LEVEL 27

Sheet: 102-001
 Date: 2025-04-03
 Author: [Name]
 A218



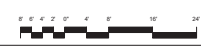
NOTE:
 ALL UNITS FROM LEVELS #10
 ARE BELOW MARKET RENTAL

- LEGEND
- 1 BED
 - 2 BED
 - ADAPTABLE 1 BED
 - ADAPTABLE 2 BED
 - AMENITY T1
 - COMMON
 - OUTDOOR AMENITY
 - SERVICE
 - STUDIO



1 LEVEL 28 - FLOOR PLAN
 SCALE: 1/8" = 1'-0"

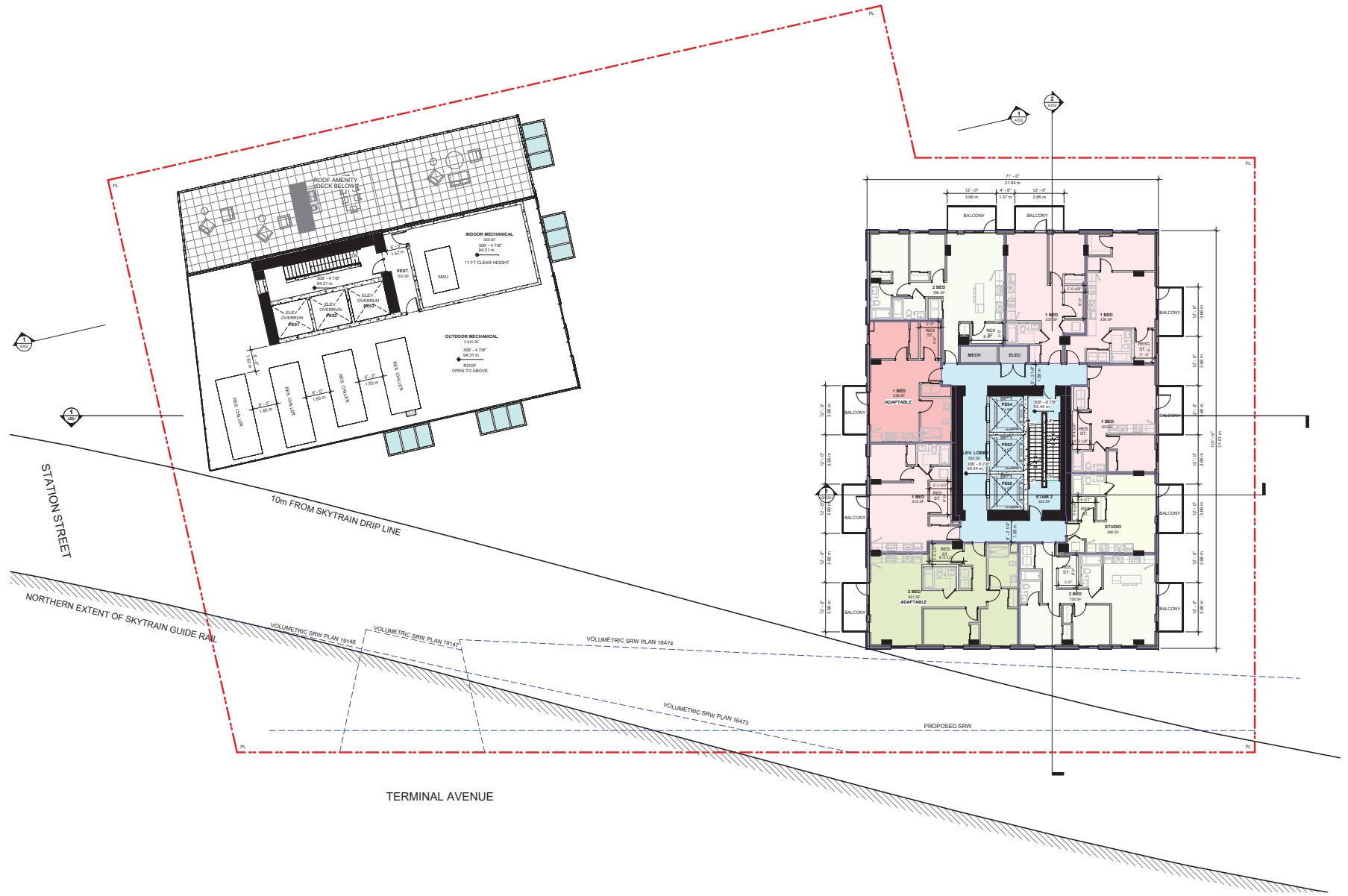
1220 STATION STREET
 1220 Station Street
 Vancouver, BC
 FLOOR PLAN LEVELS
 - LEVEL 28



NOTE:
 ALL UNITS FROM LEVELS #10
 ARE BELOW MARKET RENTAL

LEGEND

[Light Green Box]	1 BED
[Light Blue Box]	2 BED
[Light Red Box]	ADAPTABLE 1 BED
[Light Yellow Box]	ADAPTABLE 2 BED
[Light Cyan Box]	COMMON
[Light Purple Box]	SERVICE
[Light Orange Box]	STUDIO



2 LEVEL 29.
 SCALE: 1/8" = 1'-0"



1220 STATION STREET
 1220 Station Street
 Vancouver, BC
 Project:
 FLOOR PLAN LEVELS - MECH WEST TOWER LEVEL 29 EAST TOWER

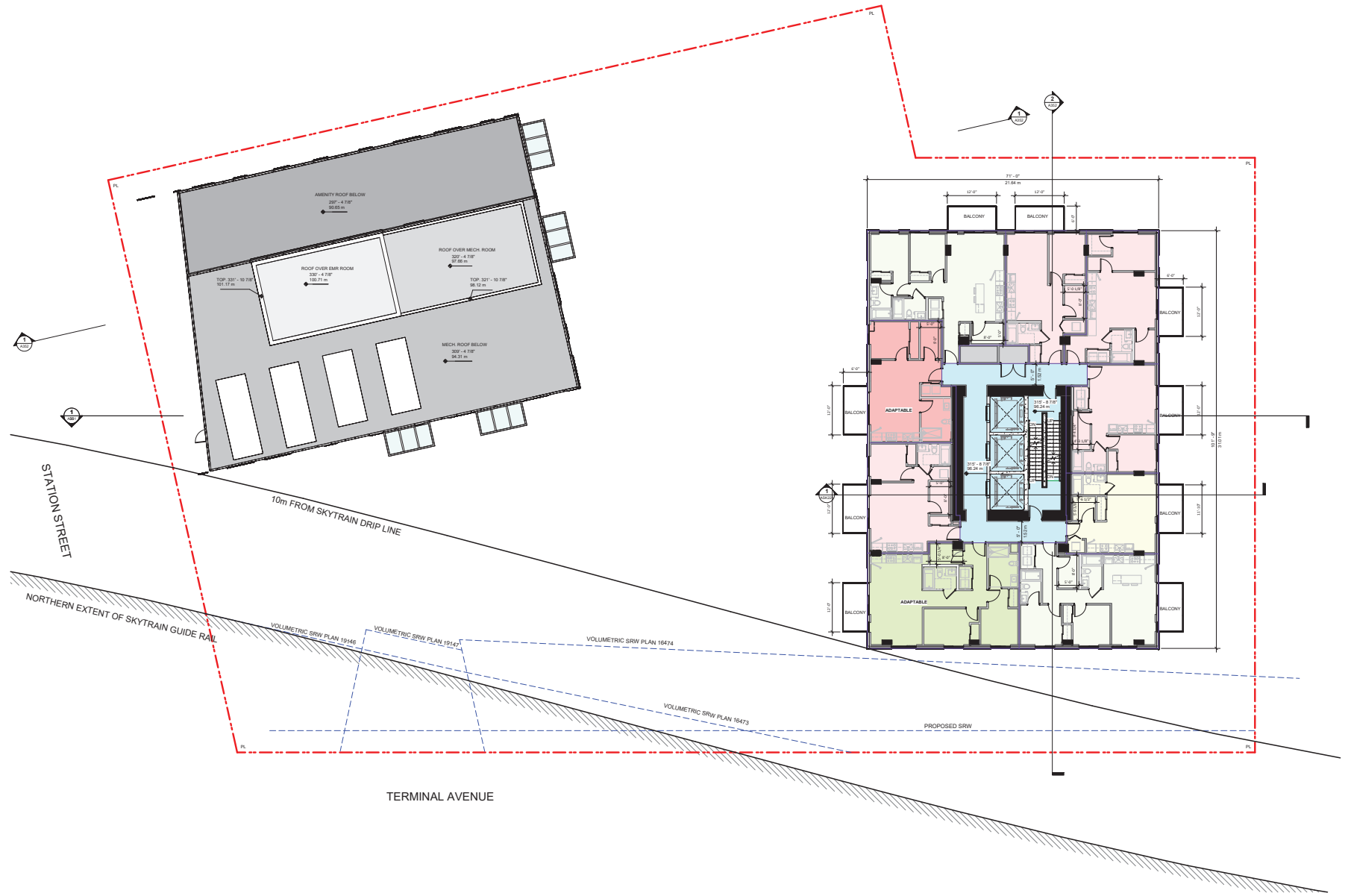
Sheet: 1220-05
 Date: 2025-04-03
 Author: MCM

A220

NOTE:
 ALL UNITS FROM LEVELS #10
 ARE BELOW MARKET RENTAL

LEGEND

[Light Green Box]	3 BED
[Light Yellow Box]	2 BED
[Light Blue Box]	ADAPTABLE 1 BED
[Light Red Box]	ADAPTABLE 2 BED
[Light Purple Box]	COMMON
[Light Cyan Box]	SERVICE
[Light Grey Box]	STUDIO



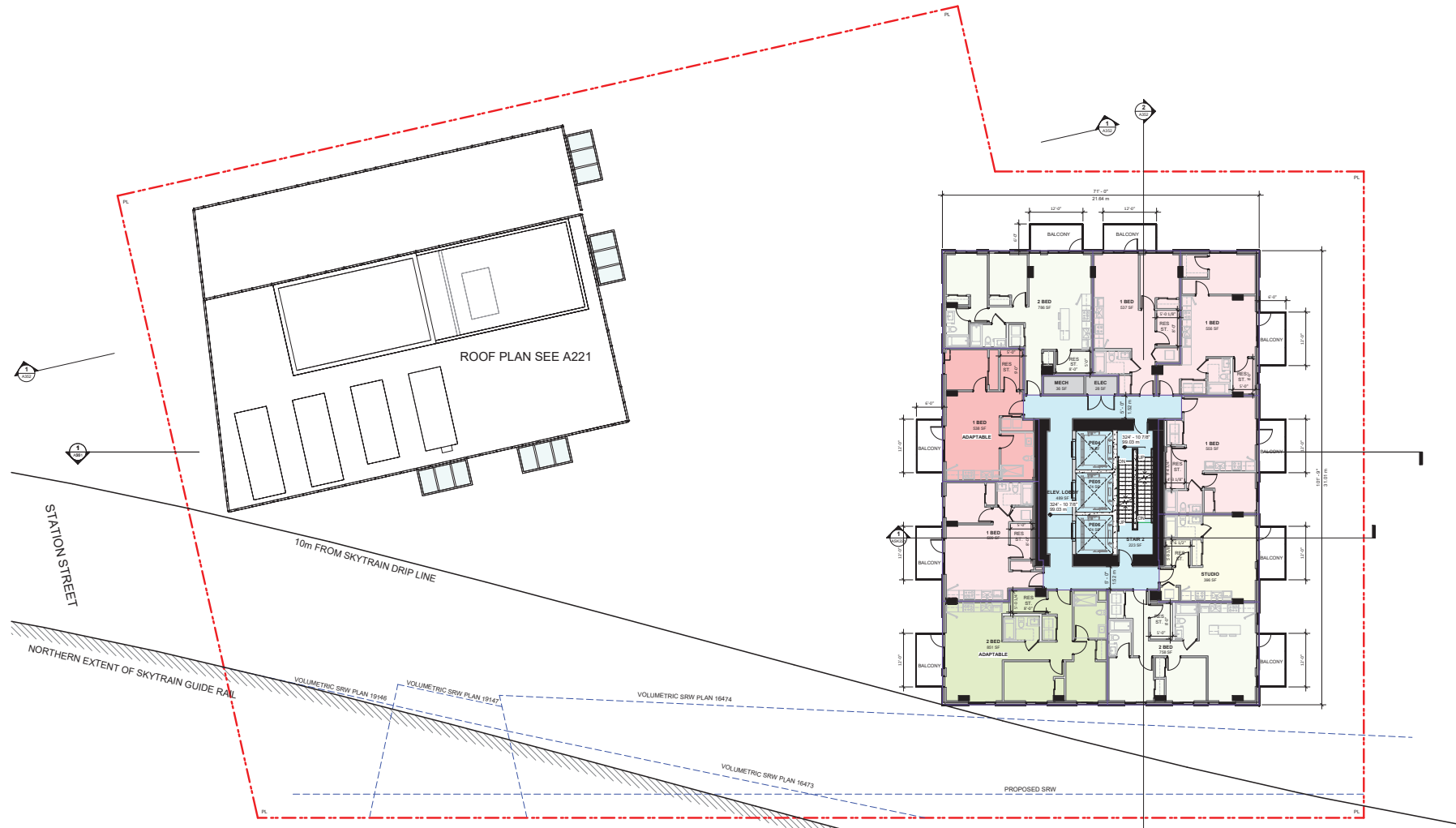
2 LEVEL 30 - FLOOR PLAN
 SCALE: 1/8" = 1'-0"



NOTE:
 ALL UNITS FROM LEVELS #10
 ARE BELOW MARKET RENTAL

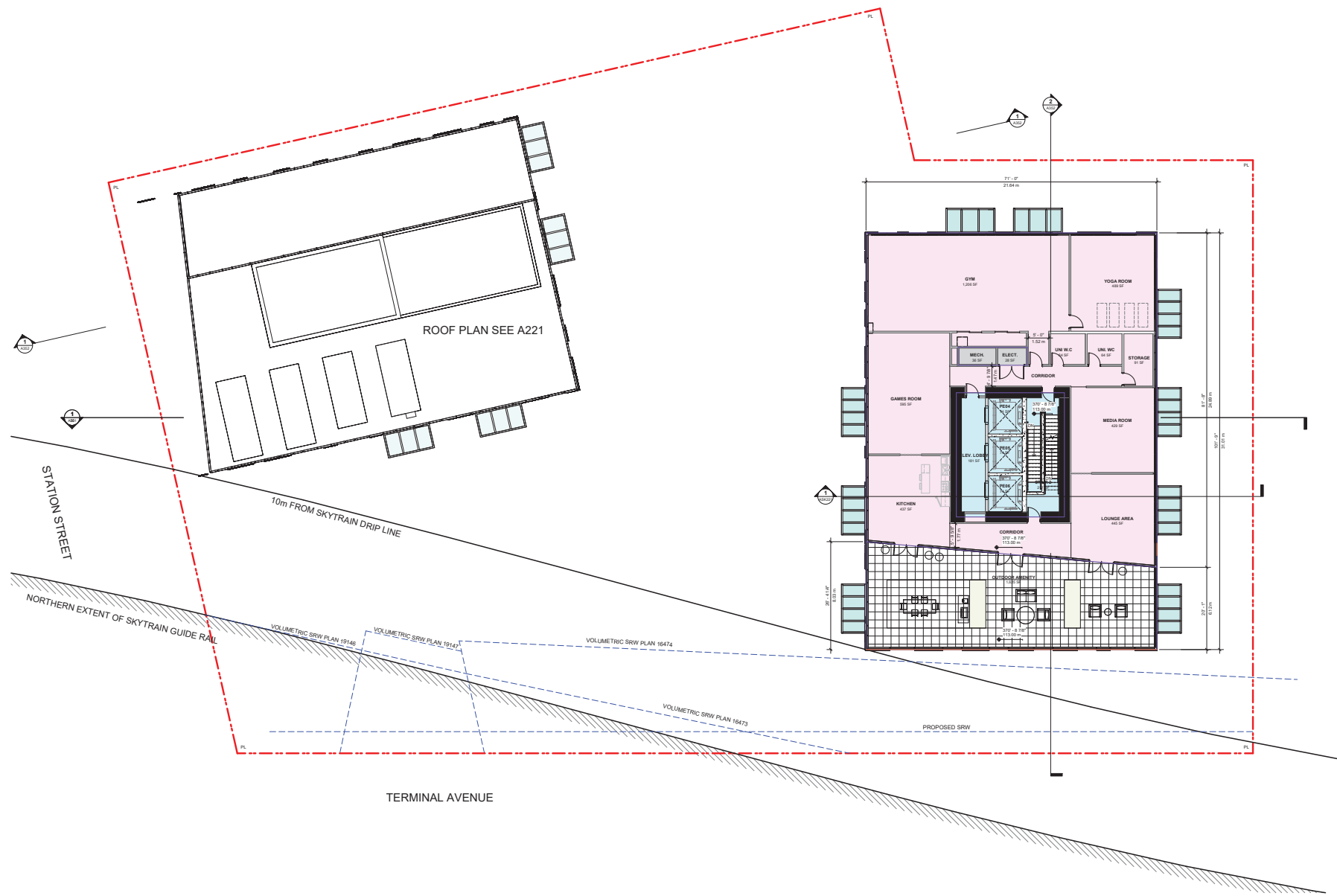
LEGEND

[Pink Box]	1 BED
[Light Green Box]	2 BED
[Light Blue Box]	ADAPTABLE 1 BED
[Light Yellow Box]	ADAPTABLE 2 BED
[Light Purple Box]	COMMON
[Light Cyan Box]	SERVICE
[Light Orange Box]	STUDIO



1 LEVEL 31-35 - FLOOR PLAN
 SCALE: 1/8" = 1'-0"

LEGEND
[Light Blue Box] STORAGE
[Light Pink Box] INDOOR AMENITY
[Light Green Box] SERVICE



1 LEVEL 36 - FLOOR PLAN
SCALE: 1/8" = 1'-0"

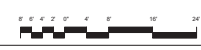
1220 STATION STREET



1220 Station Street
Vancouver, BC
Project
FLOOR PLAN - LEVELS 36

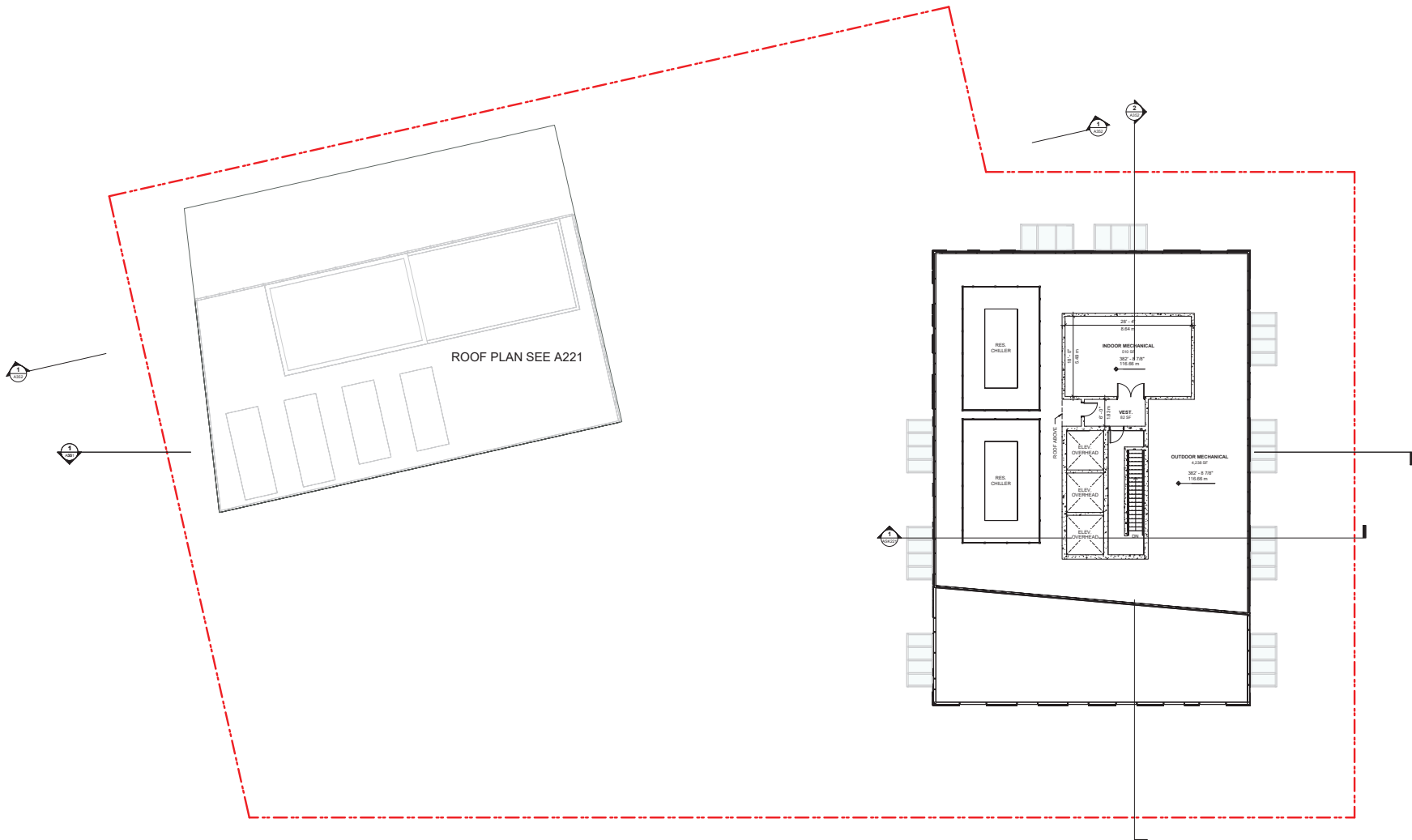
Sheet: 1/27
Scale: 1/8" = 1'-0"
Project: 202403

A223





Revision	YYYYMMDD
1	ISSUED FOR PERIZING 2024-03-11
2	ISSUED FOR PERIZING 2024-08-14
3	ISSUED FOR PERIZING 2025-04-03

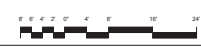


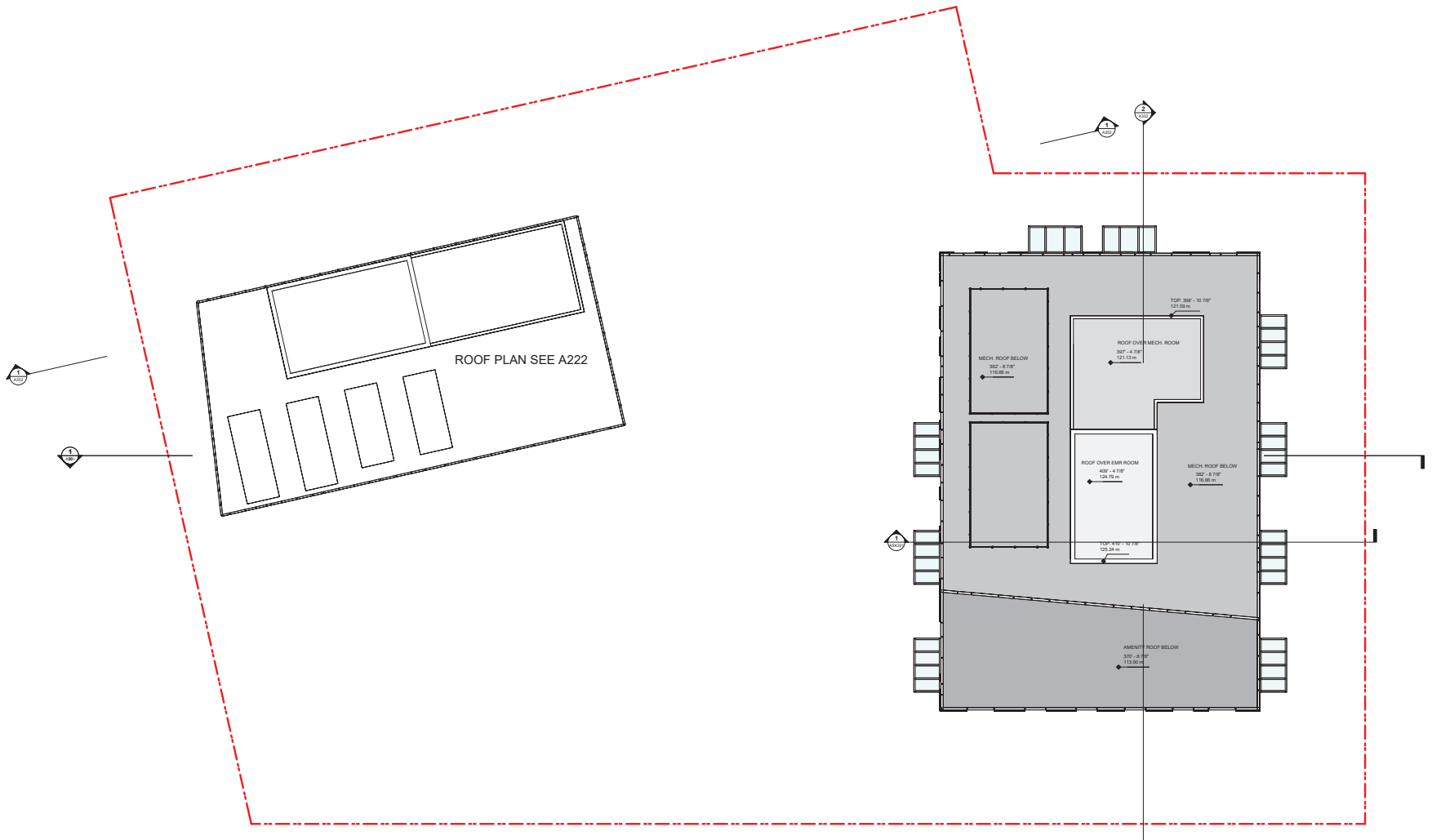
1 MECH EAST TOWER
SCALE: 1/8" = 1'-0"

Sheet
1220 STATION STREET

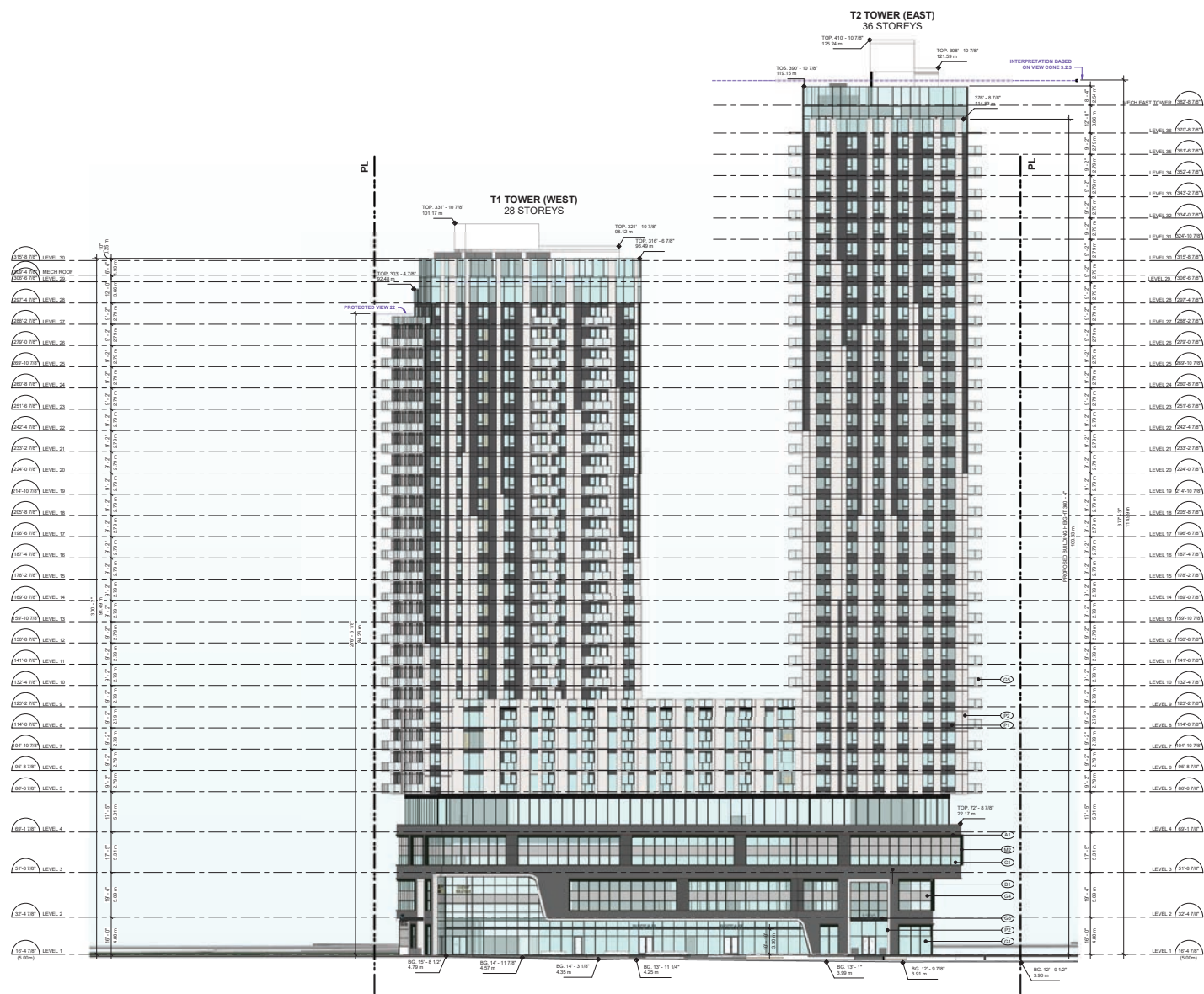
1220 Station Street
Vancouver, BC
Project
FLOOR PLAN - LEVEL
MECH EAST TOWER

Scale: 1/8" = 1'-0"
Date: 2025-04-03
Sheet: **A224**





MATERIAL LEGEND	
VALUE	DESCRIPTION
L1	ALUMINUM LITTEL COLOUR DARK GREEN
L2	ALUMINUM FLASHING COLOUR DARK GREEN
R1	BRICK VENEER WALL HORIZONTAL COLOUR MIX OF DARK GREYS
R2	BRICK VENEER WALL VERTICAL COLOUR MIX OF DARK GREYS
C1	EXPOSED CONCRETE COLOUR LIGHT GREY
C2	WIND EFFICIENCY GLAZING CURTAIN WALL W/ DARK GREEN MULLIONS
C3	WIND EFFICIENCY GLAZING WINDOW WALL W/ DARK GREEN MULLIONS
C4	INSULATED GLASS SPANDREL PANEL
C5	CURTAINWALL GLASS AND METAL FRAME COLOUR DARK GREY
M1	METAL 16-GAUGE GRIPPY FINISHED GRAPHITE COLOUR DARK GREY
M2	PURCHASATED METAL COLOUR DARK GREEN
M3	METAL FRAME W/ WINDOW COLOUR DARK GREEN
M4	METAL FRAME CURTAIN WALL DARK GREEN
C6	CURTAIN WALL METAL PANEL COLOUR DARK GREY
C7	CURTAIN WALL METAL PANEL COLOUR LIGHT GREY



2 SOUTH ELEVATION
 SCALE: 1/8" = 1'-0"

1220 STATION STREET

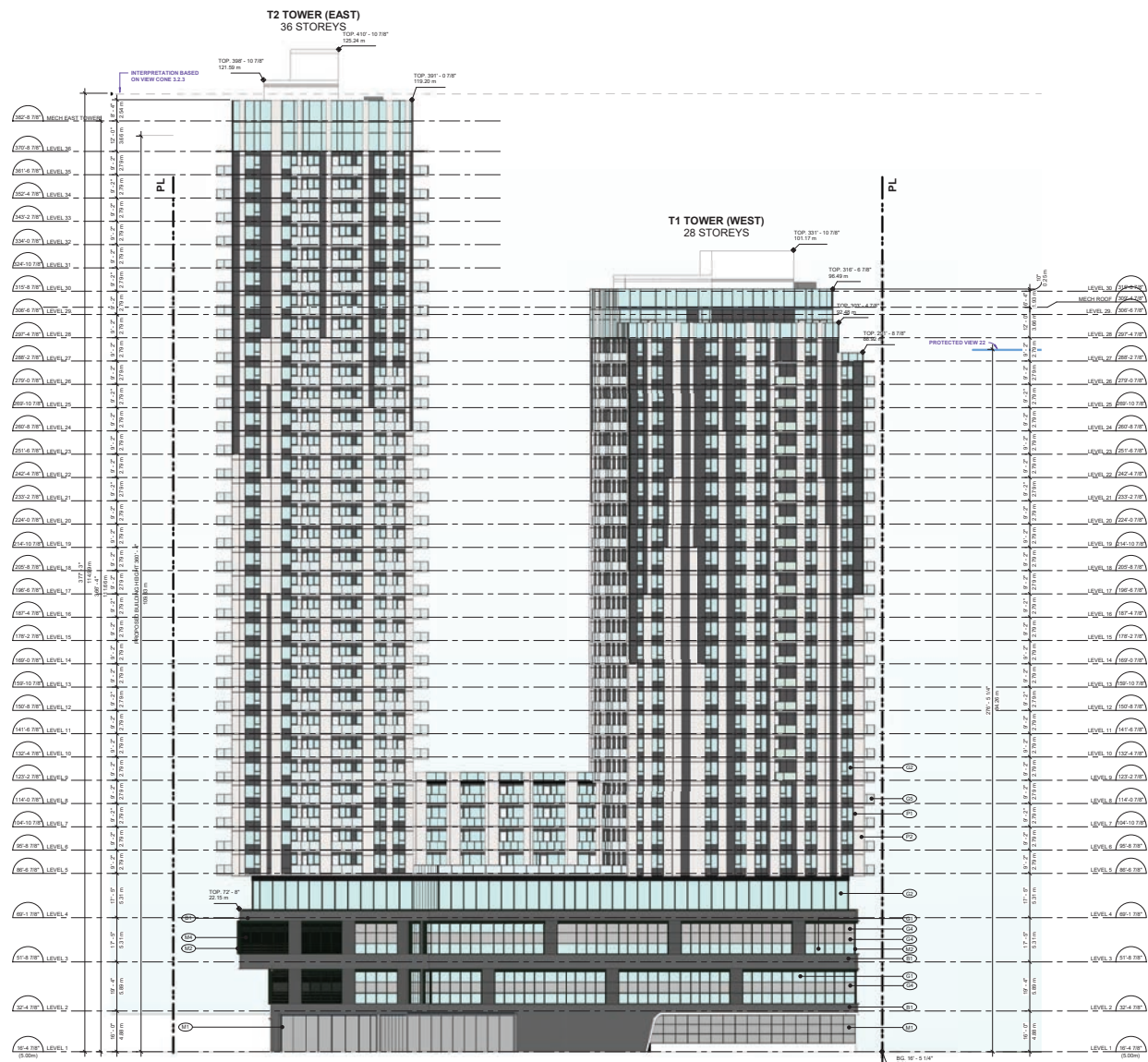
1220 Station Street
 Vancouver, BC
 Project
 SOUTH ELEVATION

Sheet: 1/10
 Date: 1/11/24
 Project: 240004



A301

MATERIAL LEGEND	
VALUE	DESCRIPTION
A1	ALUMINUM LITELE, COLOUR DARK GREEN
A2	ALUMINUM FLASHING, COLOUR DARK GREEN
B1	BRICK, WEINER WALL, HORIZONTAL, COLOUR MIX OF DARK GRAYS
B2	BRICK, WEINER WALL, VERTICAL, COLOUR MIX OF DARK GRAYS
C1	EXPOSED CONCRETE, COLOUR LIGHT GREY
C2	WIND EFFICIENCY GLAZING CURTAIN WALL W/ DARK GREEN MULLIONS
C3	WIND EFFICIENCY GLAZING WINDOW WALL W/ DARK GREEN MULLIONS
C4	INSULATED GLASS SPANDREL PANEL
D1	CURTAIN WALL GLASS AND METAL FRAME COLOUR DARK GREY
D2	METAL INSULATED GLASS CURTAIN WALL COLOUR DARK GREY
E1	SPANDREL METAL, COLOUR DARK GREEN
E2	METAL FRAME AL WINDOWS, COLOUR DARK GREEN
F1	MECHANICAL CURTAIN WALL, DARK GREEN
G1	CURTAIN WALL METAL PANEL, COLOUR DARK GREY
G2	CURTAIN WALL METAL PANEL, COLOUR LIGHT GREY



1 NORTH ELEVATION
 SCALE: 1/8" = 1'-0"

1220 STATION STREET

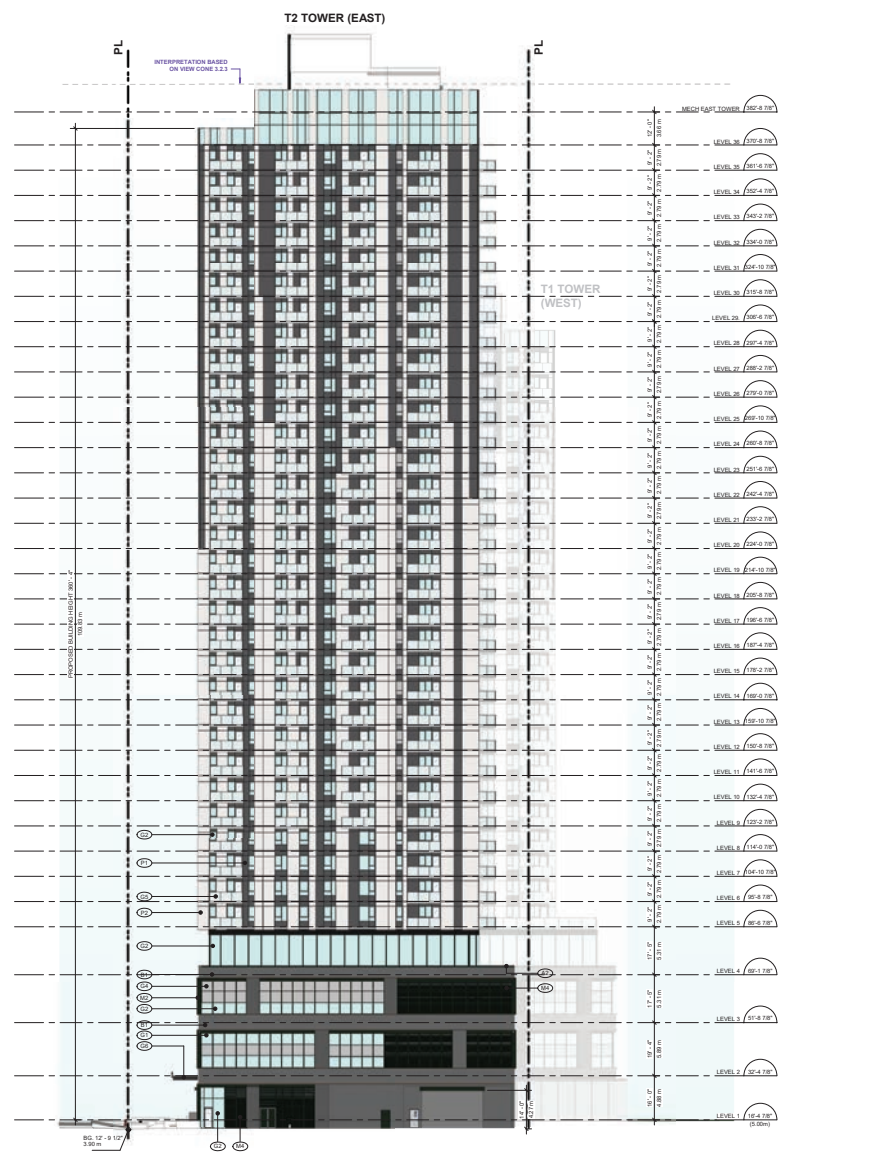
1220 Station Street
 Vancouver, BC

Scale: 1/8" = 1'-0"

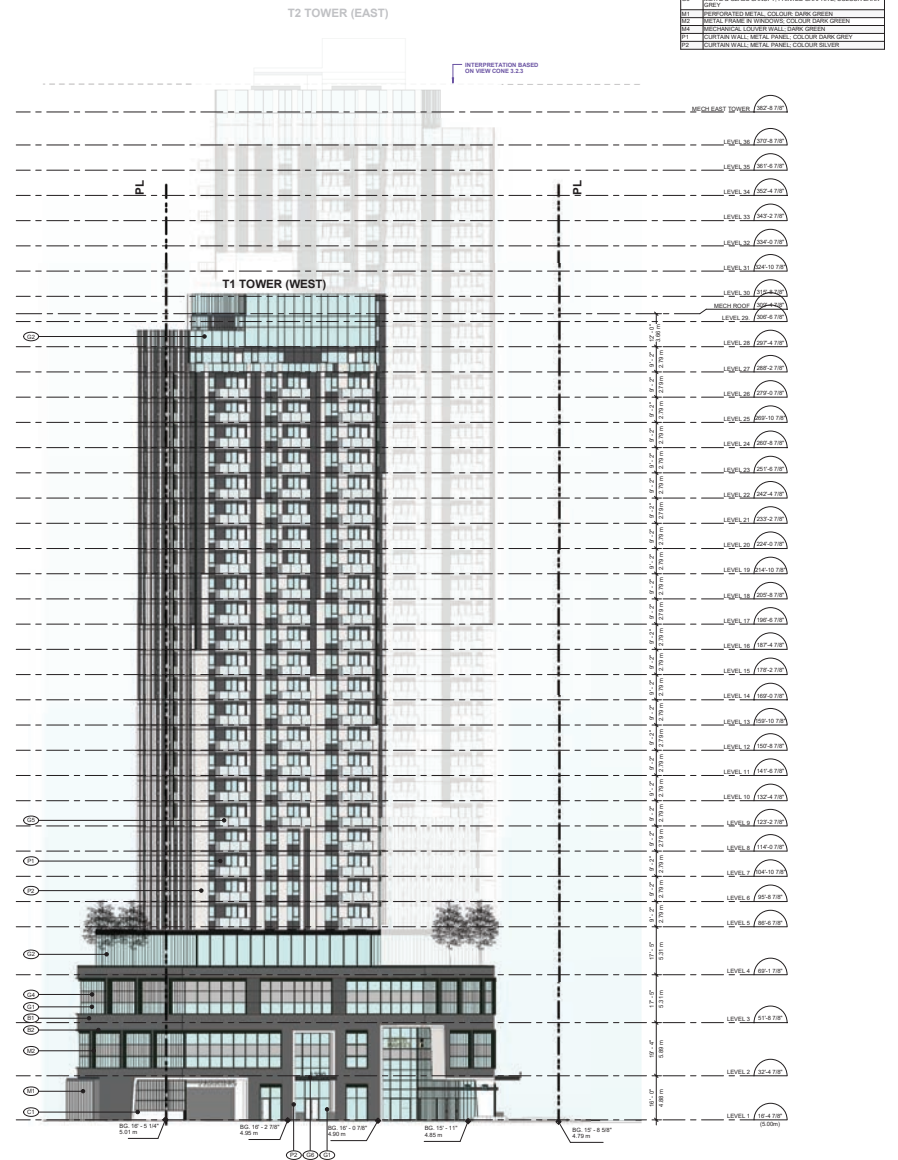


A302

MATERIAL LEGEND	
VALUE	DESCRIPTION
M1	ALUMINIUM PANEL, COLOUR DARK GREEN
M2	ALUMINIUM PANEL, COLOUR DARK GREEN
M3	BRICK VENEER WALL, HORIZONTAL, COLOUR MIX OF DARK GREYS
M4	BRICK VENEER WALL, VERTICAL, COLOUR MIX OF DARK GREYS
M5	EXPOSED CONCRETE, COLOUR LIGHT GREY
M6	HIGH EFFICIENCY GLAZING, CURTAIN WALL W/ DARK GREEN TINTING
M7	HIGH EFFICIENCY GLAZING, WINDOW WALL W/ DARK GREEN TINTING
M8	GLASS AND METAL PANEL, COLOUR DARK GREY
M9	METAL GLASS CURTAIN WALL SUPPORTS, COLOUR DARK GREY
M10	EXPOSED METAL, COLOUR DARK GREEN
M11	METAL FRAME IN WINDOW, COLOUR DARK GREEN
M12	WINDOW, COLOUR DARK GREY
M13	CURTAIN WALL METAL PANEL, COLOUR DARK GREY
M14	CURTAIN WALL METAL PANEL, COLOUR SILVER



1 EAST ELEVATION
 SCALE: 1/16" = 1'-0"



2 WEST ELEVATION
 SCALE: 1/16" = 1'-0"



MATERIAL LEGEND	
CODE	DESCRIPTION
AL	ALUMINUM LITTEL, COLOUR DARK GREEN
AT	ALUMINUM FLASHING, COLOUR DARK GREEN
BT	BRICK FINER WALL, HORIZONTAL, COLOUR MIX OF DARK GRAY
BD	BRICK FINER WALL, VERTICAL, COLOUR MIX OF DARK GRAY
CT	EXPOSED CONCRETE, COLOUR LIGHT GREY
CF	LOW EFFICIENCY GLAZING, CURTAIN WALL, W/ DARK GREEN MULLIONS
CG	HIGH EFFICIENCY GLAZING, WINDOW WALL, W/ DARK GREEN MULLIONS
CL	INSULATED GLASS SPANDREL PANEL
CS	QUADRIFRAME, GLASS AND METAL FRAME, COLOUR DARK GREY

MATERIAL LEGEND	
VALUE	DESCRIPTION
01	METAL & GLASS CANOPY, PAINTED SPARTITE, COLOUR DARK GRAY
02	PERFORATED METAL, COLOUR DARK GREEN
03	METAL FRAME W/ HORIZONTAL, COLOUR DARK GREEN
04	METAL FRAME W/ VERTICAL, COLOUR DARK GREEN
05	MECHANICAL COVER WALL, COLOUR DARK GREEN
06	PERFORATED METAL WALL, COLOUR DARK GRAY
07	CURTAIN WALL, METAL PANEL, COLOUR SILVER



1 ENLARGED - SOUTH PODIUM ELEVATION
 SCALE: 1/8" = 1'-0"



2 ENLARGED - NORTH PODIUM ELEVATION
 SCALE: 1/8" = 1'-0"



Revision: YYYYMMDD
 1 ISSUED FOR PERMITTING 2024-10-11
 2 ISSUED FOR PERMITTING 2024-08-14
 3 ISSUED FOR PERMITTING 2023-10-10

MATERIAL LEGEND		MATERIAL LEGEND	
CODE	DESCRIPTION	VALUE	DESCRIPTION
A1	ALUMINUM LENTEL, COLOUR DARK GREEN	01	METAL & GLASS CANOPY / PAINTED GRANITE, COLOUR DARK GREY
A2	ALUMINUM FLASHING, COLOUR DARK GREEN	02	DAIRY
B1	BRICK VENEER WALL, HORIZONTAL, COLOUR MIX OF DARK GREY	03	PERFORATED METAL, COLOUR DARK GREEN
B2	BRICK VENEER WALL, VERTICAL, COLOUR MIX OF DARK GREY	04	METAL FRAME W/ FINISHED COLOUR DARK GREEN
C1	EXPOSED CONCRETE, COLOUR LIGHT GREY	05	MECHANICAL COVER WALL, COLOUR GREEN
C2	LOW EFFICIENCY GLAZING, WINDOW WALL, W/ DARK GREEN MILLIONS	06	CONCRETE WALL, COLOUR LIGHT GREY
C3	HIGH EFFICIENCY GLAZING, WINDOW WALL, W/ DARK GREEN MILLIONS	07	CURTAIN WALL, METAL PANEL, COLOUR SILVER
D1	INSULATED GLASS SPANDREL PANEL		
D2	QUADRUPLE GLASS AND METAL FRAME, COLOUR DARK GREY		



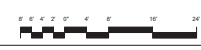
1 ENLARGED - EAST PODIUM ELEVATION
 SCALE: 1/8" = 1'-0"



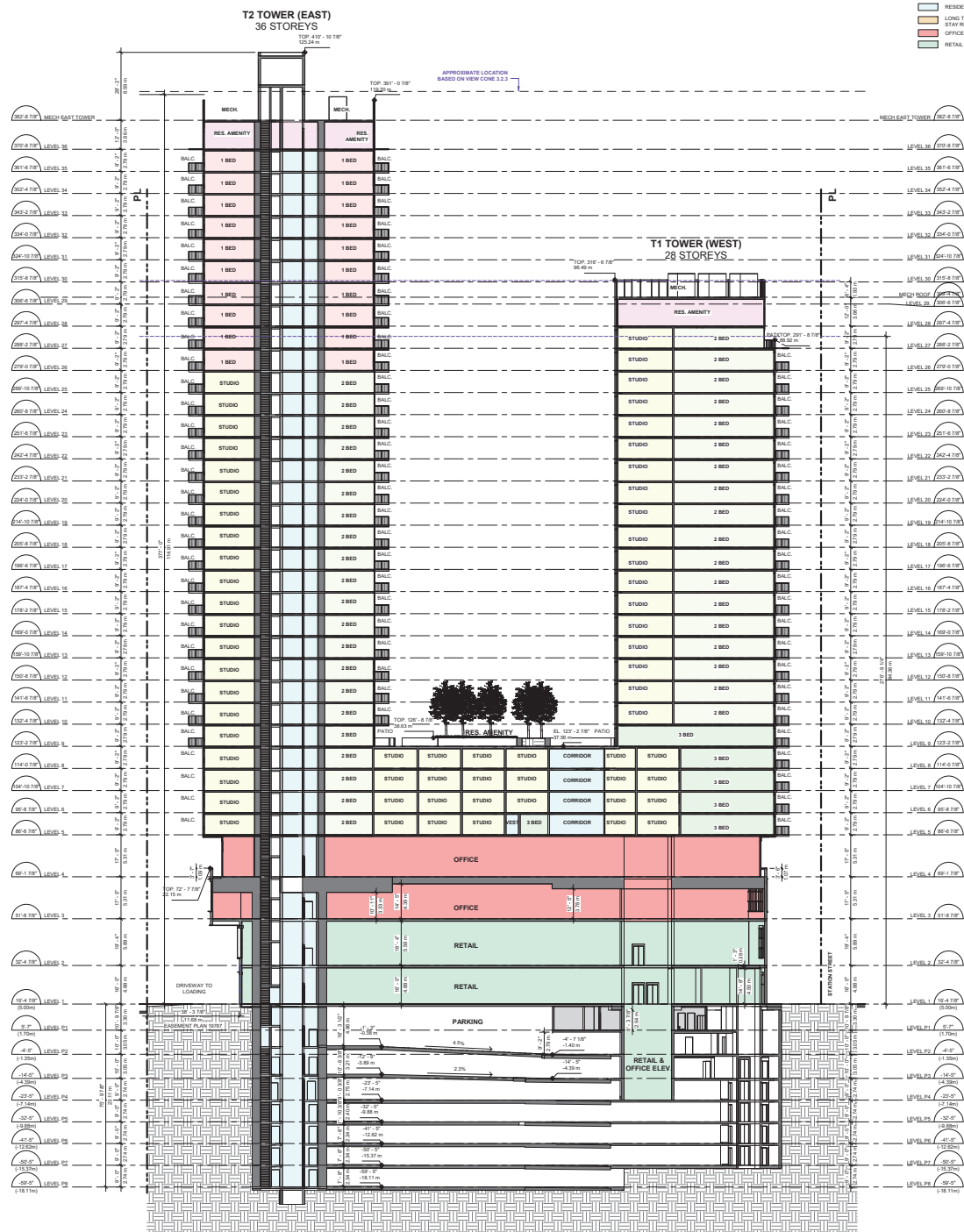
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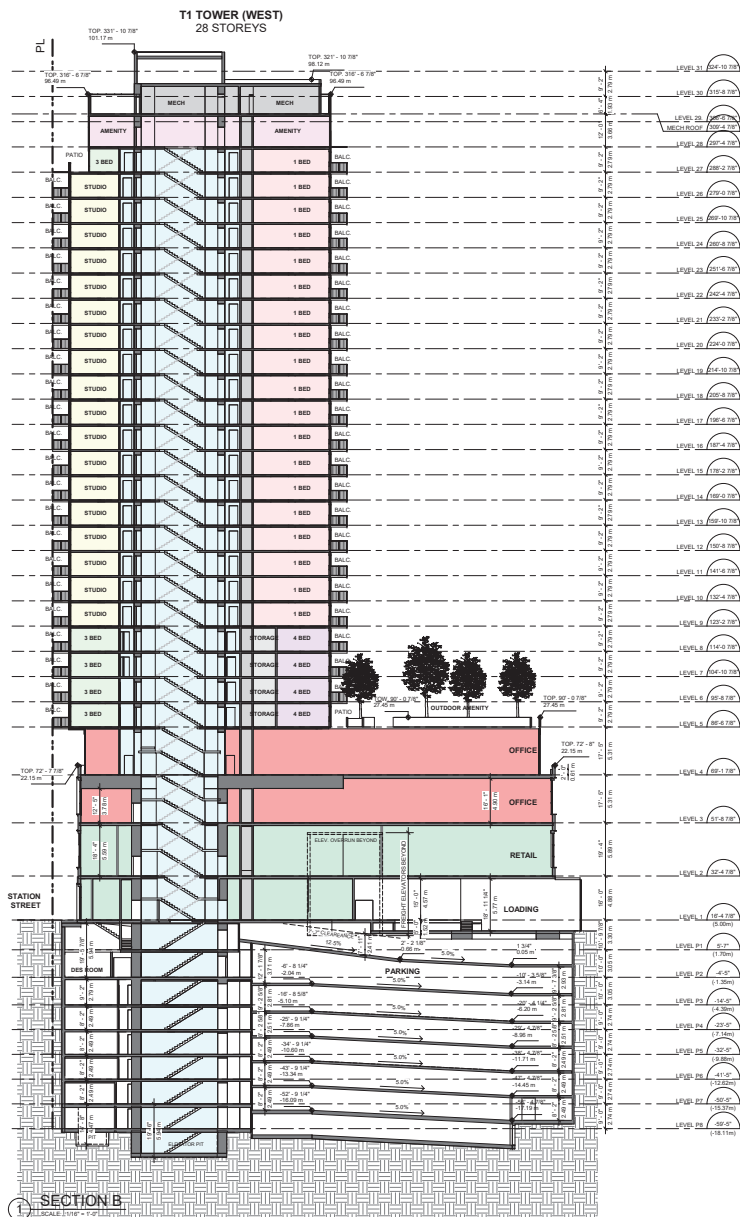
Site:
 1220 STATION STREET
 Vancouver, BC
 Project:
 ENLARGED EAST / WEST - PODIUM ELEVATION

Drawn: LSE + L2
 Scale: 1/8" = 1'-0"
 Project: 202404
 Sheet: A305

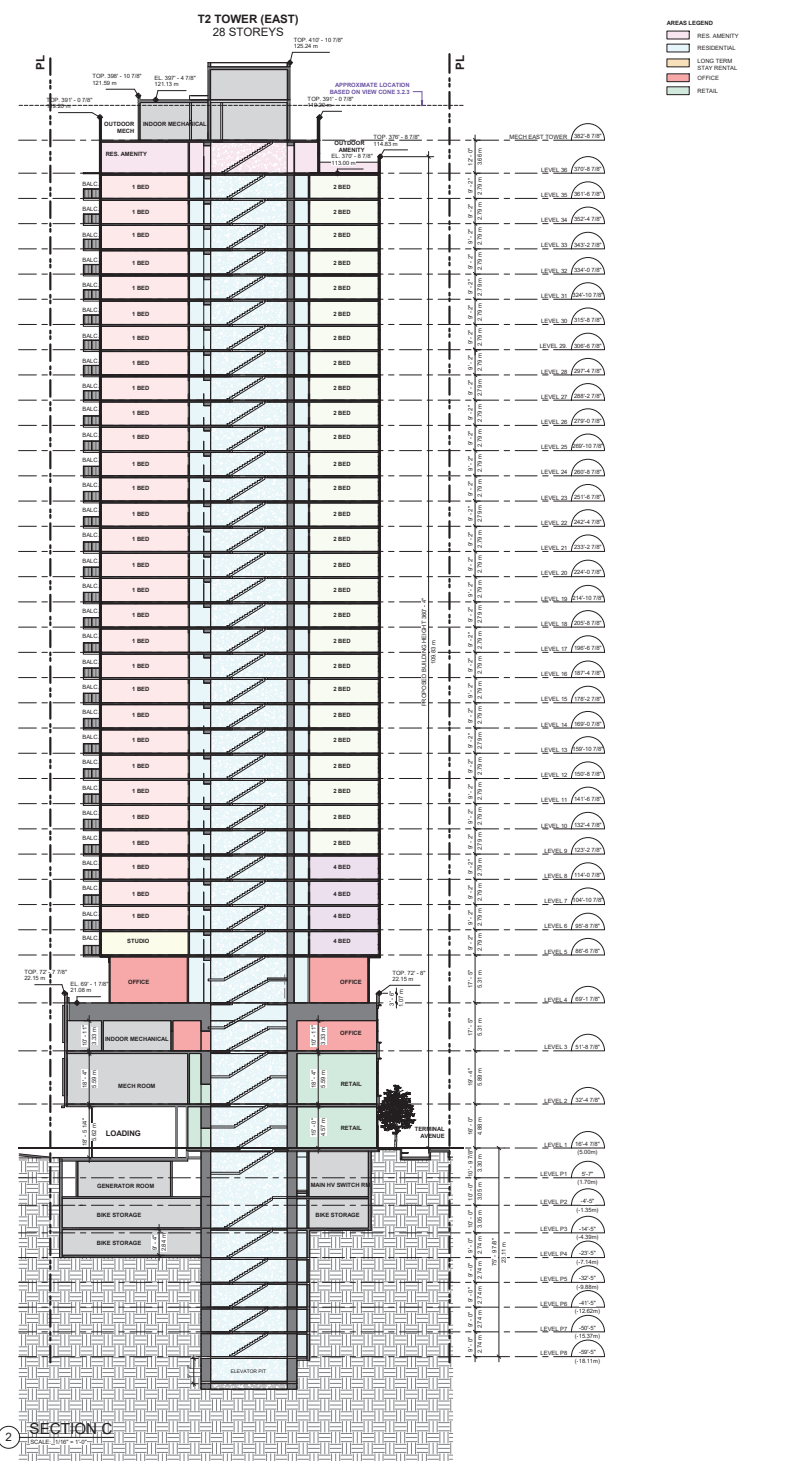


- AREAS LEGEND**
- RES. AMENITY
 - RESIDENTIAL
 - LONG TERM STAY RENTAL
 - OFFICE
 - RETAIL





SECTION B
 SCALE 1/16" = 1'-0"



SECTION C
 SCALE 1/16" = 1'-0"

AREAS LEGEND

- RES. AMENITY
- RESIDENTIAL
- LONG TERM STAY RENTAL
- OFFICE
- RETAIL

1220 STATION STREET

1220 Station Street
 Vancouver BC

SECTION B / SECTION C

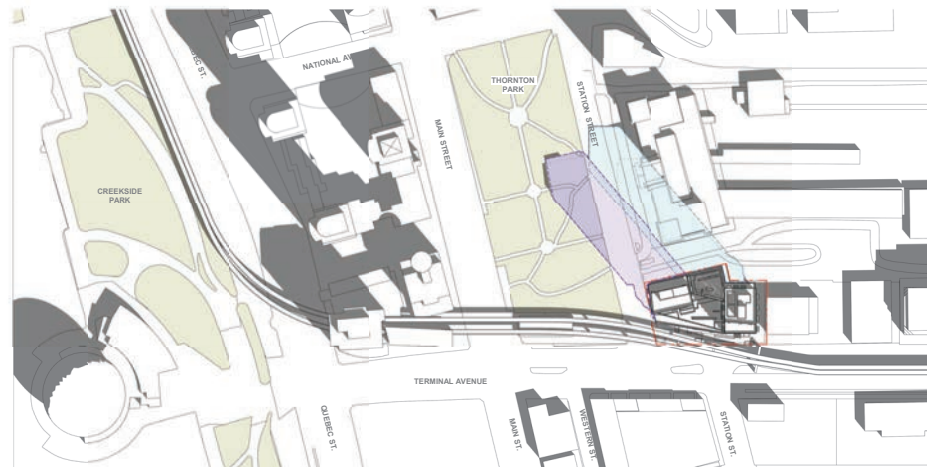
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A352

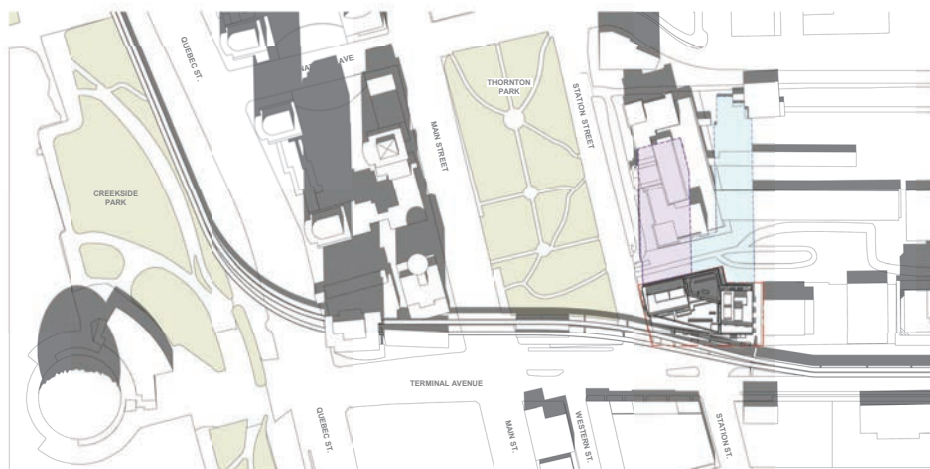




1 SPRING EQUINOX - MARCH 21 10 AM
SCALE: 1" = 100'-0"



2 SPRING EQUINOX - MARCH 21 12 PM
SCALE: 1" = 100'-0"



3 SPRING EQUINOX - MARCH 21 2 PM
SCALE: 1" = 100'-0"

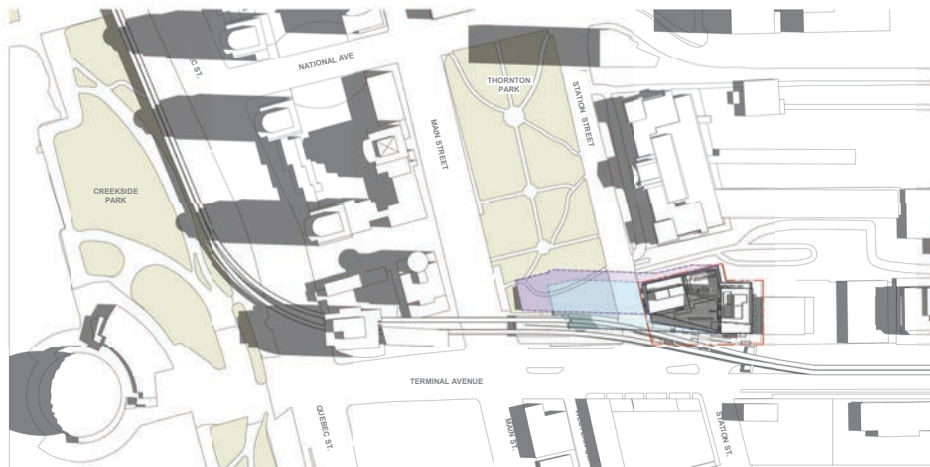


4 SPRING EQUINOX - MARCH 21 4 PM
SCALE: 1" = 100'-0"

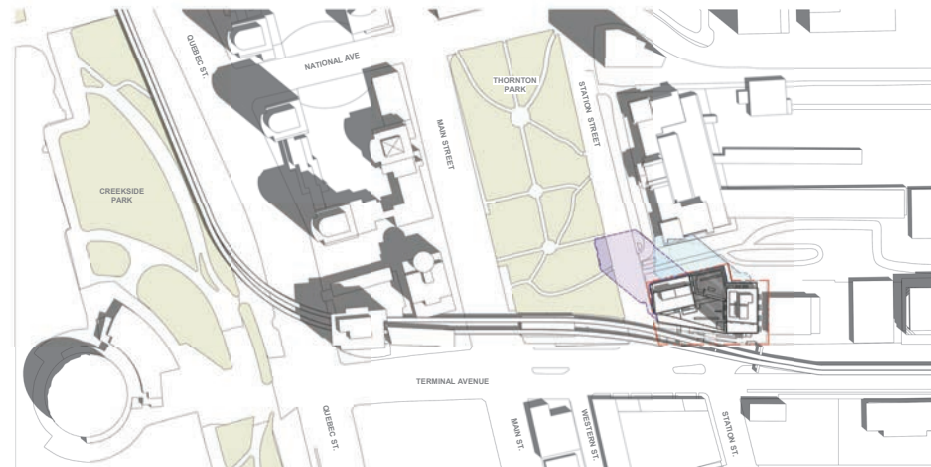
SPRING EQUINOX (MARCH 21)

PACIFIC DAYLIGHT TIME (PDT; UTC: -7:00)

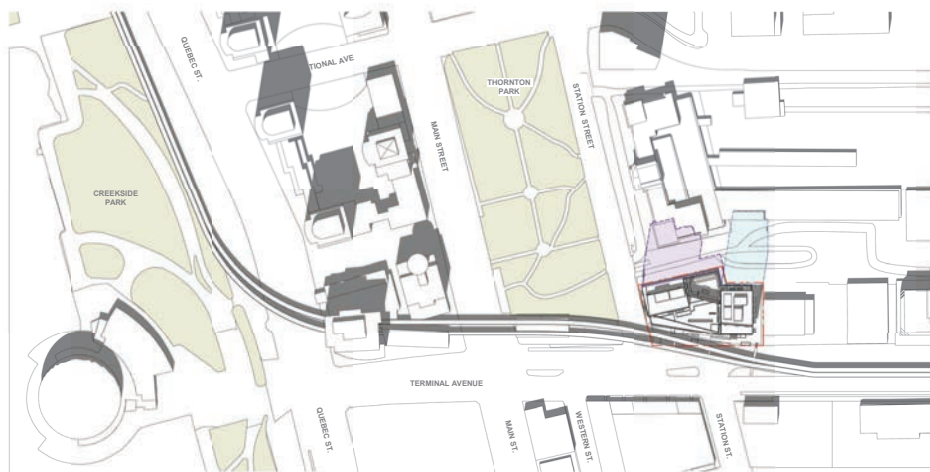




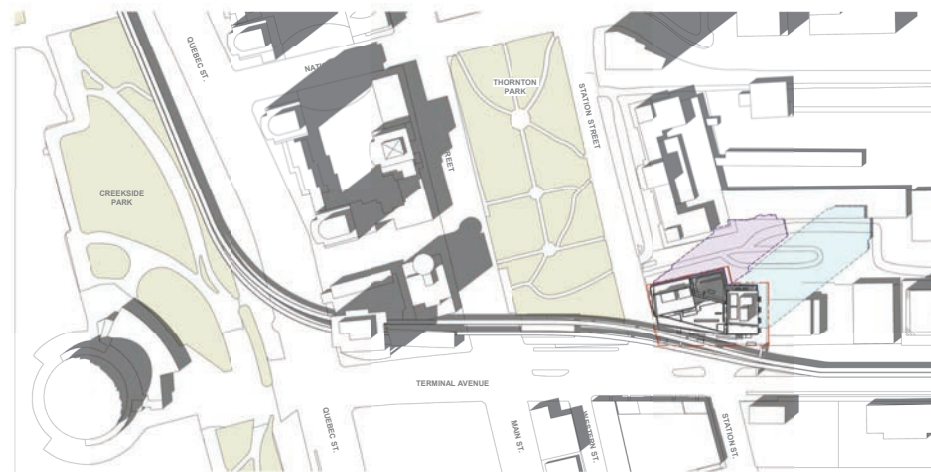
1 SUMMER SOLSTICE - JUNE 21 10 AM
SCALE: 1" = 100'



2 SUMMER SOLSTICE - JUNE 21 12 PM
SCALE: 1" = 100'



3 SUMMER SOLSTICE - JUNE 21 2 PM
SCALE: 1" = 100'



4 SUMMER SOLSTICE - JUNE 21 4 PM
SCALE: 1" = 100'

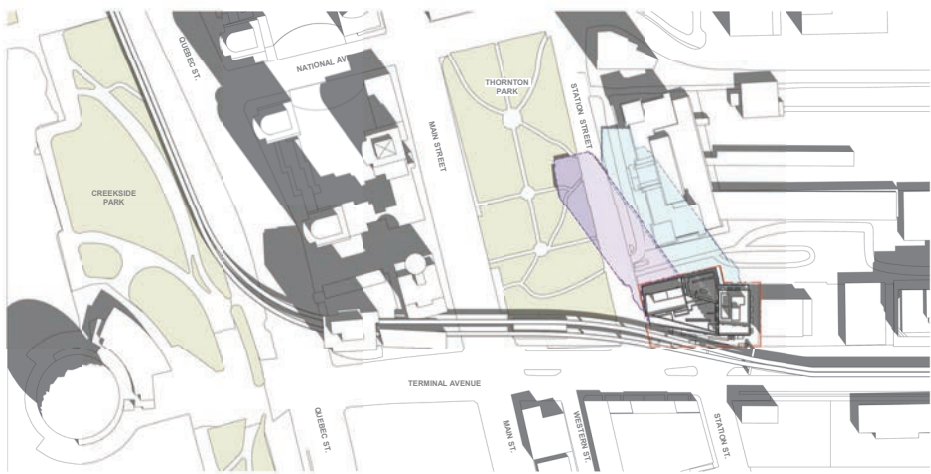
SOLSTICE (JUNE 21)

PACIFIC DAYLIGHT TIME (PDT: UTC: -7:00)





1 FALL EQUINOX - SEPTEMBER 22 10 AM
SCALE: 1" = 100'0"



2 FALL EQUINOX - SEPTEMBER 22 12 PM
SCALE: 1" = 100'0"



3 FALL EQUINOX - SEPTEMBER 22 2 PM
SCALE: 1" = 100'0"



4 FALL EQUINOX - SEPTEMBER 22 4 PM
SCALE: 1" = 100'0"

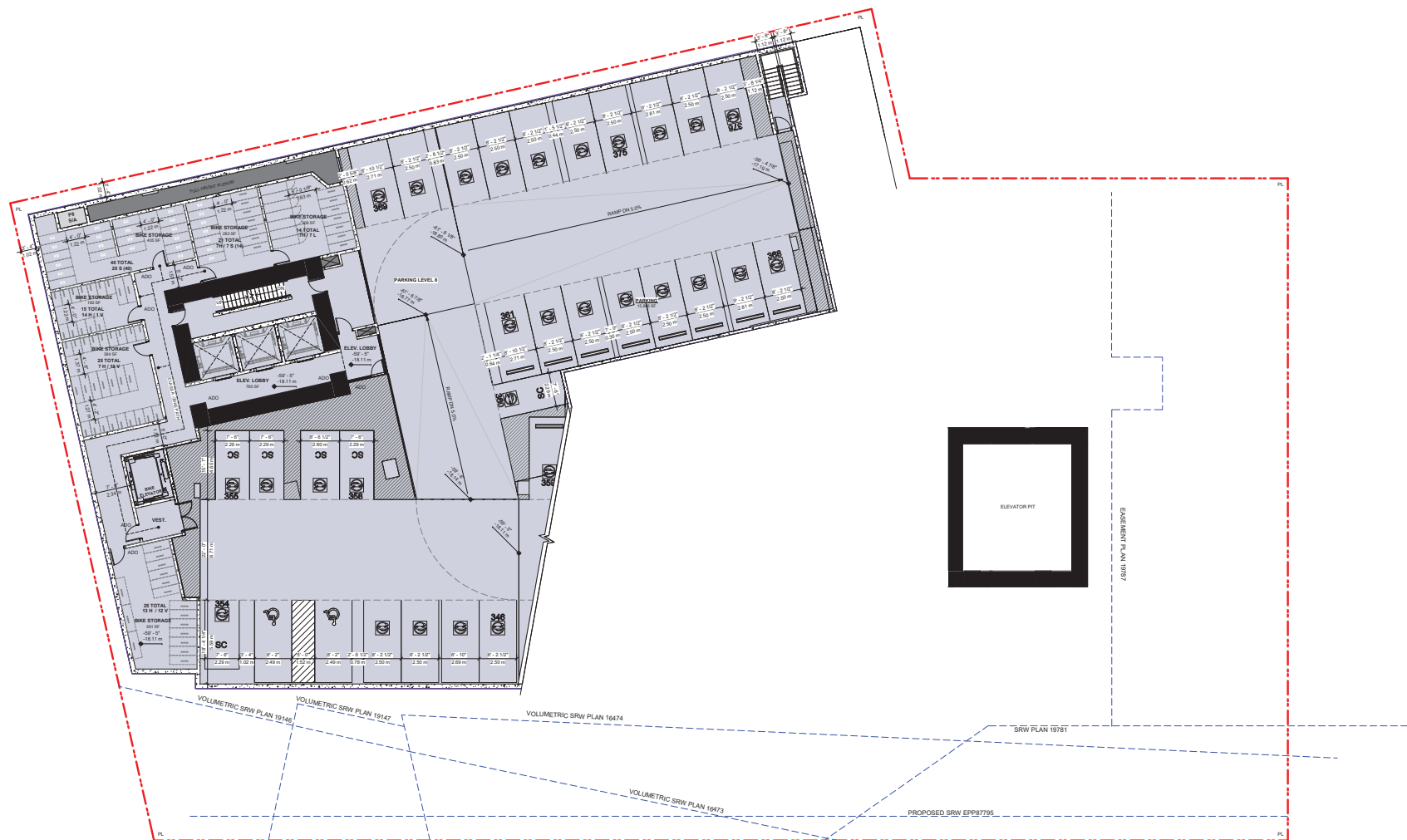
FALL EQUINOX (SEPTEMBER 22ND)

PACIFIC DAYLIGHT TIME (PDT): UTC: -7:00

FSR AREA



LEVEL P8- AREA SUMMARY	
USE	AREA
PARKING	15,656 SF / 1,473 SQM



STATION STREET

TERMINAL AVENUE

1 FSR - AREA PLAN LEVEL P8
SCALE: 1/8" = 1'-0"



1220 STATION STREET



1220 Station Street
Vancouver BC
Project
FSR AREA PLAN
LEVEL P8

Scale: 1/8" = 1'-0"

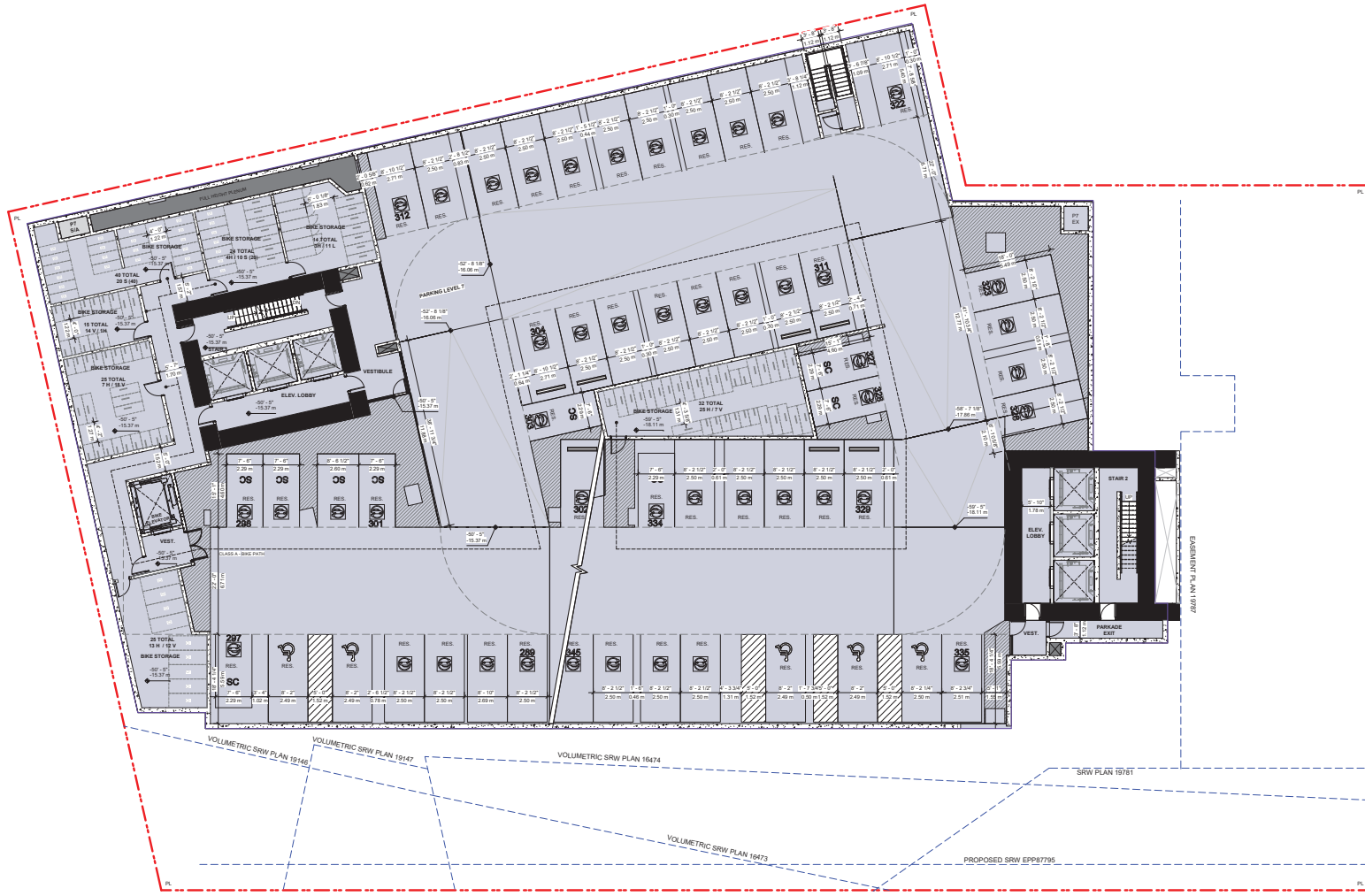
Date: 2024.06.14

Sheet: FSR201

FSR AREA


LEVEL P7 - AREA SUMMARY	
USE	AREA
PARKING	25,107 SQ FT / 2,337 SQ M

STATION STREET



TERMINAL AVENUE

1 FSR - AREA PLAN LEVEL P7
 SCALE: 1/8" = 1'-0"



1220 STATION STREET



1220 Station Street
 Vancouver BC
 Project:
 FSR AREA PLAN
 LEVEL P7

Scale: 1/8" = 1'-0"

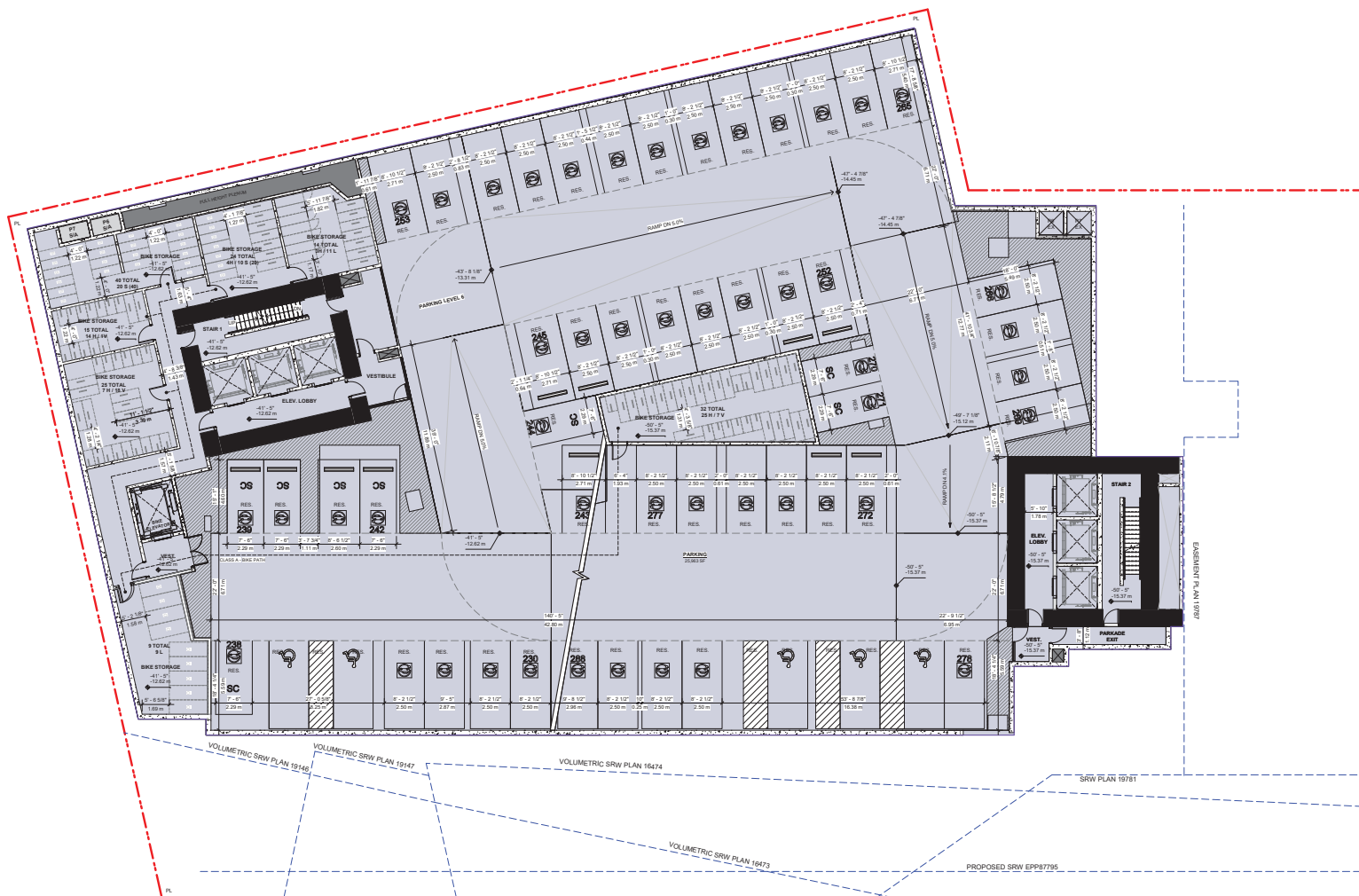
FSR202

FSR AREA



LEVEL P6 - AREA SUMMARY	
USE	AREA
PARKING	25,963 SF / 2,412 m ²

STATION STREET



TERMINAL AVENUE

1 FSR - AREA PLAN LEVEL P6
 SCALE: 1/8" = 1'-0"



1220 STATION STREET
 Vancouver, BC
 FSR AREA PLAN
 LEVEL P6

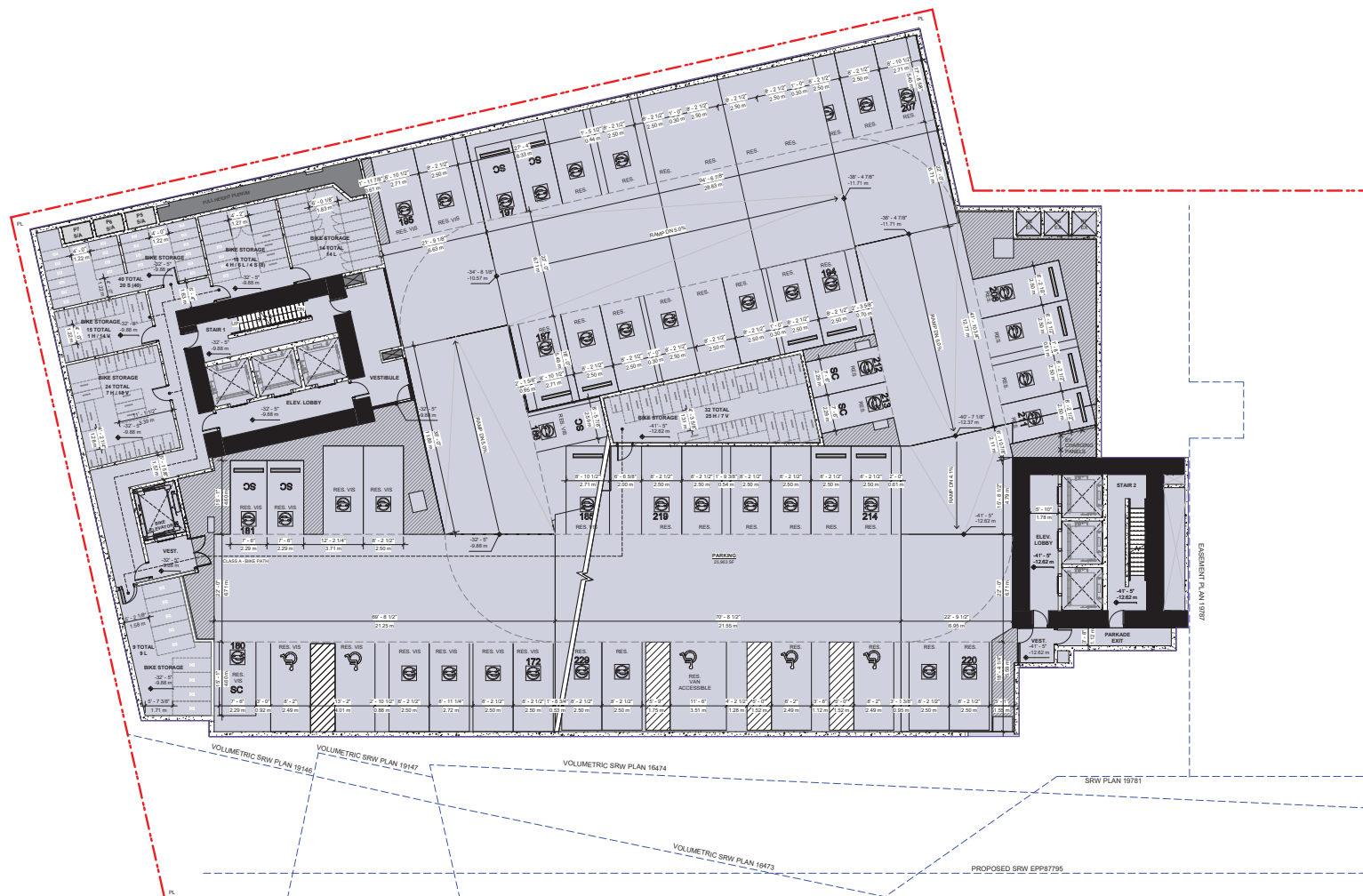
FSR203

FSR AREA



LEVEL P5 - AREA SUMMARY	
USE	AREA
PARKING	25,963 SF / 2,412 m ²

STATION STREET



TERMINAL AVENUE

1 FSR - AREA PLAN LEVEL P5
SCALE: 1/8" = 1'-0"



1220 STATION STREET



1220 Station Street
Vancouver, BC
Project
FSR AREA PLAN
LEVEL P5

Scale: 1/8" = 1'-0"

Date: 2024.01.11

Sheet: FSR204

Author: MCM

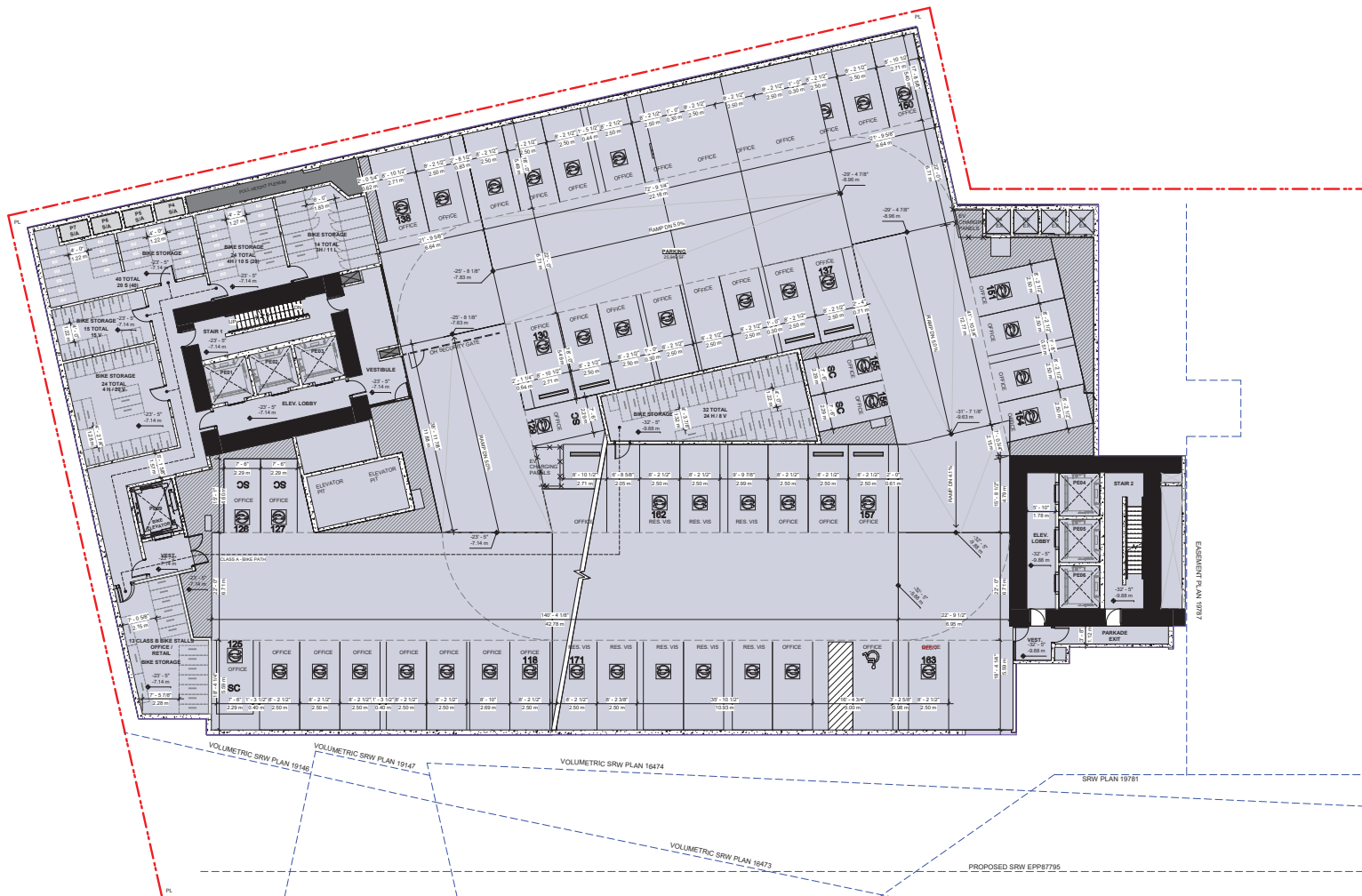
FSR204

FSR AREA



LEVEL P4 - AREA SUMMARY	
USE	AREA
PARKING	25,500 SF / 2,411 m ²

STATION STREET



TERMINAL AVENUE

1 FSR - AREA PLAN LEVEL P4
 SCALE: 1/8" = 1'-0"



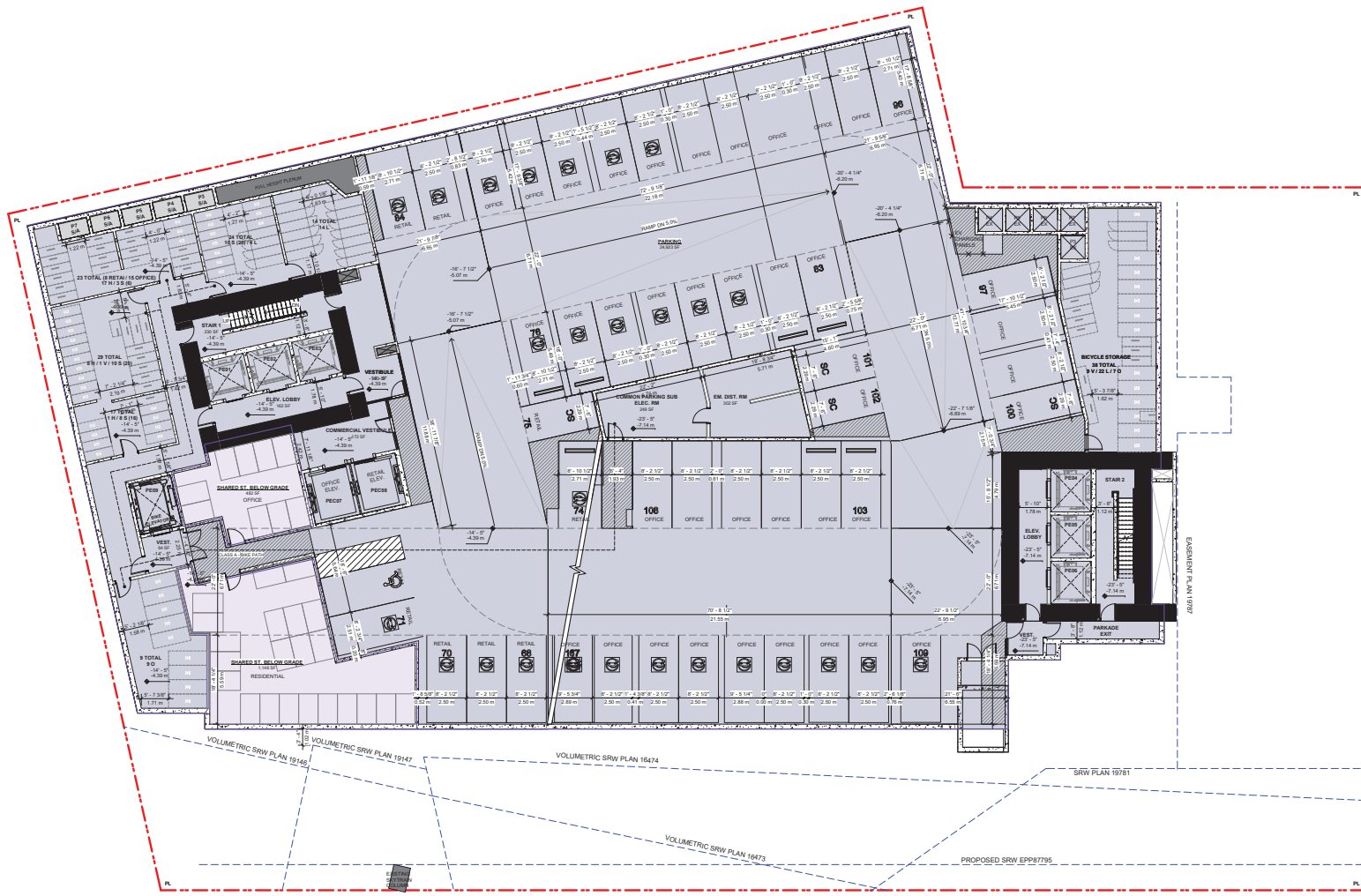
1220 STATION STREET
 Vancouver, BC
 FSR AREA PLAN
 LEVEL P4

Scale: 1/8" = 1'-0"
 Date: 2024-08-14

FSR205

FSR AREA
 [Blue Box] PARKING
 [Light Blue Box] SHARED ST. BELOW GRADE
 [White Box] SHARED ST. BELOW GRADE

LEVEL P3 - AREA SUMMARY		
USE	AREA (SQ FT)	METRIC (SQ M)
PARKING	24,207 SQ FT	2,245 SQ M
SHARED ST. BELOW GRADE	1,000 SQ FT	93 SQ M



STATION STREET

TERMINAL AVENUE

1 FSR - AREA PLAN LEVEL P3
 SCALE: 1/8" = 1'-0"



1220 STATION STREET



1220 Station Street
 Vancouver, BC

Project:
 FSR AREA PLAN
 LEVEL P3

Scale: 1/8" = 1'-0"

Date: 2024-01-11

Author: MCM

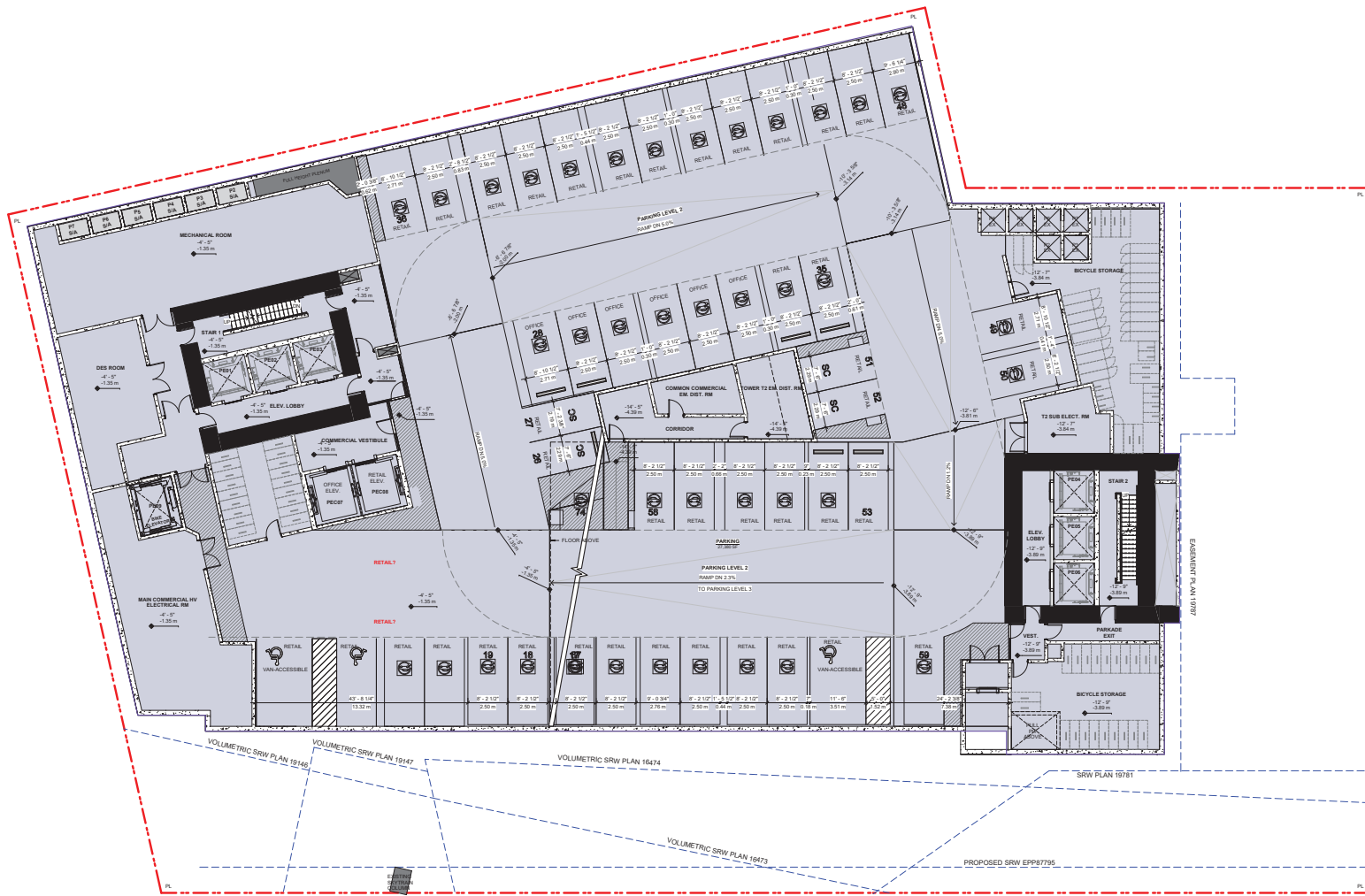
FSR206

FSR AREA



LEVEL P2 - AREA SUMMARY	
USE	AREA
PARKING	27 585 SF / 2 544 m ²

STATION STREET



TERMINAL AVENUE

1 FSR - AREA PLAN LEVEL P2
SCALE: 1/8" = 1'-0"



1220 STATION STREET



1220 Station Street
Vancouver, BC
Project
FSR AREA PLAN
LEVEL P2

Date: 2025-04-02

Scale: 1/8" = 1'-0"

Sheet: 02/04

FSR207

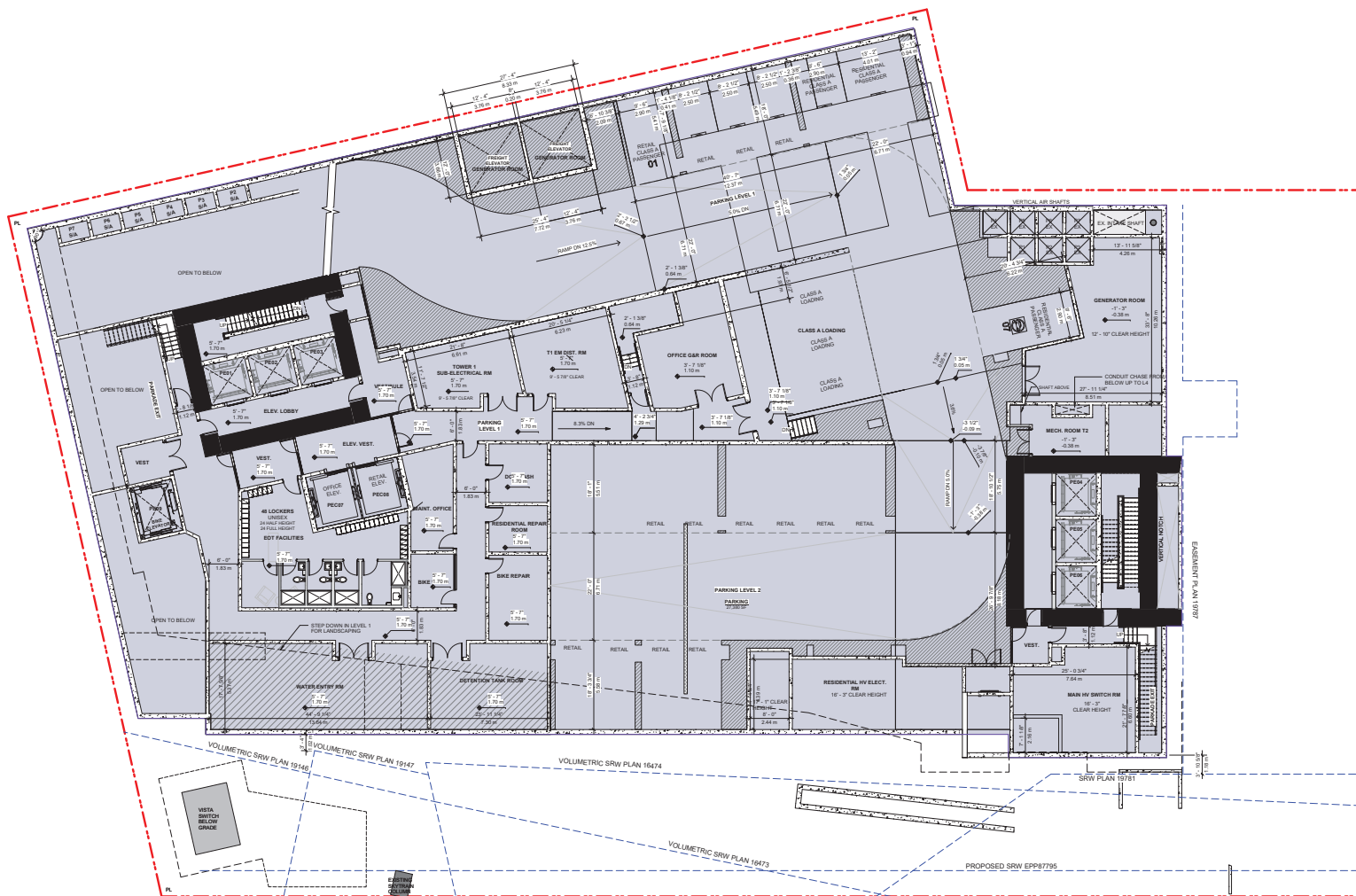
FSR AREA



LEVEL P1 - AREA SUMMARY	
USE	AREA
PARKING	27,382 SF / 2,544 m ²

STATION STREET

TERMINAL AVENUE



1 FSR - AREA PLAN LEVEL P1
SCALE: 1/8" = 1'-0"



1220 STATION STREET
1220 Station Street
Vancouver, BC
Project
FSR AREA PLAN
LEVEL P1

Sheet: 102-102
Date: 2024-01-11

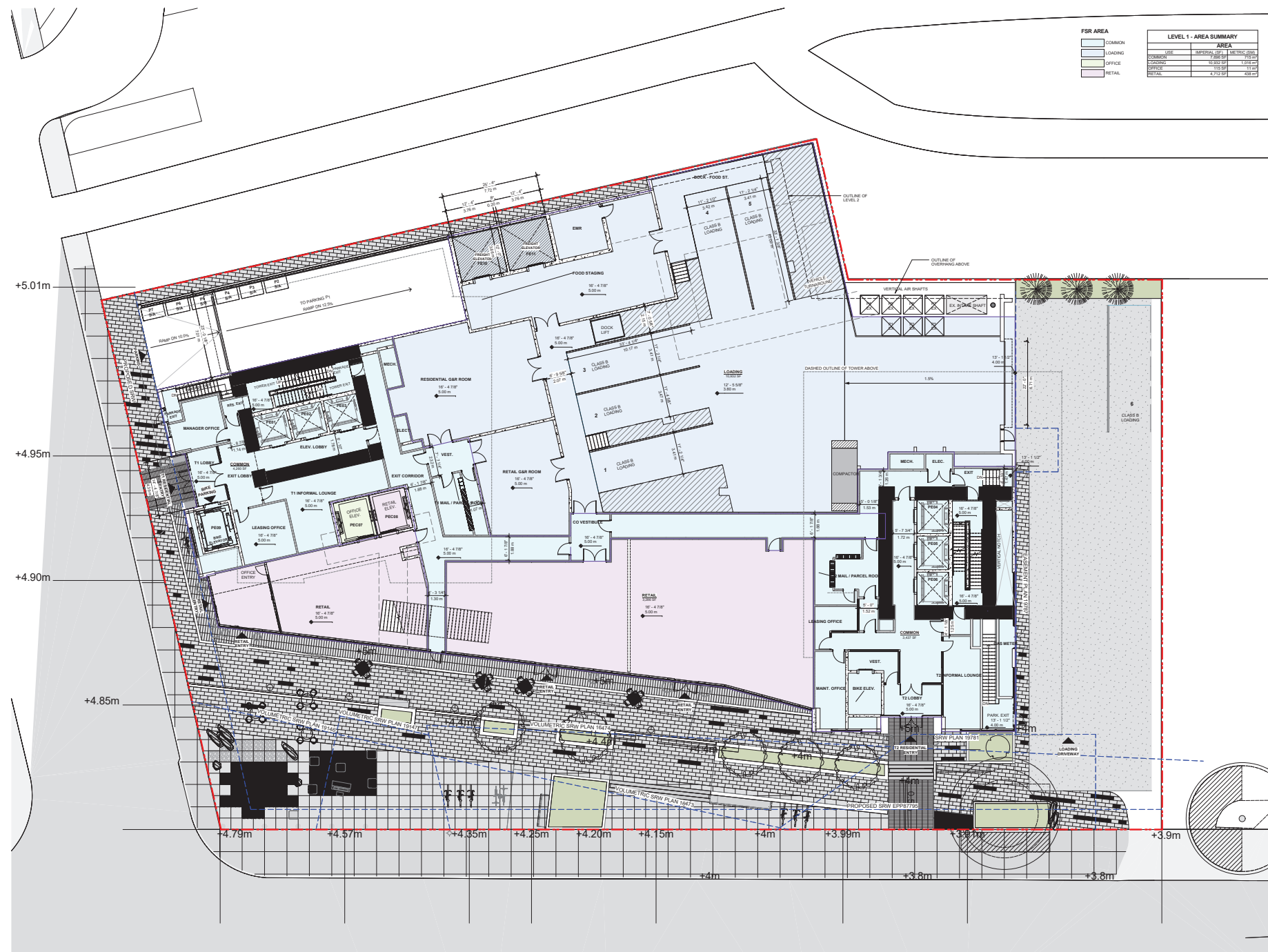
FSR208

FSR AREA

COMMON
LOADING
OFFICE
RETAIL

LEVEL 1 - AREA SUMMARY

USE	APPROX. SQ. FT. (SQ. M.)	ELECTRIC CODE
COMMON	7,855 SF (726 m ²)	
LOADING	15,252 SF (1,412 m ²)	
OFFICE	13,557 SF (1,257 m ²)	
RETAIL	4,212 SF (391 m ²)	



1 FSR - AREA PLAN LEVEL 1
SCALE: 1/8" = 1'-0"



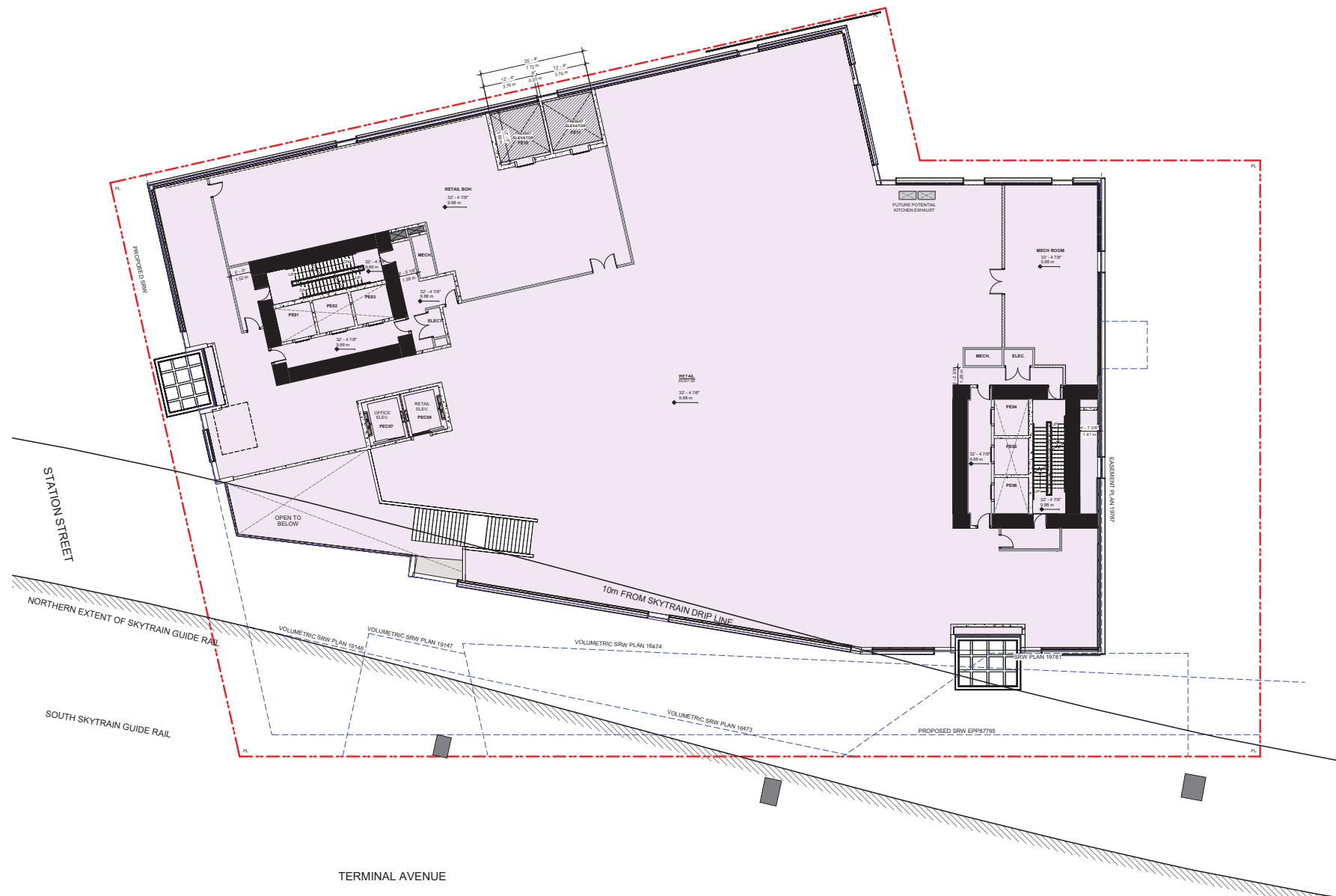
1220 STATION STREET
Vancouver, BC
1220 Station Street
Vancouver, BC
Project:
FSR AREA PLAN
LEVEL 1

FSR AREA



LEVEL 2 - AREA SUMMARY

USE	AREA
RETAIL	26,057 SF / 2,415 m ²



1 FSR - AREA PLAN LEVEL 2

SCALE: 1/8" = 1'-0"



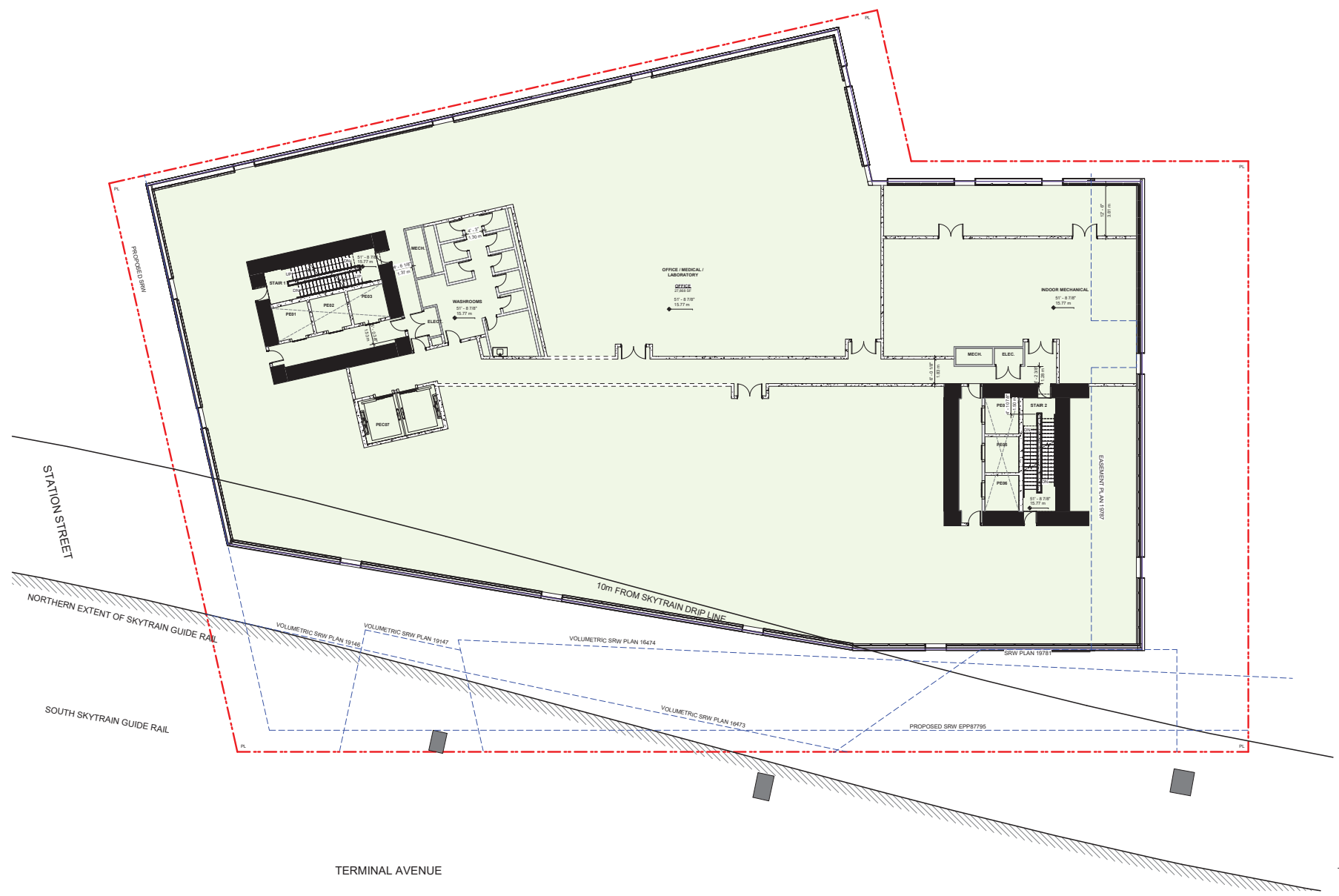
1220 STATION STREET
Vancouver, BC
Project:
FSR AREA PLAN
LEVEL 2

FSR210

FSR AREA

OFFICE

LEVEL 3 - AREA SUMMARY	
USE	AREA
OFFICE	27,000 SF / 2,500 m ²



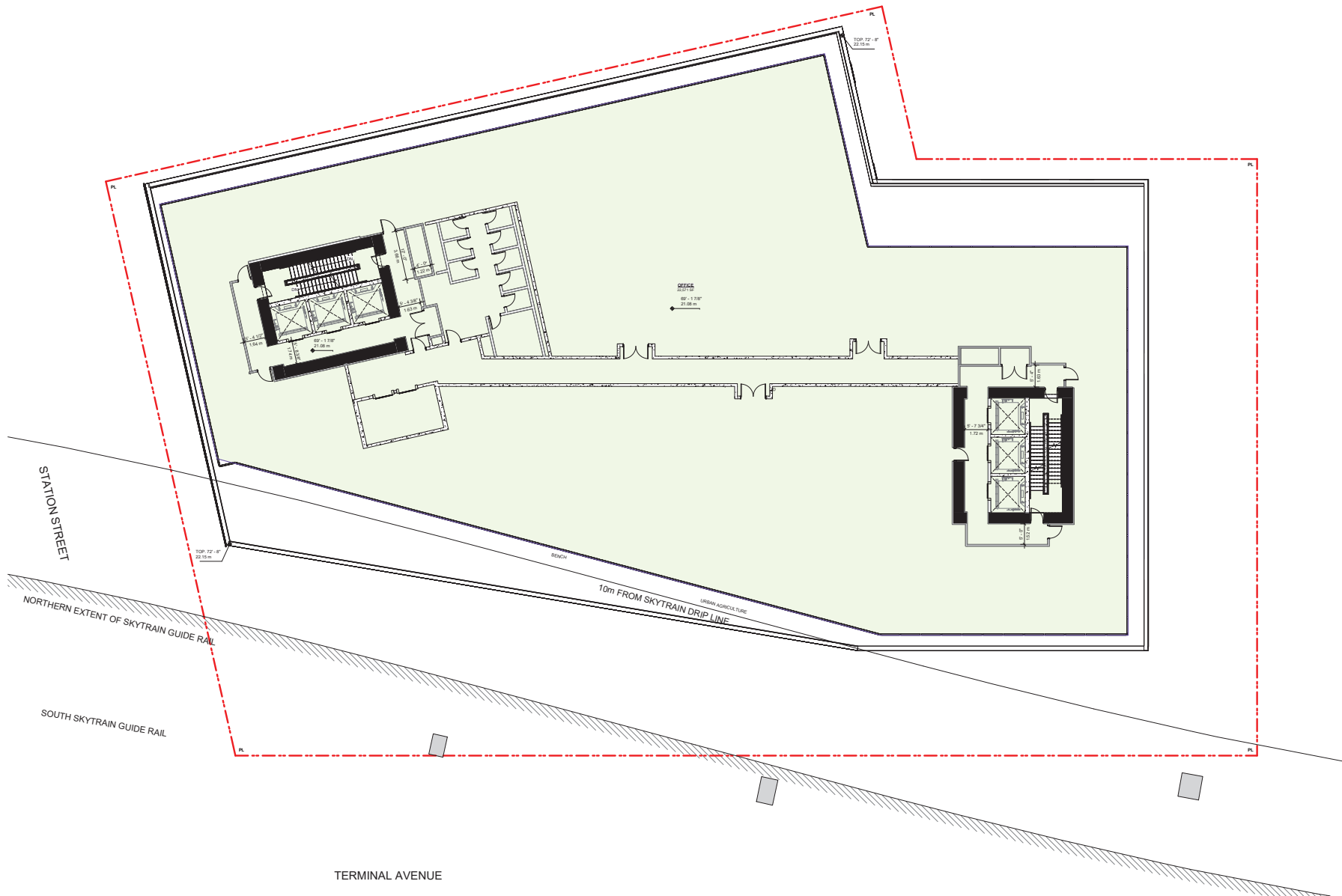
1 FSR - AREA PLAN LEVEL 3
 SCALE: 1/8" = 1'-0"

1220 STATION STREET
 Vancouver, BC
 FSR AREA PLAN
 LEVEL 3

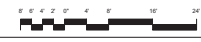


FSR AREA
 OFFICE

LEVEL 4 - AREA SUMMARY	
USE	AREA
OFFICE	22,977 SF / 2,167.7 m ²



1 FSR - AREA PLAN LEVEL 4
 SCALE: 1/8" = 1'-0"



1220 STATION STREET



1220 Station Street
 Vancouver, BC
 Project
 FSR AREA PLAN
 LEVEL 4

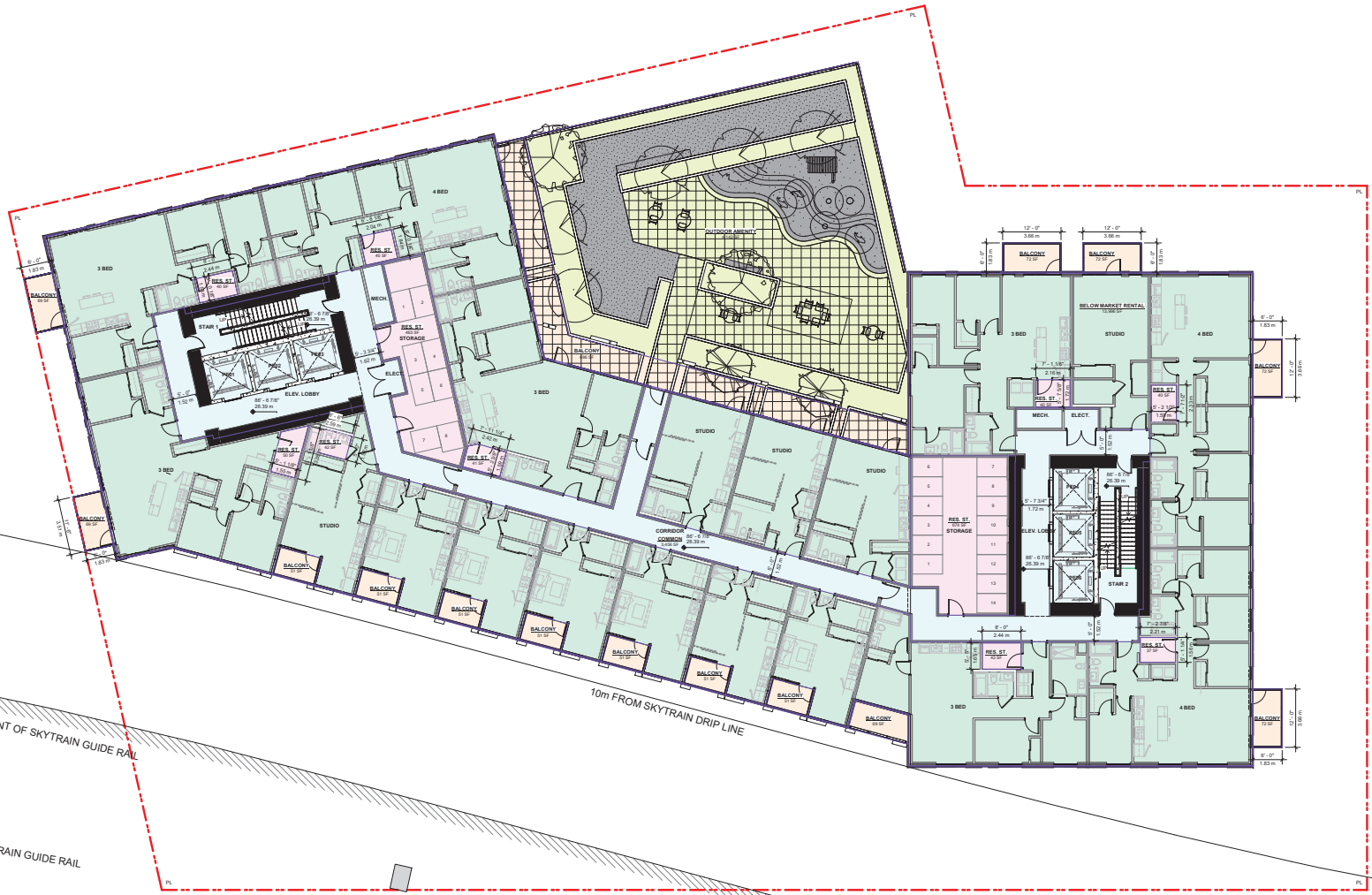
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 Date: 2024-03-11

FSR212

FSR AREA

- BALCONY
- BELOW MARKET RENTAL
- COMMON
- OUTDOOR AMENITY
- RES. ST.

LEVEL 5 AREA SUMMARY	
USE	AREA
BALCONY	4,975 SF 143.07
BELOW MARKET RENTAL	13,958 SF 402.04
COMMON	3,428 SF 97.77
OUTDOOR AMENITY	4,142 SF 119.24
RES. ST.	1,510 SF 43.67



STATION STREET

NORTHERN EXTENT OF SKYTRAIN GUIDE RAIL

SOUTH SKYTRAIN GUIDE RAIL

10m FROM SKYTRAIN DRIP LINE

TERMINAL AVENUE

1 FSR - AREA PLAN LEVEL 5
 SCALE: 1/8" = 1'-0"



Revision: YYYYMMDD
 1 ISSUED FOR PERMITS 2024-01-11
 2 ISSUED FOR PERMITS 2024-04-11
 3 ISSUED FOR PERMITS 2024-04-11

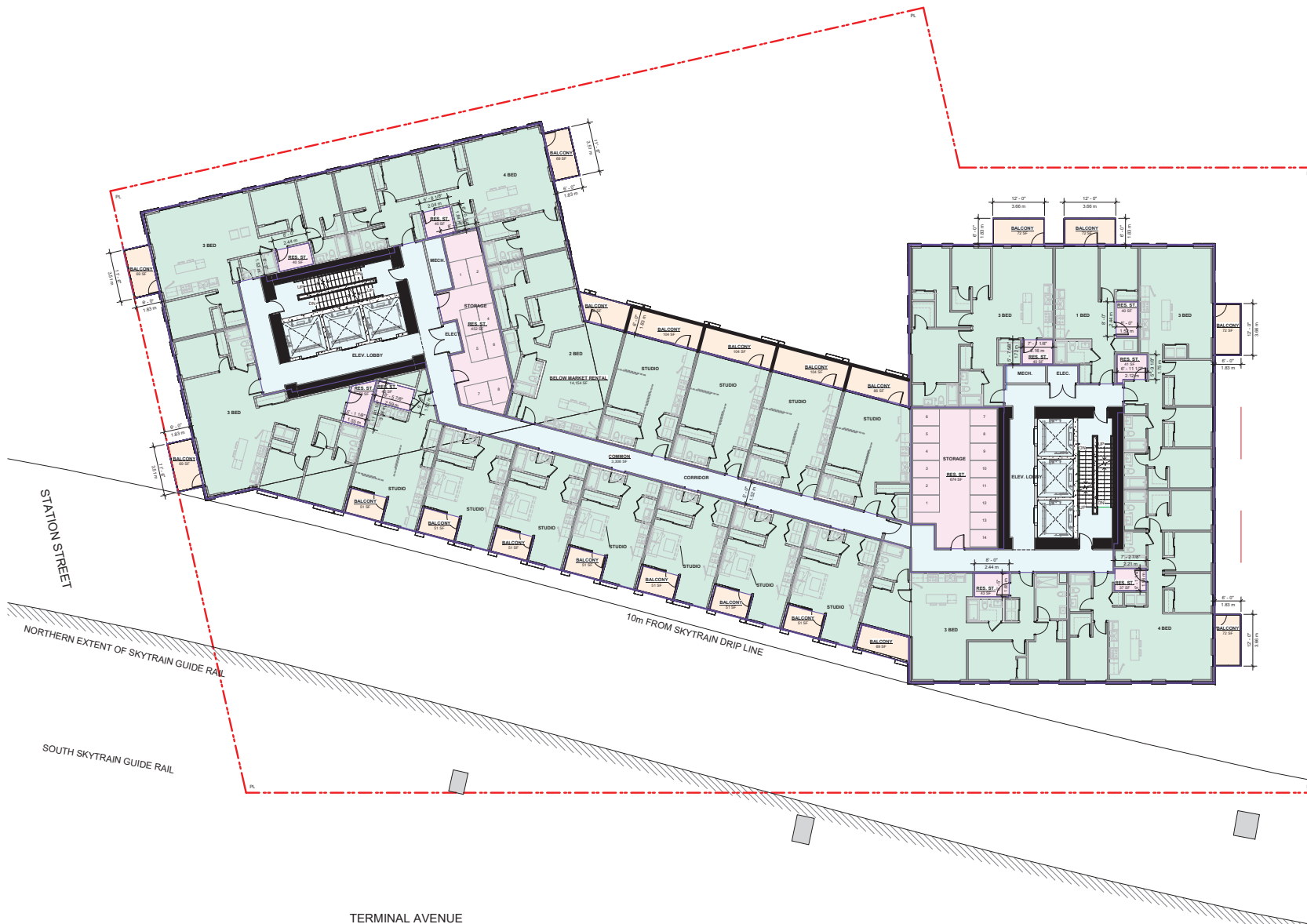
1220 STATION STREET
 Vancouver, BC
 FSR AREA PLAN
 LEVEL 5

FSR AREA

BALCONY
BALCONY MARKET
COMMON
RES. ST.

LEVEL 6-8 AREA SUMMARY

USE	AREA
BALCONY	1,588 SF
BALCONY MARKET RENTAL	11,125 SF
COMMON	3,338 SF
RES. ST.	1,252 SF



1 FSR - AREA PLAN LEVEL 8
 SCALE: 1/8" = 1'-0"



1220 STATION STREET



1220 Station Street
 Vancouver, BC
 Project

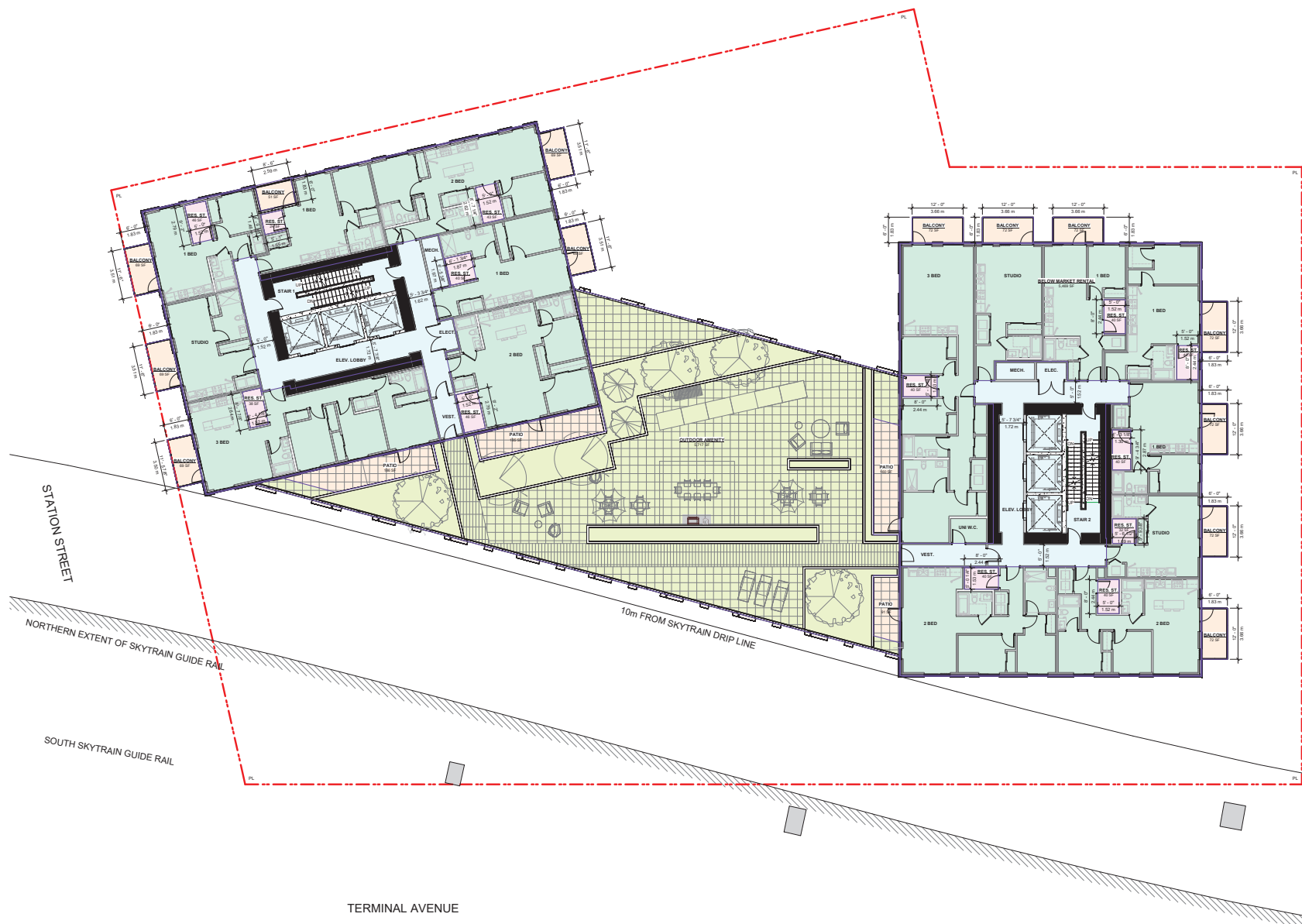
FSR AREA PLAN
 LEVELS 6-8

Scale: 1/8" = 1'-0"

DATE: 20240614

FSR214

FSR AREA		LEVEL 9 - AREA SUMMARY	
Color	Area Type	USE	AREA
Light Blue	BALCONY	APPROX. ST. 1	ELECTR. (250)
Light Green	BALCONY MARKET	BALCONY	1,522 SF
Light Yellow	COMMON	BALCONY MARKET/RENTAL	11,225 SF
Light Purple	RES. ST.	COMMON	2,739 SF
Light Orange	RES. ST.	OUTDOOR AMENITY	3,171 SF
Light Green	RES. ST.	RES. ST.	533 SF
Light Purple	RES. ST.		477*



2 FSR - AREA PLAN LEVEL 9
 SCALE: 1/8" = 1'-0"

1220 STATION STREET
 Vancouver, BC
 Project:
 FSR AREA PLAN
 LEVEL 9

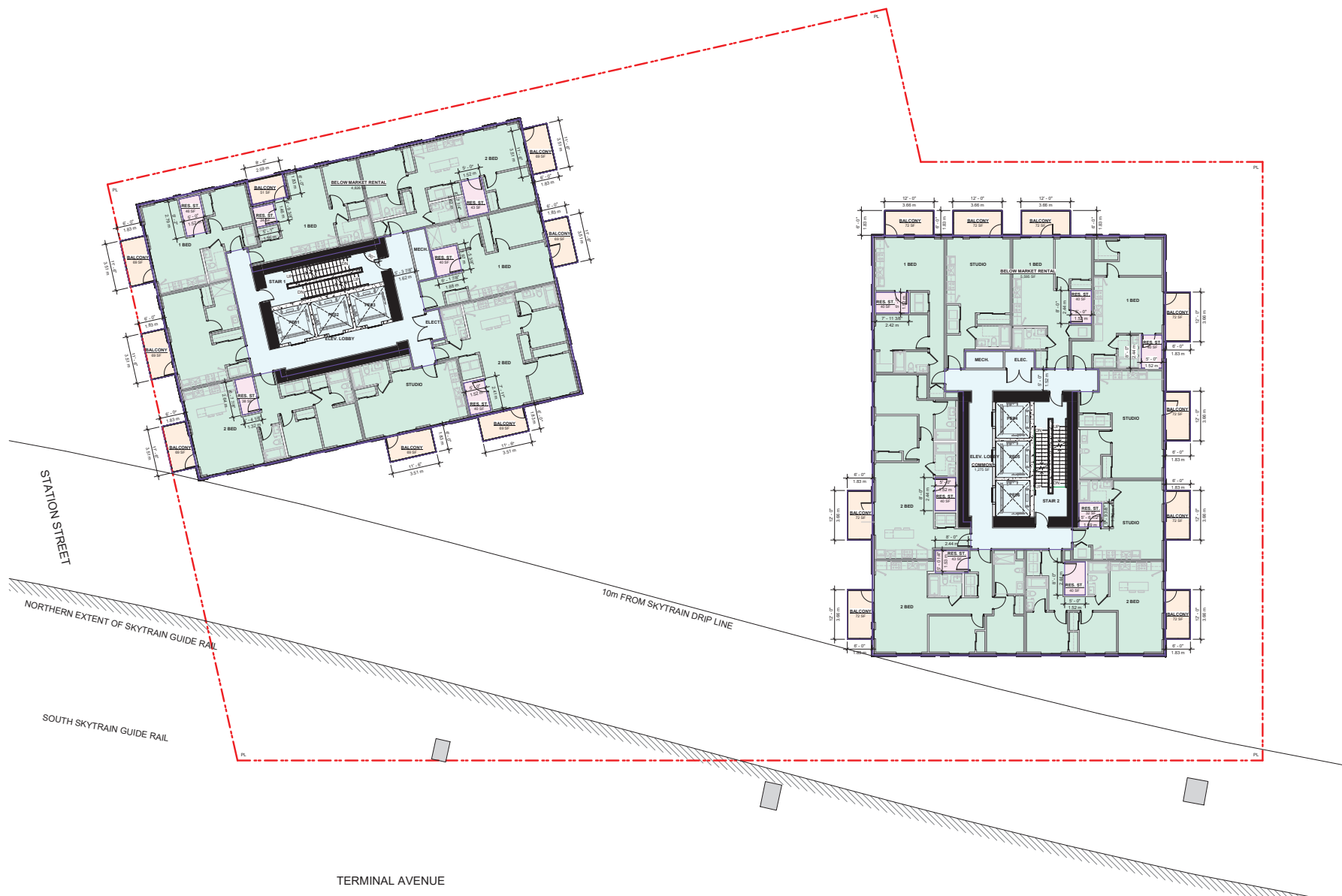


FSR AREA

BALCONY
BELOW MARKET RENTAL
COMMON
RES. ST.

LEVELS 10 AREA SUMMARY

USE	AREA
BALCONY	APPROX. SQ. FT. / ELECTRIC (SQM)
BALCONY	1,182 SF / 107.0
BELOW MARKET RENTAL	11,472 SF / 1,058.4
COMMON	2,576 SF / 239.2
RES. ST.	259 SF / 24.0



2 FSR - AREA PLAN LEVEL 10
 SCALE: 1/8" = 1'-0"

1220 STATION STREET
 Vancouver, BC
 FSR AREA PLAN
 LEVEL 10

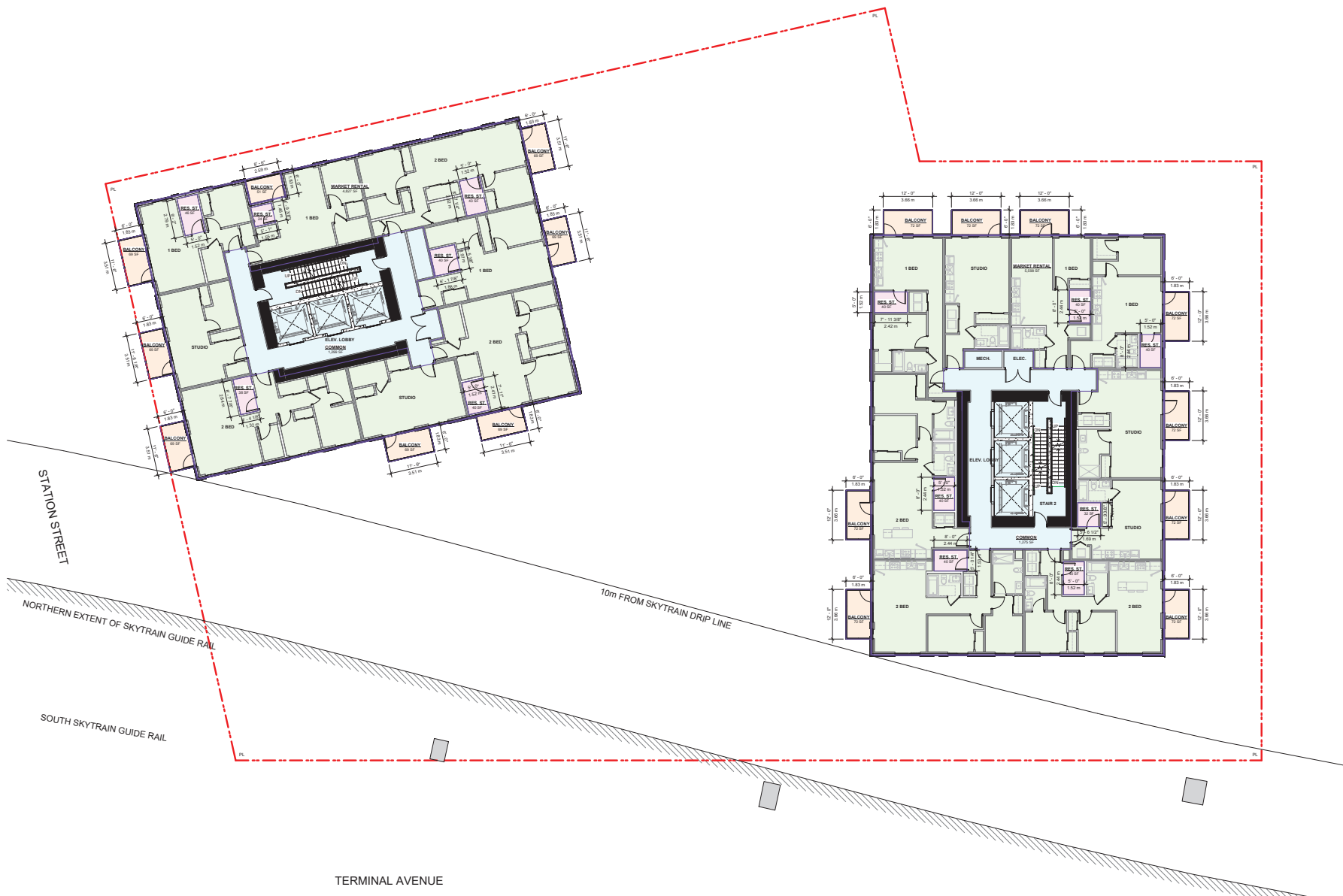


FSR AREA

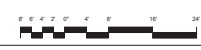
BALCONY
COMMON
MARKET RENTAL
RES. ST.

LEVELS 11-26 AREA SUMMARY

USE	AREA
BALCONY	APPROX. 2571 ELECTRIC (250)
COMMON	1,162 SF 110'
MARKET RENTAL	2,275 SF 228'
RES. ST.	10,420 SF 965'
	14,128 SF 1,313'



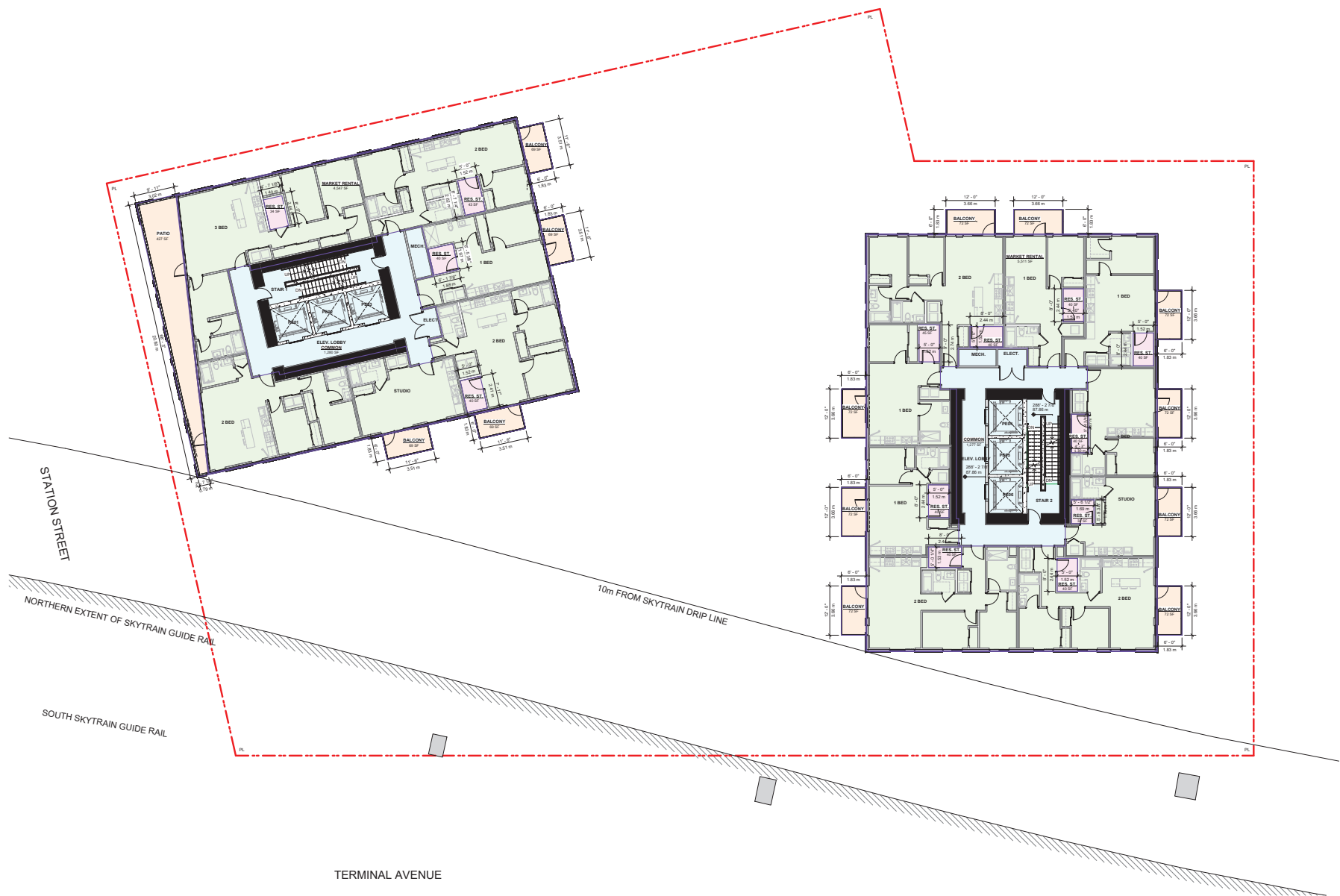
1 FSR - AREA PLAN LEVEL 11
 SCALE: 1/8" = 1'-0"



FSR AREA

- BALCONY
- COMMON
- MARKET RENTAL
- RES. ST.

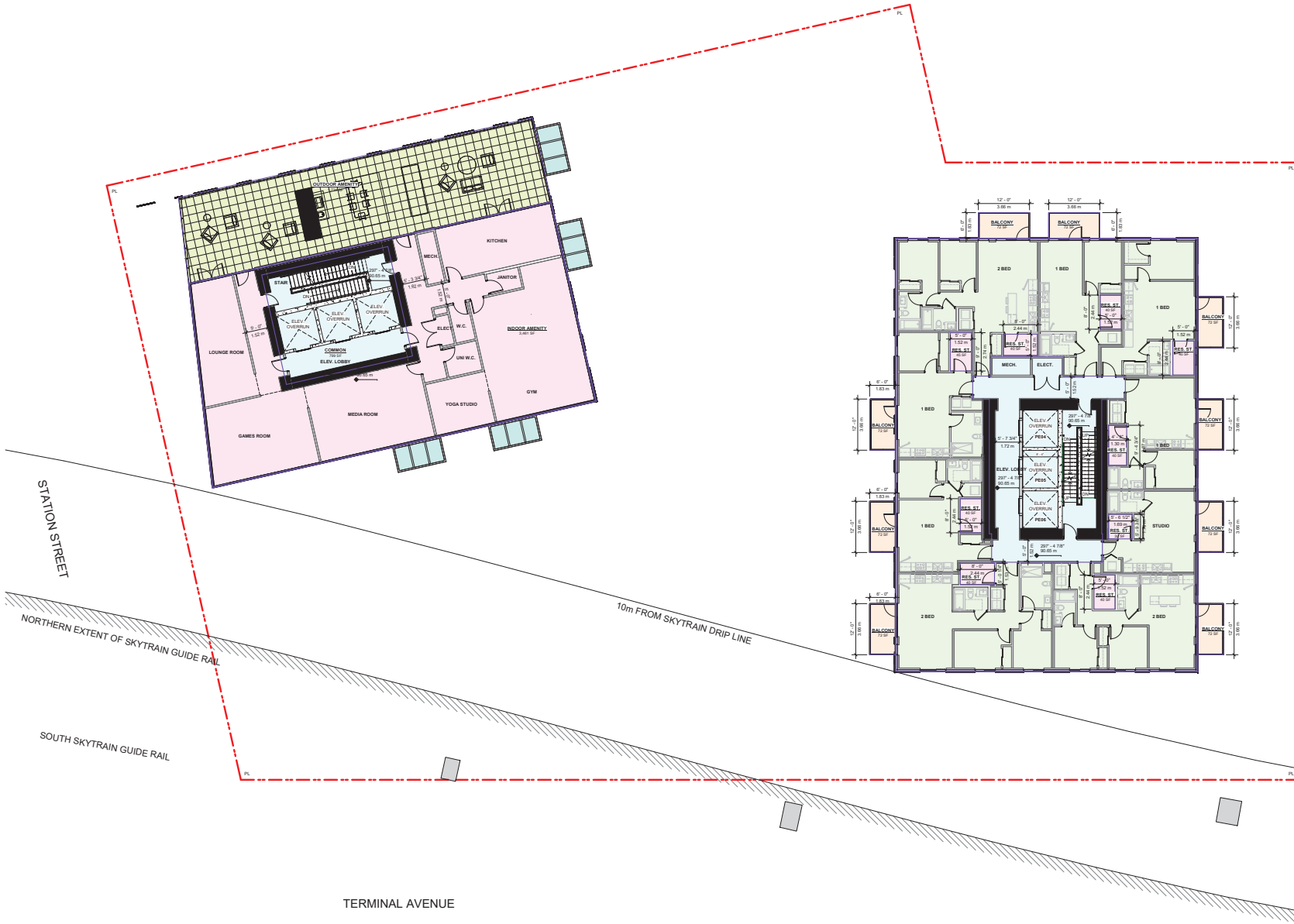
LEVEL 27 - AREA SUMMARY		
USE	IMPERIAL (SQ FT)	METRIC (SQ M)
BALCONY	1,302 SQ FT	120 m ²
COMMON	2,322 SQ FT	215 m ²
MARKET RENTAL	10,208 SQ FT	934 m ²
RES. ST.	914 SQ FT	85 m ²



2 FSR - AREA PLAN LEVEL 27
 SCALE: 1/8" = 1'-0"

1220 STATION STREET
 VICTORIA, BC
 FSR AREA PLAN
 LEVEL 27

FSR AREA	LEVEL 28 - AREA SUMMARY	
	AREA	AREA
	IMPERIAL SQ FT	METRIC SQ M
BALCONY	452 SF	42.04
COMMON	2,776 SF	257.04
INDOOR AMENITY	2,215 SF	206.04
MARKET RENTAL	1,411 SF	131.24
OUTDOOR AMENITY	1,934 SF	179.04
RES. ST.	527 SF	49.04



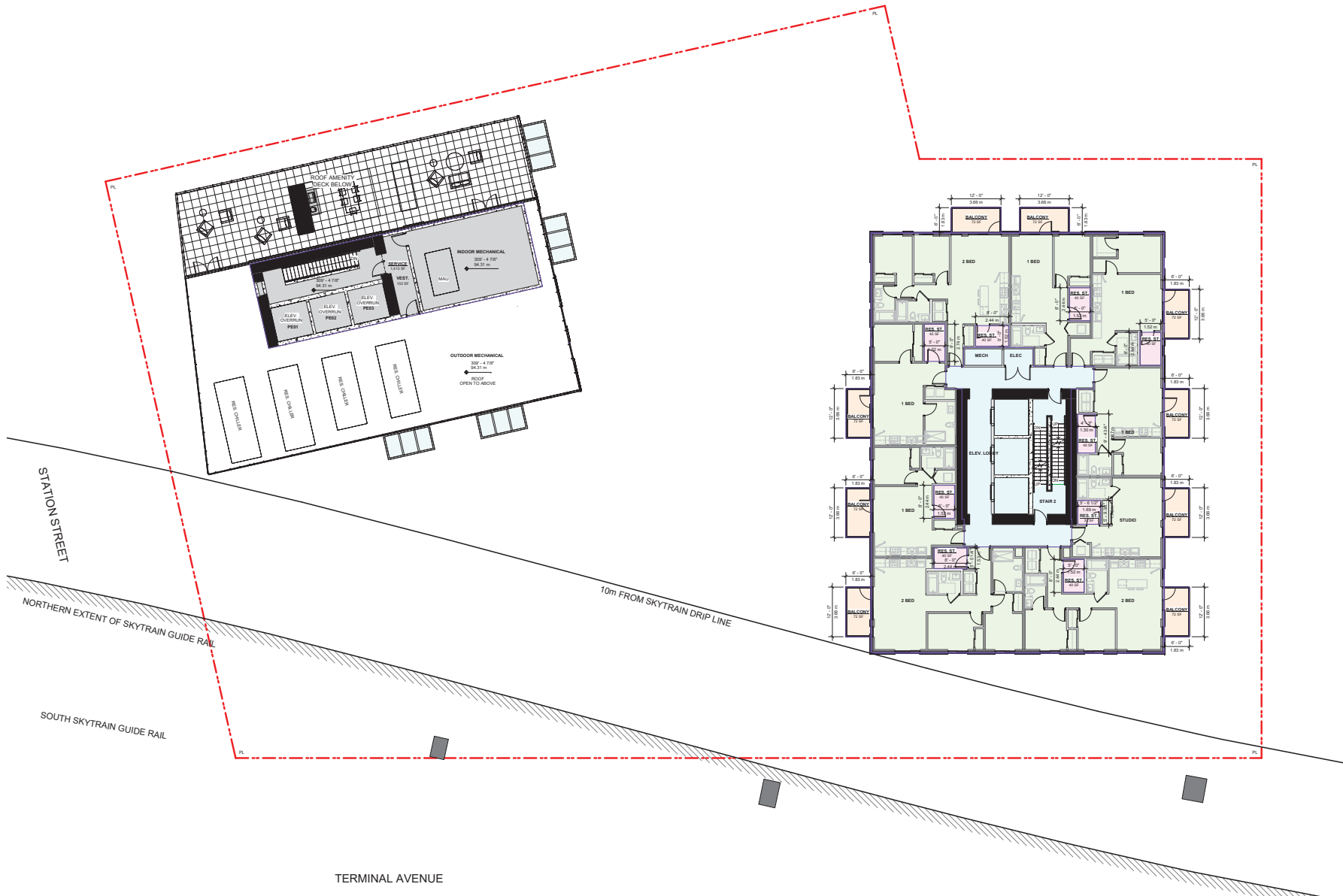
1 FSR - AREA PLAN LEVEL 28
 SCALE: 1/8" = 1'-0"

FSR AREA

BALCONY
COMMON
MARKET RENTAL
RES. ST.
SERVICE

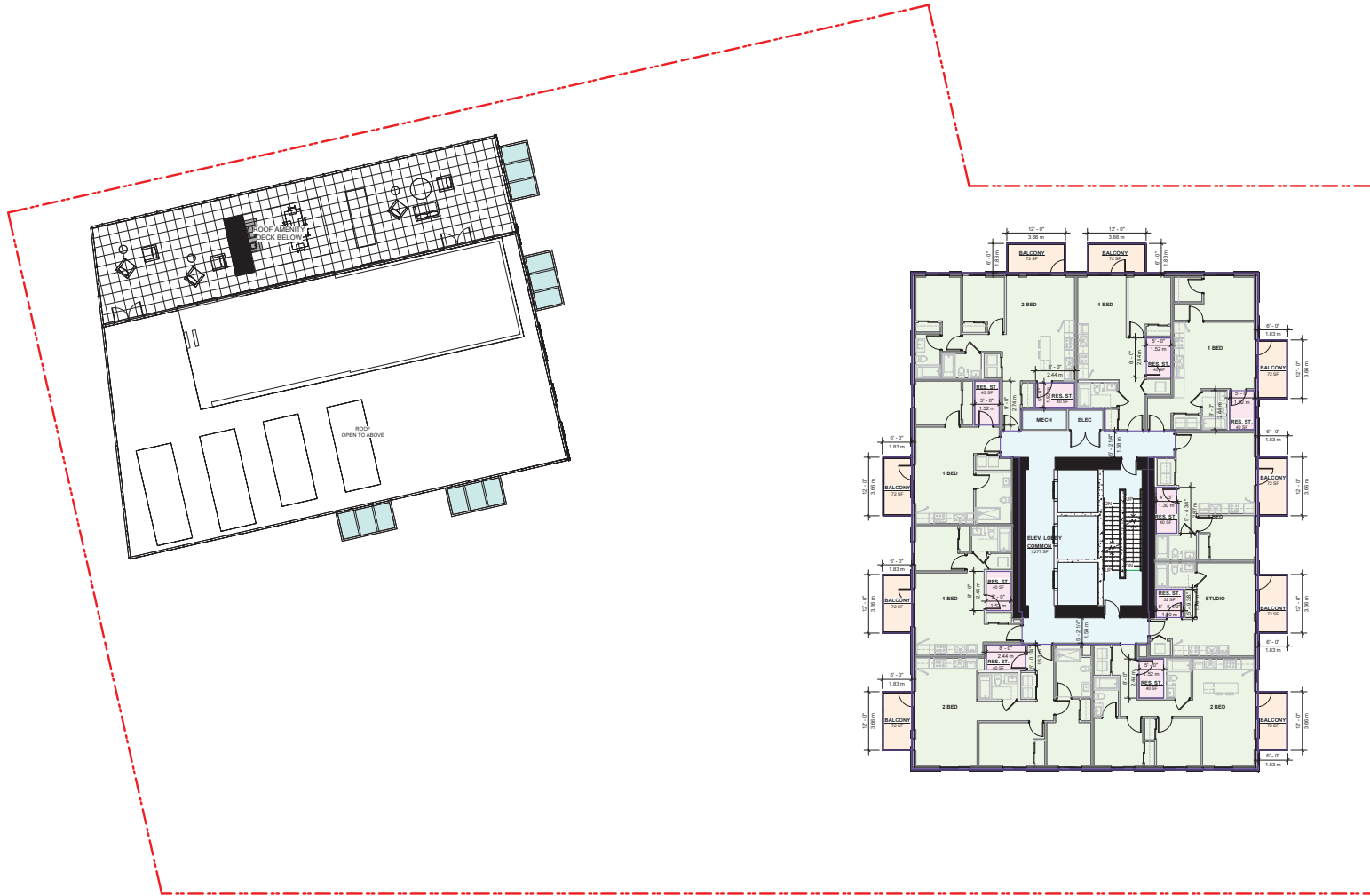
LEVEL 29 - AREA SUMMARY

USE	AREA	AREA	AREA
	IMPERIAL SQ. FT.	METRIC SQ. M.	
BALCONY	499 SF	46.20	
COMMON	1,277 SF	118.14	
MARKET RENTAL	5,213 SF	482.84	
RES. ST.	382 SF	35.44	
SERVICE	1,413 SF	131.07	

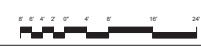


1 FSR - AREA PLAN LEVEL 29
 SCALE: 1/8" = 1'-0"

FSR AREA		LEVEL 30 - AREA SUMMARY	
		AREA	
BALCONY	548 SF	50.5 m ²	
COMMON	277 SF	25.6 m ²	
MARKET RENTAL	5,511 SF	512 m ²	
RES. ST.	381 SF	35 m ²	



1 FSR - AREA PLAN LEVEL 30
 SCALE: 1/8" = 1'-0"





Revisions

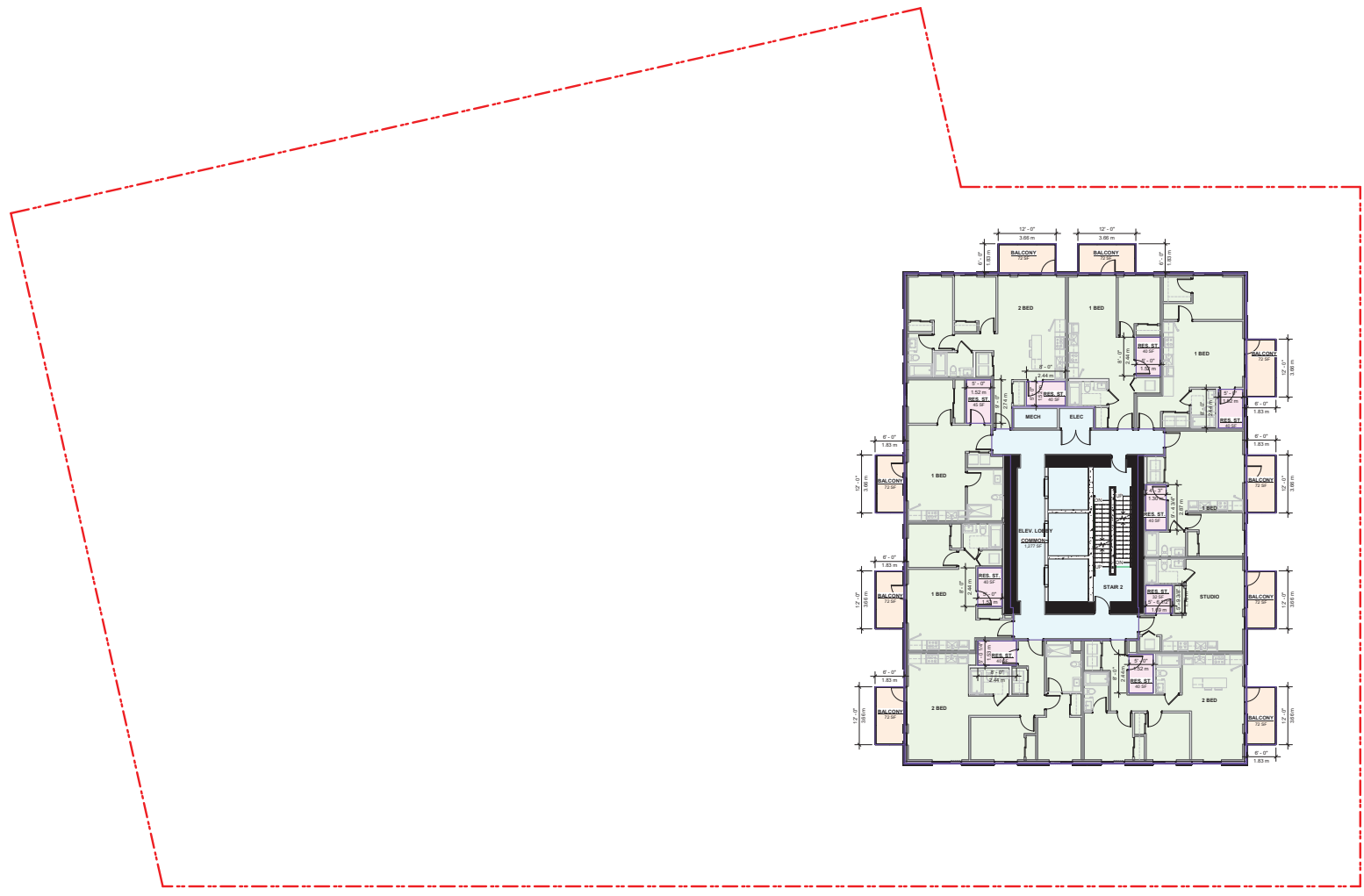
Revisions	YYYYMMDD
1 ISSUED FOR REZONING 2024-03-11	
2 ISSUED FOR REZONING 2024-08-14	
3 ISSUED FOR REZONING 2025-04-03	

FSR AREA

BALCONY
COMMON
MARKET RENTAL
RES. ST.

LEVEL 31-35 - AREA SUMMARY

TYPE	AREA	
	IMPERIAL (SQ FT)	METRIC (SQ M)
BALCONY	452 SQ FT	42.04
COMMON	3,277 SQ FT	303.94
MARKET RENTAL	5,511 SQ FT	512.04
RES. ST.	397 SQ FT	37.02



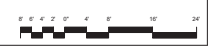
1 **FSR - AREA PLAN LEVEL 31-35**
 SCALE: 1/8" = 1'-0"

1220 STATION STREET



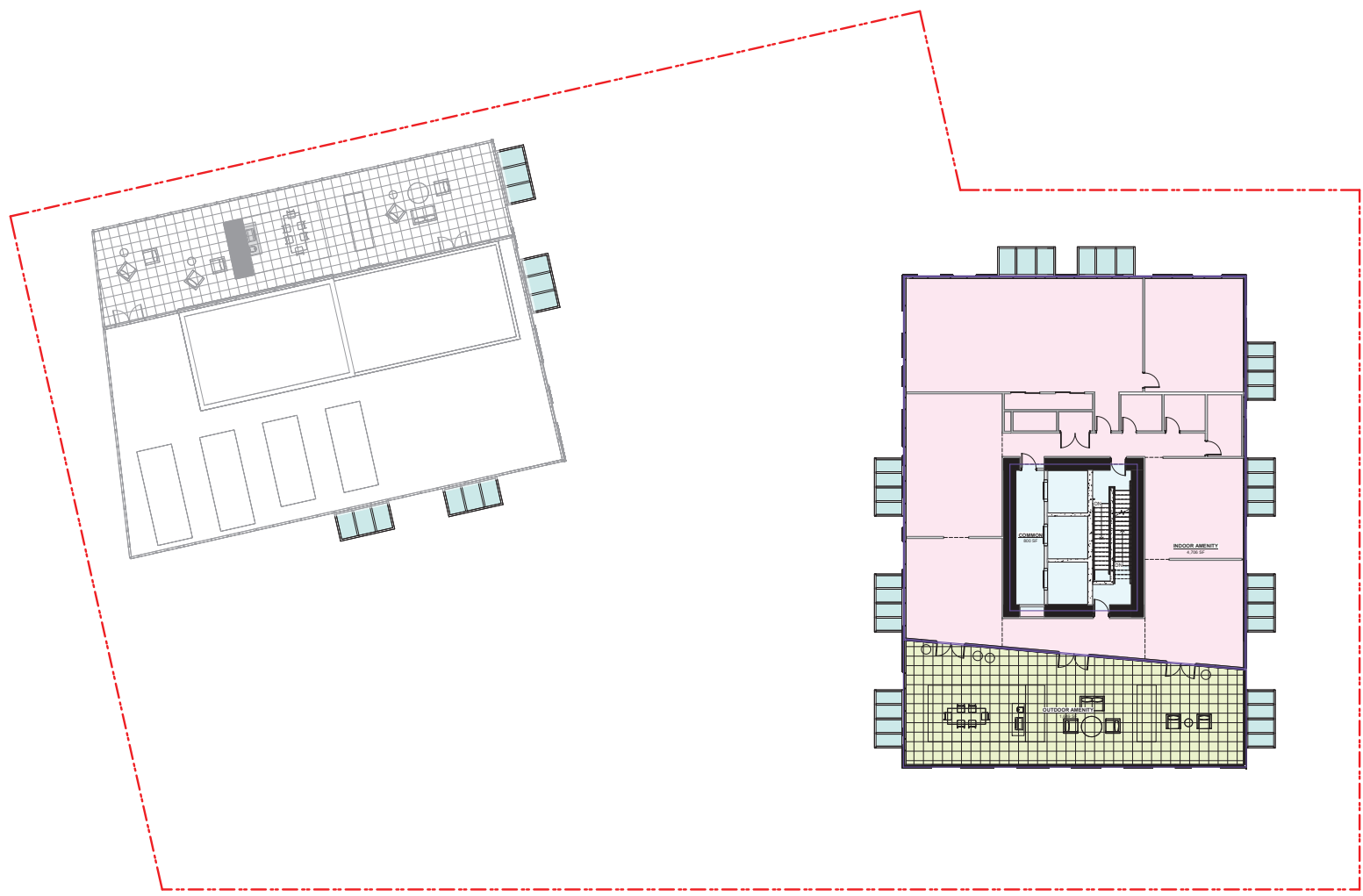
1220 Station Street
 Vancouver, BC
 Project
 FSR AREA PLAN -
 LEVELS 31-35

Sheet: 1/27
 Scale: 1/8" = 1'-0"



FSR222

FSR AREA		LEVEL 36 - AREA SUMMARY	
DOMAIN	AREA	USE	AREA (SQ FT) METRIC (SQM)
INDOOR AMENITY	INDOOR AMENITY	COMMON	302 SQ FT 28.07 SQM
INDOOR AMENITY	INDOOR AMENITY	INDOOR AMENITY	4,716 SQ FT 438.07 SQM
OUTDOOR AMENITY	OUTDOOR AMENITY	OUTDOOR AMENITY	1,049 SQ FT 97.07 SQM



1 LEVEL 36
 SCALE: 1/8" = 1'-0"

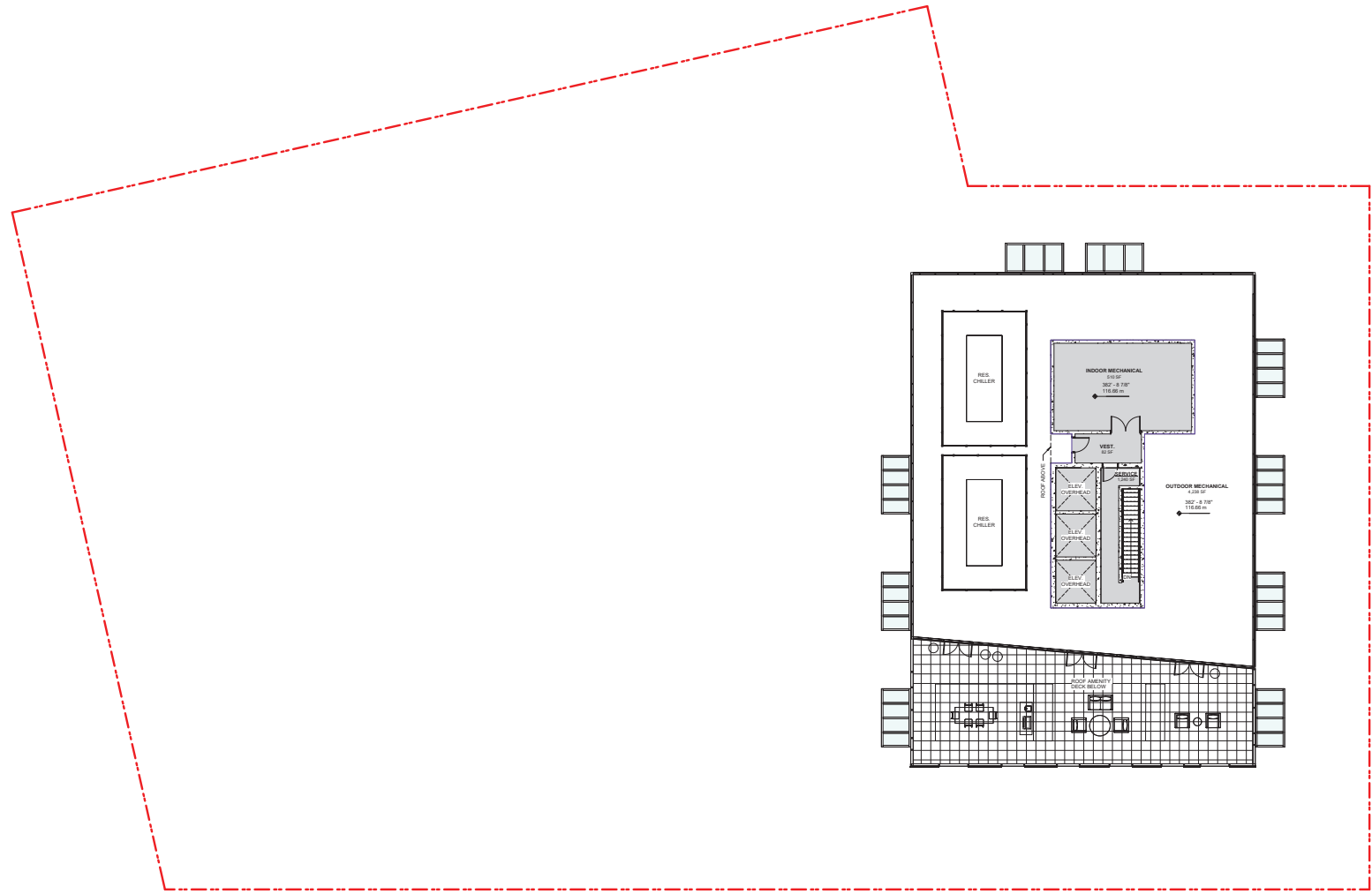


1220 STATION STREET
 1220 Station Street
 Vancouver, BC
 Project:
 FSR AREA PLAN
 ROOF AMENITY /
 LEVEL 36

Sheet: 1220-03
 Date: 2024-03-11

FSR223

FSR AREA	LEVEL MECH. EAST AREA SUMMARY		
	AREA	USE	AREA
█ SERVICE	INDOOR MECH. ELEV. 10.50'	MECH. ELEV. 10.50'	112.00 m ²
	OUTDOOR MECH. ELEV. 10.50'	MECH. ELEV. 10.50'	112.00 m ²



1 FSR AREA PLAN - LEVEL MECH EAST TOWER
 SCALE: 1/8" = 1'-0"



Appendix B: Landscape Drawings

DRAWING LIST

L-0.00	COVER SHEET
T-1.00	EXISTING TREE MANAGEMENT PLAN
L-1.00	LEVEL 1 - SITE PLAN
L-1.01	LEVEL 5 - SITE PLAN
L-1.02	LEVEL 9 - SITE PLAN
L-1.03	LEVEL 28 - SITE PLAN
L-1.04	ROOF - SITE PLAN
L-2.00	SECTIONS
L-3.00	DETAILS

LANDSCAPE NOTES

- All work shall meet or exceed the requirements as outlined in the current Edition of the Canadian Landscape Standard.
- Plant sizes and related container classes are specified according to the Canadian Landscape Standard current Edition. For container classes #3 and smaller, plant sizes shall be as shown in the plant list and the Standard; for all other plants, both plant size and container class shall be as shown in the plant list. Specifically, when the plant list call for #5 class containers, these shall be as defined in the CNLA (ANSI) Standard.
- Area of search for specified plant material shall include the Lower Mainland of British Columbia, Vancouver Island, Washington and Oregon States.
- All plant material used on this project shall be hardy in this climate. Plant types have been selected with this as a primary criteria. This Contractor shall guarantee that plant material supplied has equal provenance, i.e. it is developed from cuttings or seeds collected in an area of similar climatic characteristics.
- Substitutions in plant material will not be considered unless written proof is submitted thirty (30) days prior to scheduled installation stating a specified plant cannot be obtained within the specified area of search.
- All trees to be staked in accordance with CNLA Standards.
- For all existing on site services and survey symbols refer to survey drawings.
- All landscaping and treatment of the open portions of the site shall be completed in accordance with the approved drawings within six (6) months of the date of issuance of any required occupancy permit, or any use or occupancy of the proposed development not requiring an occupancy permit, and thereafter permanently maintained in good conditions.

ADDITIONAL NOTES

- "This plan is "NOT FOR CONSTRUCTION" and is to be submitted for review to Engineering Services a minimum of 8 weeks prior to the start of any construction proposed for public property. No work on public property may begin until such plans receive "For Construction" approval and related permits are issued. Please contact Engineering, Development and Major Projects and/or your Engineering, Building Site Inspector for details."
- "Tree species, final spacing, quantity and location to the satisfaction of the General Manager of Engineering Services. New trees must be of good standard, minimum 6cm caliper, and installed with approved root barriers, tree guards and appropriate soil volumes. Installation of Engineered Soil may be required to obtain appropriate soil volumes based on site conditions. Root barriers shall be of rigid construction, 8 feet long and 18 inches deep, centre on each street tree adjacent to the sidewalk and dry off-street bike facility. Planting depth of root ball must be below sidewalk grade. Contact Park Board at postdevelopment.trees@vancouver.ca for inspection after tree planting completion."
- "Off-site assets are to be constructed to the satisfaction of the General Manager of Engineering Services and as per the approved City geometric design, with the final design and location to be determined once the City geometric is received."
- Note to Applicant: Drawings submitted as part of the Development Permit application will be preliminary with appropriate placeholders, and the final off-site geometric design will be provided by the City of Vancouver. An Engineering Project Coordinator will engage the Developer to facilitate the delivery of any City design after Development Permit issuance.
- "The required Green Infrastructure Improvements on 520 West 29th Avenue will be as per City-issued design."
- Poles and guy wires.
Note to Applicant: Poles and guywires that are to be removed or relocated must be called out and the existing and proposed locations shown. Letters must be provided from the appropriate public utility companies that confirm that pole relocation proposed is possible.

IRRIGATION NOTES

- All on-site "Soft Landscape Areas" are to be irrigated with high-efficiency automatic irrigation systems, including rain sensors, to IAABC Standards.
- Irrigation stub-outs and hose bib locations will be provided to mechanical and electrical consultants for coordination.
- Irrigation system is a design-build. Landscape contractor to provide irrigation design/layout for review by Landscape Consultant for approval.
- No visible irrigation lines will be accepted.
- All irrigation boxes to be equipped with quick couplers.
- Irrigation controller to be located in mechanical room.

BIRD FRIENDLY STRATEGY

Elements of the landscape design and planting selection have been thoughtfully considered to support the City of Vancouver's Bird Friendly Strategy Design Guidelines. The use of native tiered planting materials will provide a variety of textures and heights that will support bird habitat. The plantings should provide opportunities for nesting, food sources, pollination support, shelters as well as protection.

A landscape consideration that will make this proposed development more bird friendly will be the expansion of the urban tree canopy. The development proposes to add trees on the ground, level 5 and level 9. These trees will grow up to provide shade, perching and nesting opportunities.

The proposed landscape planting incorporates canopy and habit stratification with tree(s), shrub(s) and groundcover planting, utilizing plants that attract birds, insects and includes native and non-native/non-invasive plants.

MATERIALS LEGEND

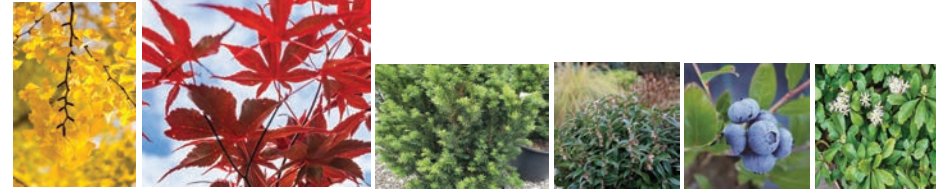
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	CIP CONCRETE VEHICULAR
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	SLAB PAVER BY BELGARD - CHARCOAL TEXADA 24 X 24 X 2"
	SLAB PAVER BY BARKMAN - STERLING TERRACE 24 X 24 X 2"
	BROADWAY 65MM PLANK PAVER - RUNNING BOND 12 X 24 X 3 (colouring may differ)
	BROADWAY 65MM PLANK PAVER - RUNNING BOND 12 X 24 X 3 (colouring may differ)
	MODULINE PLANK PAVER - CHARCOAL 4 X 24 X 4
	MODULINE PLANK PAVER - GREY 4 X 24 X 4
	SYNTHETIC TURF DOG RUN
	RUBBER SURFACING (colouring may differ)
	GRANULAR MATERIAL DRIP STRIP / FEATURE

SOFTSCAPE

	PLANTING BEDS REFER TO PLANT LIST
	SOIL
	EVERGREEN HEDGE REFER TO PLANT LIST

PLANTING SCHEDULE - OVERALL (trees only for rezoning)

TREES symbol	qty	botanical name	common name	size	notes
	06	Acer buergerianum	Trident maple	1.8m (6'-0") height	balled & burlapped
	04	Acer circinalum	Vine maple	1.8m (6'-0") height	balled & burlapped, multistem
	01	Acer griseum	Paperbark/Blood-mark maple	2m (6'-6") height,	balled & burlapped, multistem, specimen
	05	Acer rubrum 'Bowhall'	Bowhall red maple	6cm caliper	balled & burlapped
	02	Ginkgo biloba 'Ladee butterfly'	Jade Butterfly ginkgo	#5 pot	spaced as shown
	03	Picea pungens var glauca festigiata	Columnar Colorado spruce	1.8m (6'-0") height	balled & burlapped
	05	Populus tremuloides	Quaking aspen, trembling	5cm caliper min.	balled & burlapped



PLANTING INSPIRATION



RAISED PLATFORM - KIDS PLAY



FEATURE PAVING



SITE FURNISHING - BENCH



OUTDOOR AMENITY / MULTI-PURPOSE SPACE



BOARDFORM CONCRETE



SITE FURNISHING - BIKE RACK



13	2025-08-12	Issue for rezoning
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1	2024-04-26	Issue for client review

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 Revisions:
 Stamp:

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 152-1537 West 5th Avenue Vancouver B.C. V6L 1N6
 Client: F 604.684.4611 | F 604.684.0577 | www.dk.la.bc.ca

GWL REALTY ADVISORS

Project:
1220 STATION STREET

1220 STATION STREET, VANCOUVER BC

Drawn by: AGB
 Checked by: SV
 Date: 16 JUN 2023
 Scale: as shown

Drawing Title:

COVER SHEET

Project No.:
24012

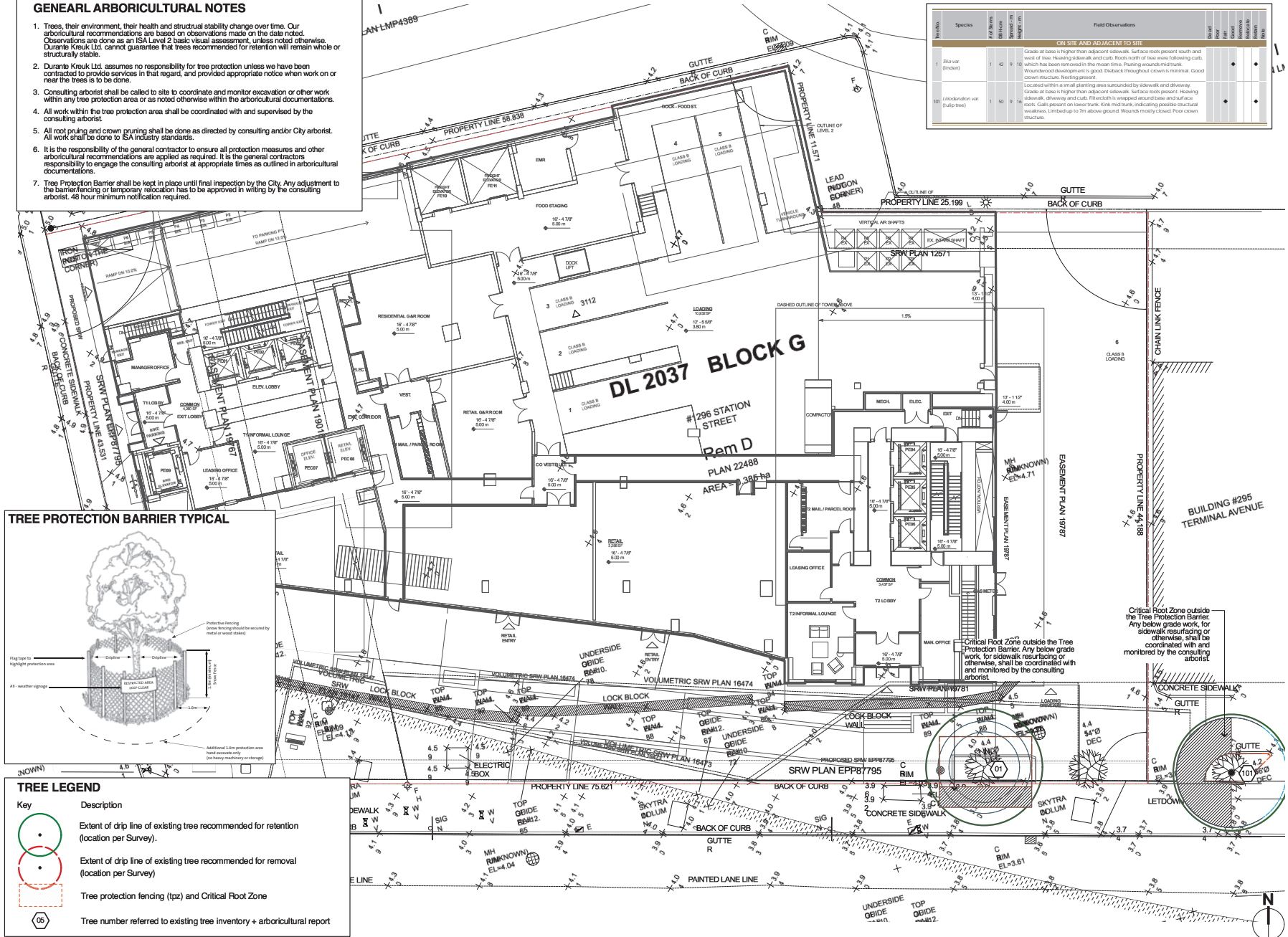
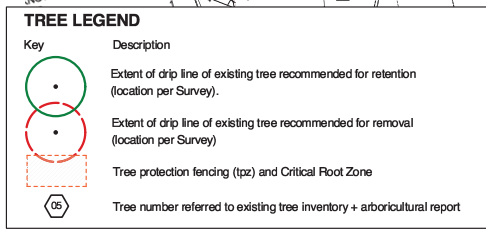
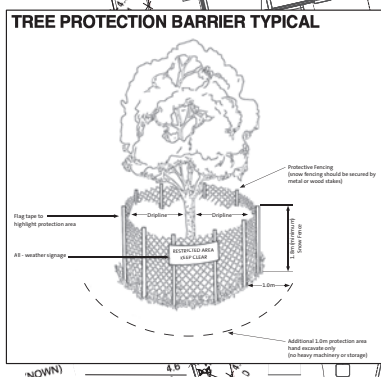
Sheet No.:

L.00

GENERAL ARBORICULTURAL NOTES

1. Trees, their environment, their health and structural stability change over time. Our arboricultural recommendations are based on observations made on the date noted. Observations are done as an ISA Level 2 basic visual assessment, unless noted otherwise. Durante Kreuk Ltd. cannot guarantee that trees recommended for retention will remain whole or structurally stable.
2. Durante Kreuk Ltd. assumes no responsibility for tree protection unless we have been contracted to provide services in that regard, and provided appropriate notice when work on or near the trees is to be done.
3. Consulting arborist shall be called to site to coordinate and monitor excavation or other work within any tree protection area or as noted otherwise within the arboricultural documentations.
4. All work within the tree protection area shall be coordinated with and supervised by the consulting arborist.
5. All root pruning and crown pruning shall be done as directed by consulting and/or City arborist. All work shall be done to ISA industry standards.
6. It is the responsibility of the general contractor to ensure all protection measures and other arboricultural recommendations are applied as required. It is the general contractor's responsibility to engage the consulting arborist at appropriate times as outlined in arboricultural documentations.
7. Tree Protection Barrier shall be kept in place until final inspection by the City. Any adjustment to the barrier/fencing or temporary relocation has to be approved in writing by the consulting arborist. 48 hour minimum notification required.

No.	Species	DBH (cm)	Height (m)	Field Observations	Health			
					Root	Trunk	Branches	Leaves
1	Red Oak (Quercus)	1	10	Grade at base is higher than adjacent sidewalk. Surface roots present south and west of base. Heavy shade and curb. Bottom of tree over following curb which has been removed in the mean time. Pruning wounds mid trunk. Wood-bored development is good. Dashed through-pipe crown is minimal. Good crown structure. Nesting present.	+	+	+	+
10	Red Robin (Junco)	1	1.5	Located within a small planting area surrounded by sidewalk and driveway. Grade at base is higher than adjacent sidewalk. Surface roots present. Heavy shade, driveway and curb. Filtration is wrapped around base and surface roots. Curb present on lower trunk. Risk mid trunk, indicating possible structural weakness. Limited up to 7m above ground. Wounds mostly closed. Poor crown structure.	+	+	+	+



13 2025-08-12 Issue for recording
 1 3 April 2024 Issued with Arborist Report FF
 no.: date: term: by:
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 102-1537 West 5th Avenue Vancouver B.C. V6J 1N6
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 Client:

Project:
1220 STATION STREET
VANCOUVER BC

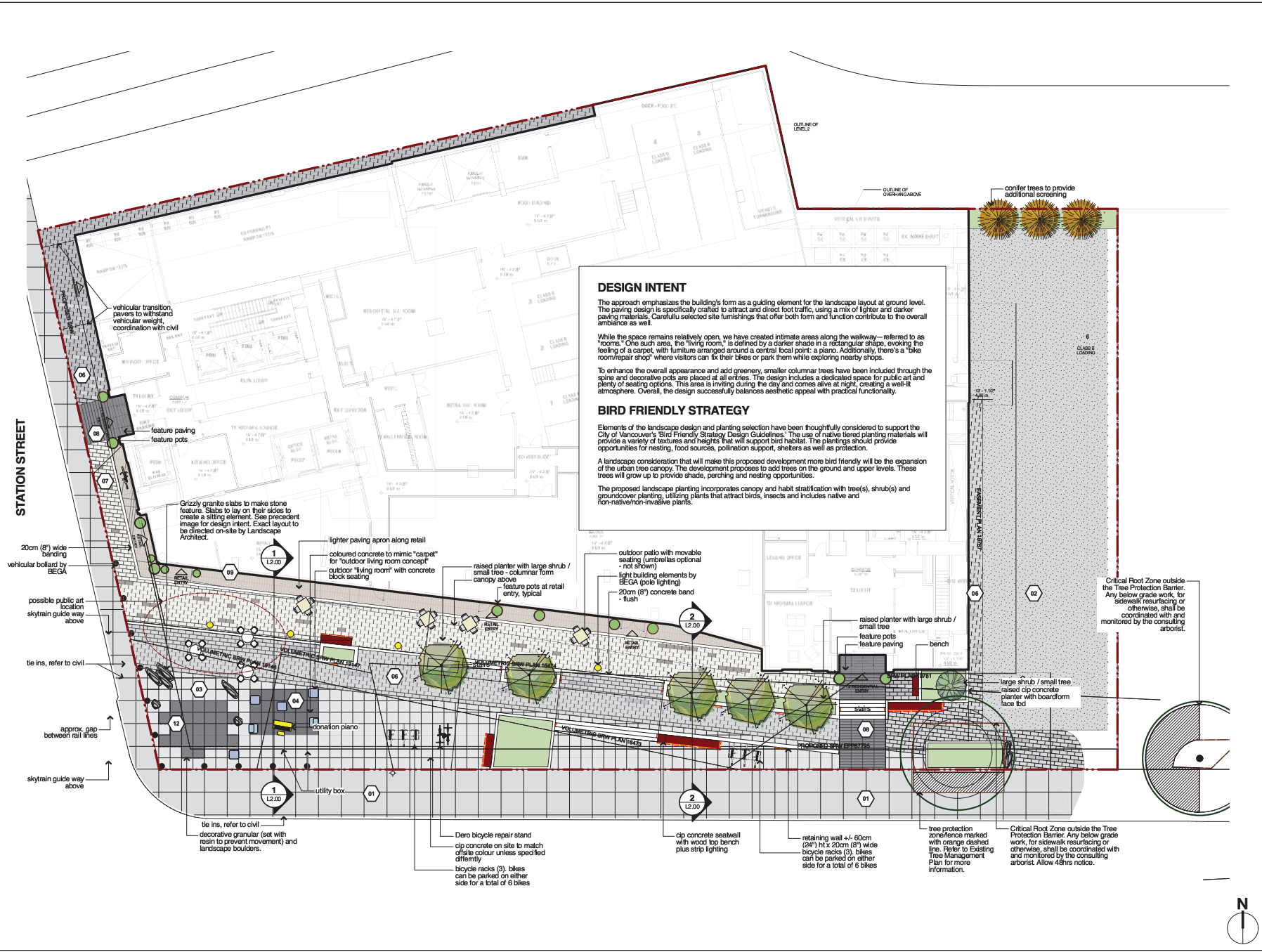
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VANCOUVER BC

Drawn by: FF
 Checked by: FF
 Date: 3 APRIL 2024
 Scale: 3/32" = 1'-0"

Drawing Title:
EXISTING TREES MANAGEMENT PLAN

Project No.:
 24012
 Sheet No.:

T1.00



DESIGN INTENT

The approach emphasizes the building's form as a guiding element for the landscape layout at ground level. The paving design is specifically crafted to attract and direct foot traffic, using a mix of lighter and darker paving materials. Carefully selected site furnishings that offer both form and function contribute to the overall ambience as well.

While the space remains relatively open, we have created intimate areas along the walkway—referred to as "rooms." One such area, the "living room," is defined by a darker shade in a rectangular shape, evoking the feeling of a carpet, with furniture arranged around a central focal point, a piano. Additionally, there's a "bike room/repair shop" where visitors can fix their bikes or park them while exploring nearby shops.

To enhance the overall appearance and add greenery, smaller columnar trees have been included through the spine and decorative pots are placed at all entries. The design includes a dedicated space for public art and plenty of seating options. This area is lively during the day and comes alive at night, creating a well-lit atmosphere. Overall, the design successfully balances aesthetic appeal with practical functionality.

BIRD FRIENDLY STRATEGY

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The proposed landscape planting incorporates canopy and habit stratification with tree(s), shrub(s) and groundcover planting, utilizing plants that attract birds, insects and includes native and non-native/non-invasive plants.

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1	2024-04-26	issue for client review

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 102-1837 West 5th Avenue Vancouver, B.C. V6J 1N5
 P: 604.684.4611 | F: 604.684.0577 | www.dkbc.ca
 Client:

GWL REALTY ADVISORS

Project:
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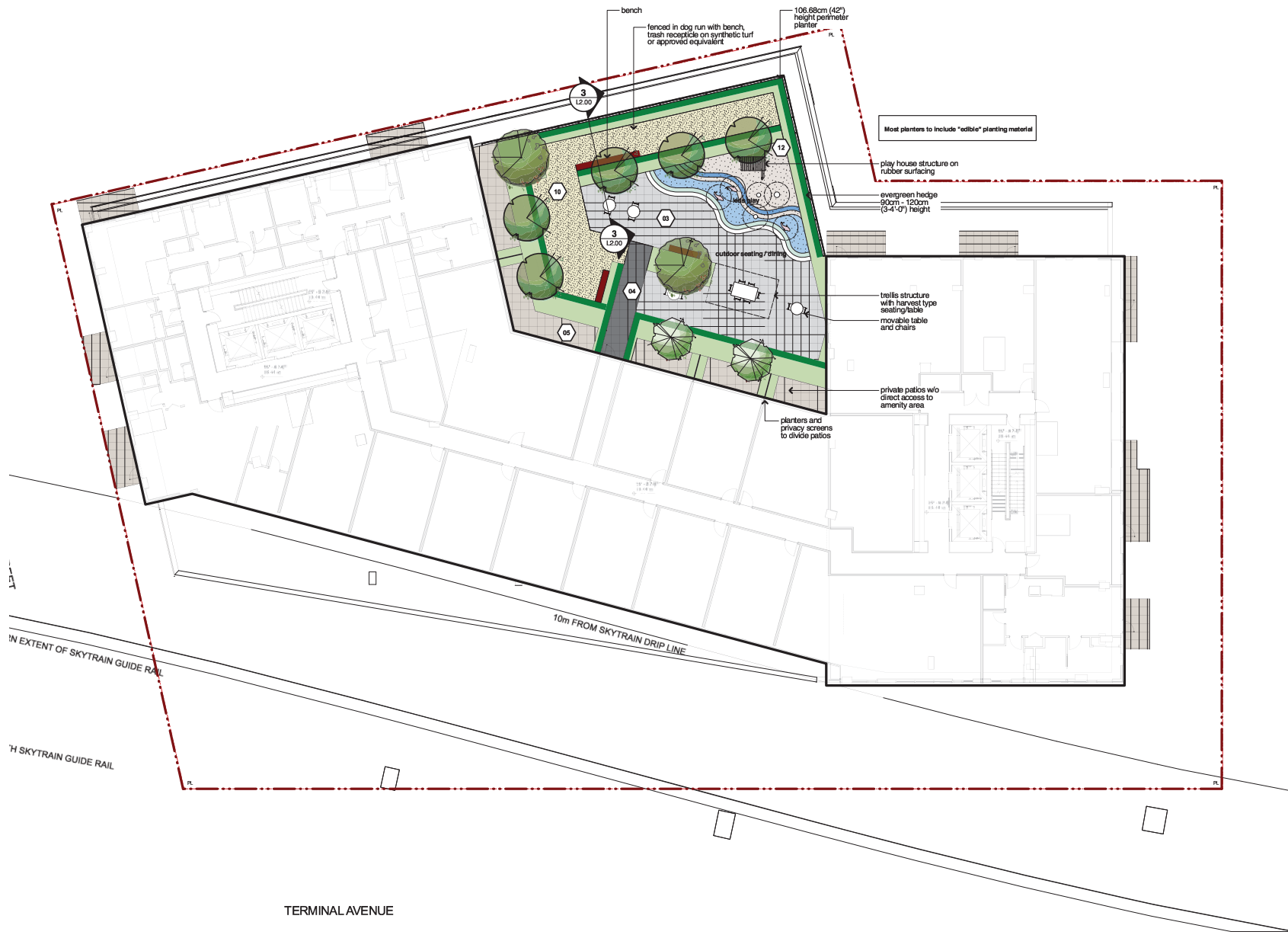
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Drawn by: AGB
 Checked by: SV
 Date: 16 JUN 2023
 Scale: 3/32" = 1'-0"
 Drawing Title:

LEVEL 1 - SITE PLAN

Project No.: 24012
 Sheet No.:

L1.00



- 13 2025-08-12 issue for rezoning
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1220 STATION STREET VANCOUVER BC

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 Date: 16 JUN 2023
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 Drawing Title:

LEVEL 5 - SITE PLAN

Project No.:
24012
 Sheet No.:

L1.01



TERMINAL AVENUE

13	2025-08-12	issue for rezoning
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 Client:

**GWL REALTY
 ADVISORS**

Project:
1220 STATION STREET

**1220 STATION STREET,
 VANCOUVER BC**

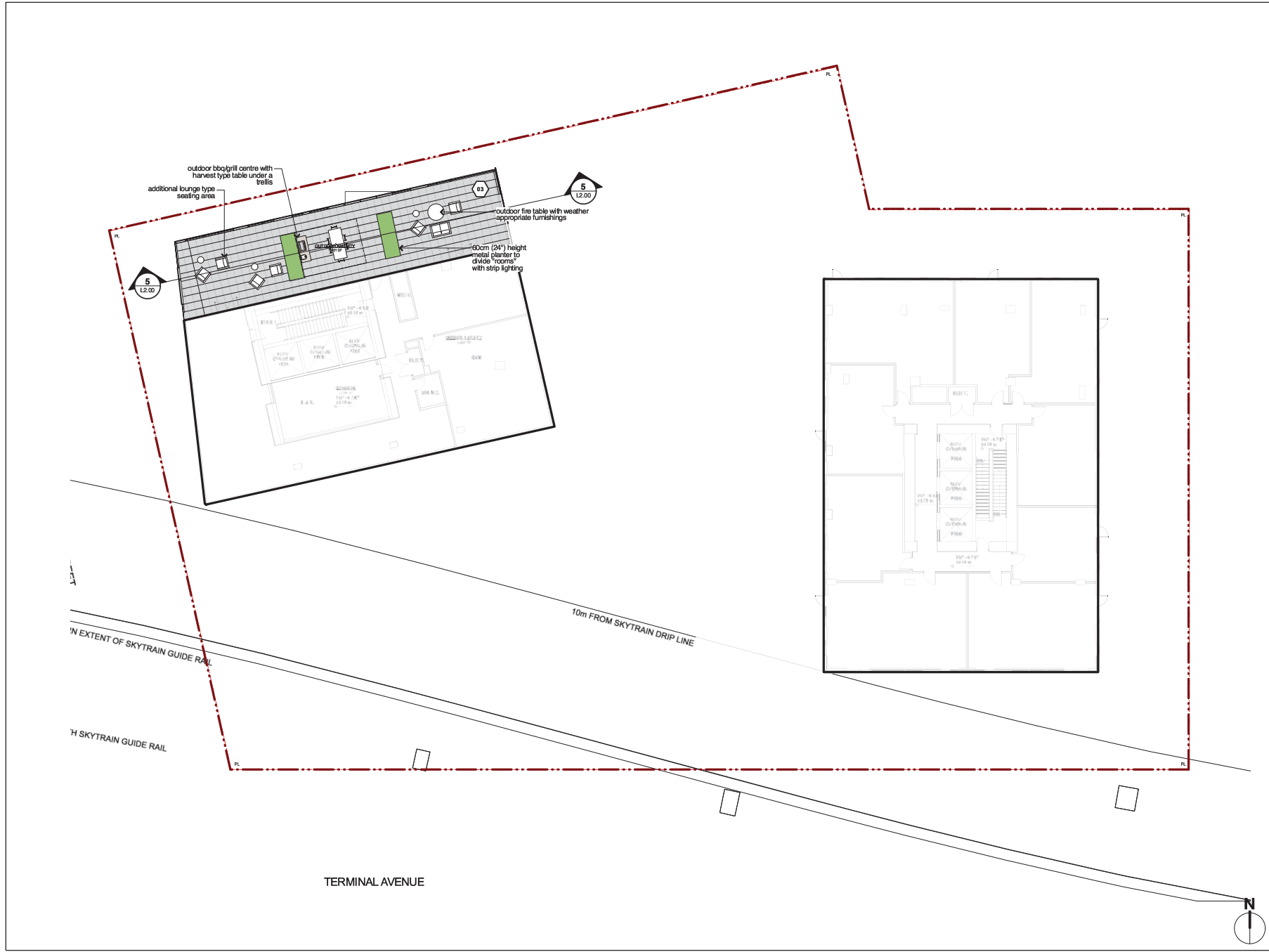
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LEVEL 9 - SITE PLAN

Project No.:
24012
 Sheet No.:

L1.02





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Project:
1220 STATION STREET VANCOUVER BC

1220 STATION STREET, VANCOUVER BC

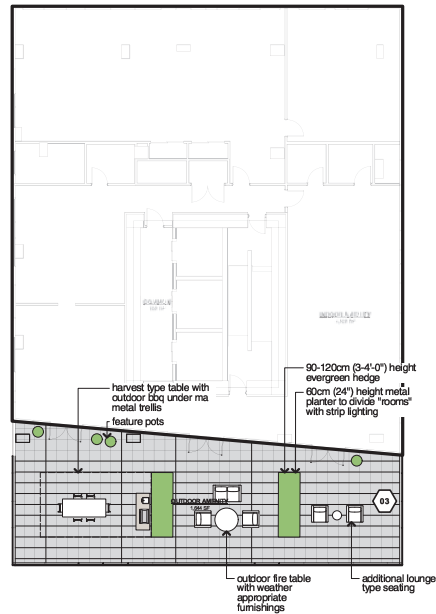
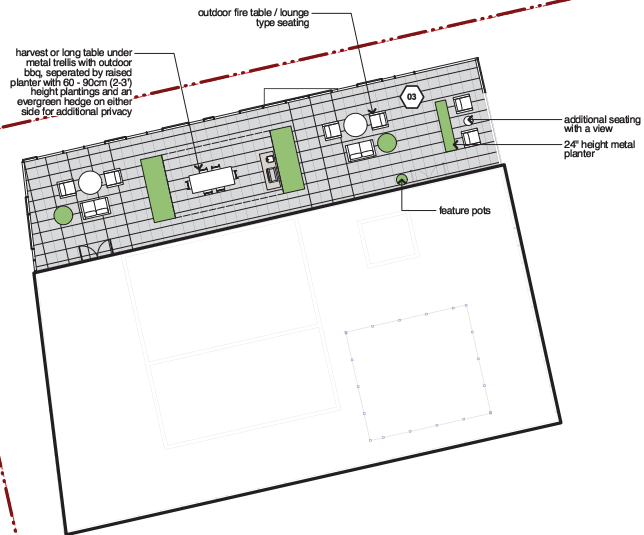
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 Checked by: SV
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Drawing Title:
LEVEL 28 - SITE PLAN

Project No.:
24012
 Sheet No.:

L1.03





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Project:
1220 STATION STREET

1220 STATION STREET, VANCOUVER BC

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 Checked by: SV
 Date: 16 JUN 2023
 Scale: 3/32" = 1'-0"

Drawing Title:

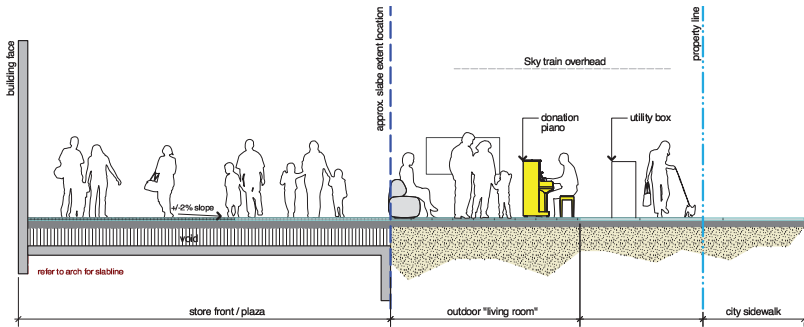
ROOF - SITE PLAN

Project No.:
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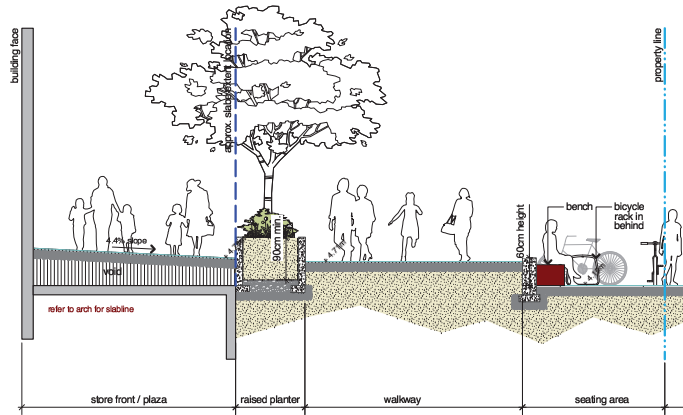
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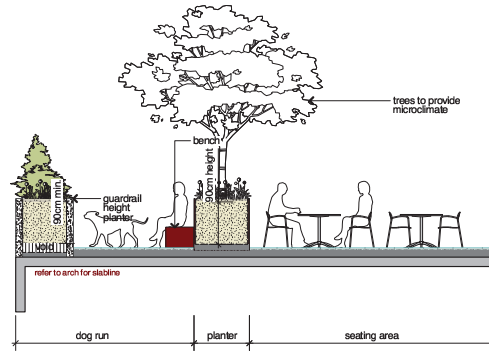




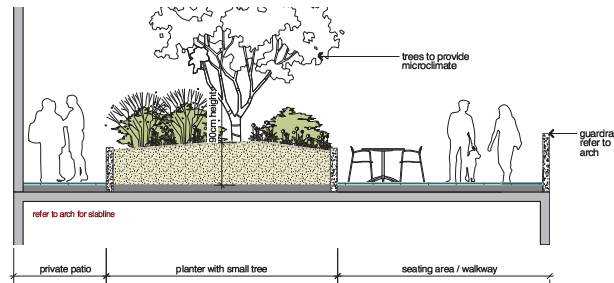
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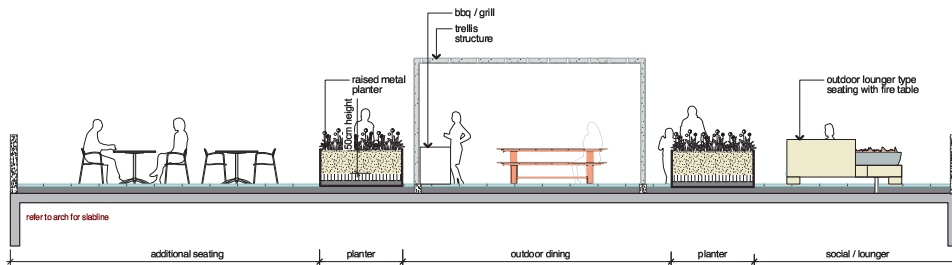
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3 LEVEL 5 - DOG RUN
L2.00 Scale = 1:50



4 LEVEL 9 - AMENITY
L2.00 Scale = 1:50



5 LEVEL 28 - ROOF AMENITY
L2.00 Scale = 1:50

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P:604.684.4611 | F:604.684.0577 | www.dk1bc.ca

Client:

**GWL REALTY
ADVISORS**

Project:

1220 STATION STREET

**1220 STATION STREET,
VANCOUVER BC**

Drawn by: AGB

Checked by: SV

Date: 16 JUN 2023

Scale: as shown

Drawing Title:

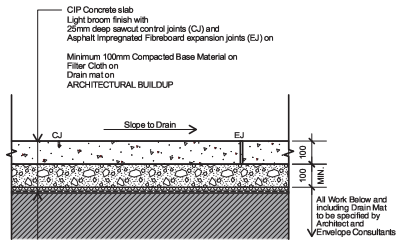
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Project No.:

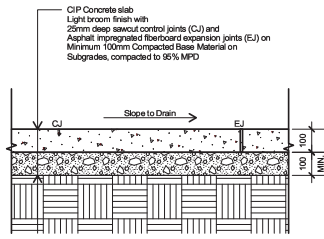
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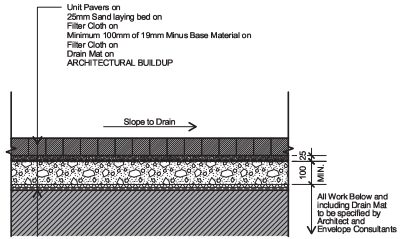
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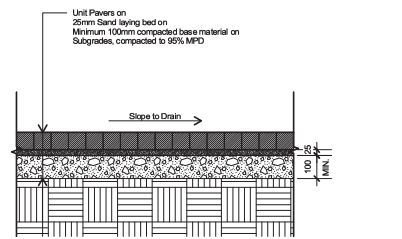
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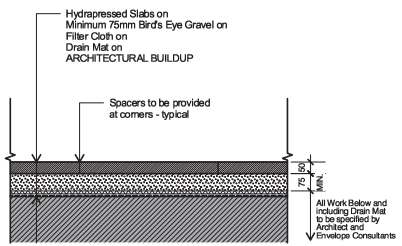
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L3.00 Scale = 1:10



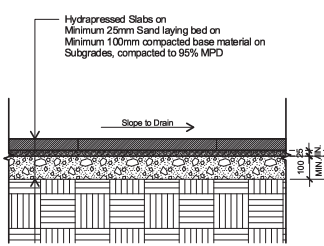
3 UNIT PAVERS - ON SLAB
L3.00 Scale = 1:10



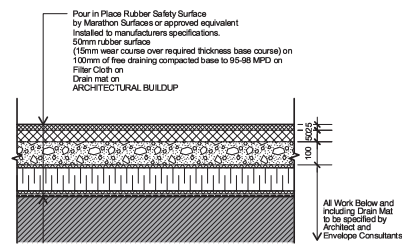
4 UNIT PAVERS - ON GRADE
L3.00 Scale = 1:10



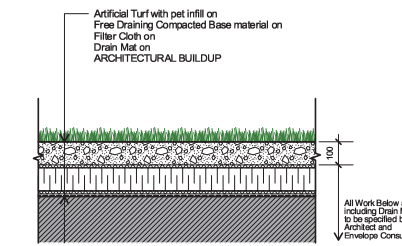
5 CONCRETE SLAB PAVER - ON SLAB
L3.00 Scale = 1:10



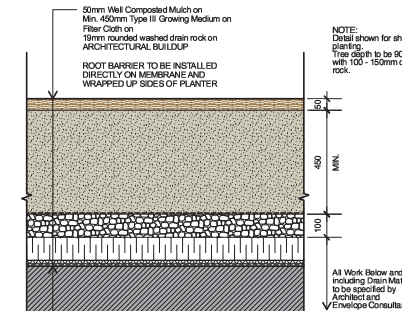
6 CONCRETE SLAB PAVER - ON GRADE
L3.00 Scale = 1:10



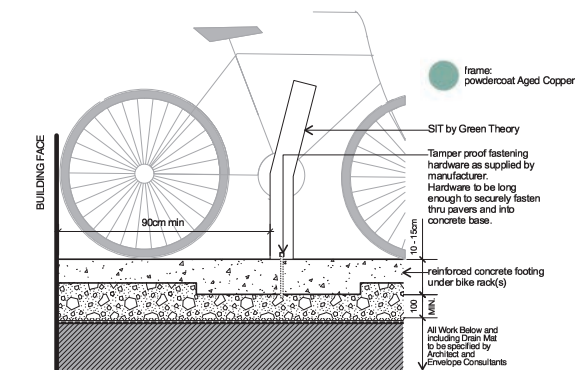
7 RUBBER SURFACING - ON SLAB
L3.00 Scale = 1:10



8 SYNTHETIC TURF - ON SLAB
L3.00 Scale = 1:10



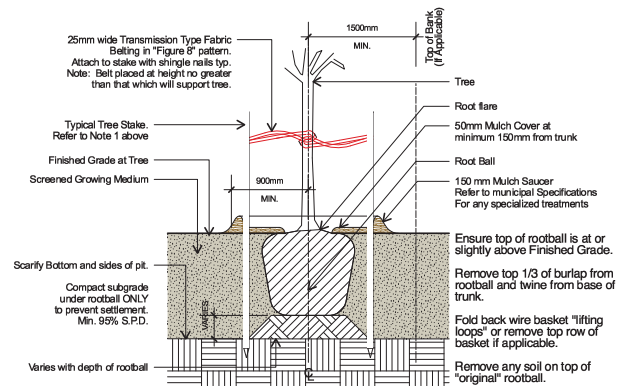
9 PLANTING - ON SLAB
L3.00 Scale = 1:10



10 BICYCLE RACK - ON SLAB
L3.00 Scale = 1:10

TREE PLANTING NOTES

- All street trees to be staked with 2 - 100mmØ x 2.5m long. Minimum depth of stake embedment is 1m. Ensure stakes do not penetrate or damage rootball. Ensure all stakes are aligned parallel to sidewalk/road on tree pit centreline.
- Do not cut Tree Leader.
- Ensure tree location does not conflict with Underground Services. "Call before digging".
- Protect tree from damage during planting. Ensure rootball protected from Sun, Frost or Desiccation.
- Provide min. 5 cubic meters of growing medium per tree, unless otherwise stated.
- Refer to Growing Medium Chart below for surface area depth of Growing Medium.



Depth	10 Cubic Meter - Soil Volume		5 Cubic Meter - Soil Volume	
	Area (m ²)	SQUARE Size of Surface	Area (m ²)	SQUARE Size of Surface
450	22.10	4.7m x 4.7m	11.11	3.3m x 3.3m
600	16.80	4.1m x 4.1m	8.33	2.9m x 2.9m
750	13.70	3.7m x 3.7m	6.67	2.6m x 2.6m

11 TREE PLANTING - ON GRADE
L3.00 Scale = 1:10

Rev.	Date	Item	By
13	2025-08-12	Issue for recording	
12	2024-10-03	Issue for coordination review	
11	2024-11-28	Issue for client review	
10	2024-10-16	Issue for client review	
9	2024-10-10	Issue for client coordination	
8	2024-09-09	Issue for client review	
7	2024-09-04	Issue for client review	
6	2024-05-25	Issue for client review	
5	2024-05-21	Issue for client review	
4	2024-05-19	Issue for client review	
3	2024-05-14	Issue for client review	
2	2024-05-08	Issue for client review	
1	2024-04-26	Issue for client review	

no.: | date: | item: | by:
Revisions:
Stamp:

dk
DURANTE KREUK LTD., LANDSCAPE ARCHITECTS
152-1537 West 5th Avenue Vancouver B.C. V6J 1N5
P: 604.684.4611 | F: 604.684.0577 | www.dkbc.ca
Client:

GWL REALTY ADVISORS

Project:
1220 STATION STREET

1220 STATION STREET, VANCOUVER BC

Drawn by: AGB
Checked by: SV
Date: 16 JUN 2023
Scale: as shown

DETAILS

Project No.:
24012

Sheet No.:

L3.00

